## City of John Day Innovation Gateway Area Plan: Technical Appendix

Technical Memos Meeting Notes Presentations

## Acknowledgements

## PAC/TAC

#### Project Advisory Committee (PAC)

Chris Cronin Chair of School Board Kara Kohfield John Day Resident Rancher, Seneca Alec Oliver Levi Manitsas Pastor / Hospital Board Ashlev Stevick John Dav Resident Lindsay Madden Eastern OR Realty Business Owner/Rancher, Grant County Gordon Larson Haley Walker Airport Manager / School Board **Dustin Oates** Ed Staubs Parks & Rec / Former Councilor Lisa Weigum King Williams Business Owner, Canyon City Resident Elliot Sky John Day Resident / New Resident Leann Gast Cosmetology College former owner Blue Mountain Land Trust, Mt. Vernon Amanda Martino Kate Cueno Forest Service, John Day Resident Shaun Robertson **Business Owner Cindy Lemcke** Worksource Oregon Grant Co. Economic Development Office Allison Field Didgette McCracken OSU, Extension & Outreach Jake Bellinger Business Owner in John Day **Rob Raschio** Public Defender/Local Attorney

#### **Technical Advisory Committee (TAC)**

Ken Boethin	Planning Commission Chair
Angie Jones	District Manager, Grant Co. Transp. District
Shane Griffin	ODOT District 14
Shannon Adair	City Councilor
Derek Daly	CEO, Blue Mountain Hospital District
Bret Uptmor	School District 3 Superintendent
Scott Myers	Grant County Judge
Amy Charette	Watershed Restoration Coordinator, Warm Springs
	Reservation of Oregon
Russ Young	Parks & Rec District Representative
Roy Walker	USDA Forest Service Representative
Brent Smith	ODFW Representative
Bruce Ward	President, Grant Co. Chamber of Commerce
Robert Waltenburg	Grant ESD Superintendent

## City of John Day

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Cheryl Jarvis-Smith

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Walker Macy Landscape Architects and Planners Ken Pirie, Principal Mike Zilis, Principal Aaron Maples, Landscape Designer Tiffany Swift, Landscape Designer

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This project is partially funded by a grant from the Transportation and Growth Management ("TGM") Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Fixing American's Surface Transportation Act ("FAST-Act"), local government, and State of Oregon funds.

The contents of this document do not necessarily reflect views or policies of the State of Oregon.

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## **Consultant Presentations**

PAC/TAC, Open House #1 and City Council (January 8, 2019) PAC/TAC, Open House #2 and City Council (March 12, 2019) PAC/TAC, REDS & Open House #3 (June 10-11 2019) Planning Commission and City Council Work Session (August 27, 2019)

## Public Involvement and Outreach Approach for City of John Day Innovation Gateway Area Plan Process

Prepared by JLA Public Involvement and Walker Macy

This Public Involvement Plan will guide stakeholder outreach and public involvement activities for the development of an Innovation Gateway Area Plan for the City of John Day, which will include concepts for circulation, redevelopment of the Oregon Pine mill site, parks, gardens and open spaces along a restored John Day River and community amenities such as an Aquatic Center, conference and hotel space, greenhouses and public gathering places. This PI plan describes activities that the consultant and City of John Day will implement in order to assure that interested and affected parties have adequate opportunities to provide meaningful input to the plan.

## **Project Purpose**

The John Day Innovation Gateway Area Plan will provide guidance on the redevelopment over time for the 83-acre riverfront property on the west side of the City to function as a gateway and focal point for the community. The plan will integrate transportation, open space, public infrastructure opportunities, and economic development opportunities.

## **Public Involvement Goals**

Throughout this planning effort, the consultant team and City of John Day are committed to sharing information and gathering input on ideas, concerns, and questions from the public, stakeholders, and affected parties related to the project area.

The public involvement goals are to:

- Identify and engage potentially affected and/or interested individuals, communities, and organizations that live, learn, work, and play in the City of John Day and Grant County.
- Educate public on the importance of active transportation and active transportation infrastructure to the immediate community and those who visit.
- Encourage creativity and visioning for increasing active transportation and economic opportunities on the site.
- Foster and sustain a collaborative and mutually respectful process, while being transparent about opportunities that may be presented during or after the area plan development.
- Communicate complete, accurate, understandable, and timely information to the public and partners throughout the area plan effort.
- Demonstrate how input has influenced the process and is incorporated into the final plan.

## **Concurrent Efforts/Opportunities and Coordination**

There are several existing efforts in the area that may influence the site or need to be accommodated in the area plan or in linkages with community activities. As this process moves forward, new opportunities may also arise that require coordination. Some of the current considerations and coordination include:

- Urban renewal efforts/funding opportunities
- Greenhouse start-up
- Treatment plant area planning including adjacent parks and gardens
- Adjacent property acquisition
- Highway frontage improvements
- Potential opportunity site for a future aquatic center or other community need

- Kam Wah Chung State Heritage Site plans (State Parks Centennial is in 2022 – opportunity to highlight the area)
- Parks and trails adjacent to the John Day River and a remapped floodplain
- Street and trail extensions
- Grant or other funding opportunities that are being explored

## Key Messages

The use of key messages throughout project communications is helpful in maintaining consistent information about the project goals, scope and desired outcomes. These messages are to be used both on written communications and as talking points for the project team staff.

## Objectives of this effort

- Support development of the former Oregon Pine mill site and adjacent City-owned land to unify design, land uses and transportation connections.
- Rebrand John Day as a thriving rural community to retain residents, attract visitors and sustainable commerce.
- Identify an area for the new wastewater treatment plant to be constructed outside the 100year floodplain.
- Create a community destination attraction focused on rural innovation and rural value creation to support infill development and a multimodal transportation system.
- Create a beautiful and scenic environment to promote inviting community gathering places and launch new opportunities through transit supportive urban designs.
- Provide active transportation choices to serve all ability levels and provide a safe and comfortable user experience.
- Restore public access to the riverfront on both sides of the John Day River and enhance the greenbelt along Canyon Creek to link recreational areas and neighborhoods.

• Improve efficiency in use of land and public infrastructure investments so they become vibrant places.

• Enhance planned and existing amenities which support conservation and renewable energy.

• Apply smart growth development strategies to optimize collaboration and leverage publicprivate partnerships.

• Capitalize on natural assets that improve livability and health of the community.

## The Process

- This project is committed to a **transparent and collaborative process** in which community members are invited to participate.
- This project will aim to **educate** community members about the different types and benefits of active transportation and how connections can support the site and greater community. This information can help community members give informed opinions about what best meets their needs.
- This project will **build on current momentum, opportunities, plans and projects** in order to create a coherent and effective plan for land uses, connectivity on the site to support economic activity and serve placemaking in the City of John Day.
- At this project's conclusion, the goal should be community support for the project and for subsequent implementation efforts. No community members should be reasonably expected to express a lack of knowledge about at least some aspects of the project. Ideally, the PAC/TAC and other stakeholders can continue to be community champions for the concepts after the consultant team work has wrapped up.

## **Public Involvement Process and Outreach**

## **Advisory Bodies**

This project will include a Technical Advisory Committee (TAC) and a Project Advisory Committee (PAC) comprised of individuals from a variety of agency and community organizations, as well as local individuals in the area or who represent specific interests or connections in the area. The TAC and PAC will likely meet jointly during the course of the planning effort. The TAC and PAC members will provide input on project objectives, existing conditions, site concepts, infrastructure needs, active transportation opportunities and connections, as well as other topics related to the gateway or focal point of the community. They will also support engaging others in the community and explore future implementation discussions. They will review project deliverables and provide feedback. In addition to providing ongoing project input with the PAC, the TAC will ensure consistency with any relevant plans or priorities for their organizations in an advisory role. The project will seek City Council and Planning Commission adoption for the final plan at the end of the process and a letter of adoption will be submitted to DLCD to summarize how the project meets Statewide **Planning Goals and the Transportation Planning Rule.** Certain elements of the plan will need to be approved by ODOT, the County, or other agencies related to land use or transportation.

## Public Involvement, Outreach, and Communication Schedule

The following outlines the strategies for involving, engaging, and informing the public and community stakeholders about the planning effort.

Public Involvement. Outreach and Communications Activities	Purpose	Responsibility	Date
Project website	Provides all project information in one location	City of John Day Consultant team provides content updates; graphics to post	Updates Ongoing
Project fliers/mailers/postcards	Serve as invitation to public events/input opportunities	City of John Day and consultant team to determine when City of John Day to print distribute if needed	As needed at input milestones
Project Area map and factsheet	Introduce the project process, timeline, and how to be involved.	Consultant team	December 2018 plus updates prior to/after meetings
Social Media/Email distribution	Build overall awareness and promote input opportunities Share project progress	City of John Day	2 and 1 weeks prior to each event 1 day reminders prior to each event Ongoing, upon findings
Community introduction in Quarterly City newsletter	Introduce or share project information and updates, advertise open house/workshops	City of John Day with consultant team	Provide contact to City for January by December 21 Quarterly - Identify next

			content deadlines
Media Releases	Introduce project and milestones, advertise public input opportunities	City of John Day with consultant team	December 21 City submits media release for January events Media releases 2 weeks prior to public meetings Ongoing
Online introduction/feedback survey	Initial opportunity to share project and provide a method for collecting initial input related to goals, objectives, and community identity	Consultant create; City to post to website	Survey posted by January 1
TAC/PAC meetings 1, 2, and 3 (coordinated with broader public input opportunities/open houses)	Meeting 1: Introduce team committee, discuss goals and vision, existing conditions, set up workshop map exercises to explore site opportunities Meeting 2: Review and provide input on concepts Meeting 3: Review revised concept		Mtg 1: January 8 (conf) Mtg 2: March 12 (tent) Mtg 3: May 14 (tent)
Public Open House/Workshop #1 (in-person and online) Plus complementary online survey launched	Introduce project and gather feedback from community members about redevelopment	Consultant and City	January 8 Simplified online survey January 1-31

January 1	objectives and community identity; use maps to collect information on existing conditions, connections, ideas, and environment Simplified yet complimentary online survey to introduce process and collect information on goals, objectives, and community identity (reserve any map exercise for online open house #2)		
City Council Briefing with State of the City Address	Share process and what we've heard, introduce concepts	City of John Day Consultant to provide graphics, summary info, as needed	1 <sup>st</sup> Council meeting in March
Open house #2 (online only)\ Online open house with interactive mapping exercise and active lifestyle survey questions	Share concepts for public feedback, discuss concepts related to community connections, active use, etc Provide an in- person option for those without access at a public site (self-guided or stop by and provide feedback)		March
Public Open House/Workshop #3 (in-person and online)	Share revised concept Seek input on opportunities and conversation on implementation	Consultant with City of John Day	May (potential for coordination with Economic Summit)

Regional Economic Development Summit	Highlight Funding/Partnering opportunities Tour of Innovation Gateway – How to make it real	City of John Day	May (pending scheduling)
City Council/Planning Commission Worksession	Worksession on draft plan elements	City of John Day	June 25, 2019 (tent)
Summer Outreach	Tabling at community events to share draft plan	City of John Day	June through August
City Council/Planning Commission Hearings	Hearing on plan	City of John Day	July 23, 2019 (tent)
Media event	Share details of plan, images, announcements	City of John Day	August 2019
Community Celebration	Host an onsite tour and celebration onsite	City of John Day	August 2019

Ongoing information out (City serves at public and media contact):

- Project fliers and media notices 2 weeks prior to events
- Social media 2 weeks, 1 week, 1 day prior to events
- Web updates before and after events
- Online survey 1 launched in advance of meeting 1 to double as notification
- Online open houses 2 and 3 likely to launch day of PAC/TAC meetings, but may need to follow (depending on graphic or input needs)
- Quarterly City manager updates (deadline specific)



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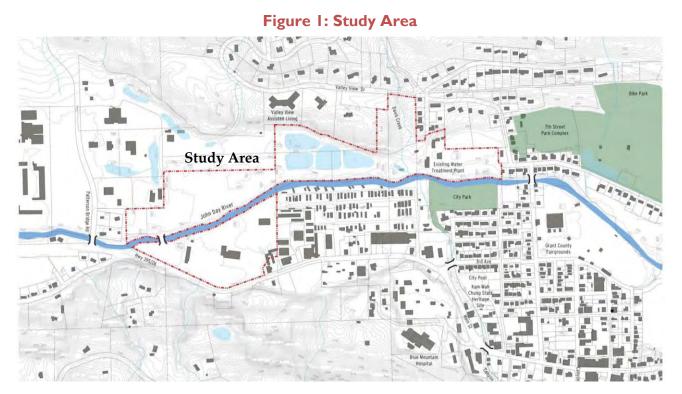
## MEMORANDUM

DATE:	March 19, 2019	www.dksassociates.com
TO:	John Day Innovation Gateway Area Plan Team	
FROM:	Kevin Chewuk, DKS Associates	
SUBJECT:	Technical Memo #2:	
	Baseline Transportation System Opportunities and Needs Assessment	P18194-000

This memorandum summarizes the multimodal transportation system and analysis for the John Day Innovation Gateway Area Plan study area. Included is an inventory of the existing transportation facilities, a safety evaluation of the roadways, and a qualitative review of the pedestrian, bicycle, transit and motor vehicle networks.

## **Study Area**

The study area encompasses 83-acres on the west side of the City of John Day and is generally bounded by Valley View Drive to the north, Bridge Street to the east, W Main Street (US 26 / US 395) to the south, and Patterson Bridge Road to the west.





## Where do People Want to Go?

One of the first steps in planning for an effective transportation system is gaining an understanding of the key destinations that people travel to throughout the City. Demand for travel is created by locations where people go to work, school, or to take care of other daily needs. These destinations are referred to as activity generators (or trip attractors). Activity generators represent important starting and ending points for travel in John Day, and they provide a basis for assessing important travel patterns.

## Within the City

John Day has numerous activity generators that attract residents and visitors alike. The most common categories of activity generators in the City include the following:

- Recreational/Entertainment (e.g., 7th Avenue Sports Complex, Kam Wah Chung State Heritage Site, John Day River)
- Schools (e.g., Blue Mountain Community College, Grant Union High School)
- Places of employment (e.g., Blue Mountain Hospital)
- Shopping (e.g., Downtown John Day, grocery stores, shopping centers, restaurants)
- Community/Government (e.g., City Hall, John Day Senior Center, Gleason Pool)
- Public Transportation (e.g., People Mover bus stop)

Each of these categories of activity generators represents important starting and ending points for travel and provides a good basis for planning ideal routes.

## **Outside of the City**

Having safe and efficient access to areas outside of the City is critical for many people who either live or work outside of John Day. Much of the traffic in John Day, especially during the weekday peak periods, is related to employment. As shown in Table 1, more than 60 percent of the workers in John Day live in another City that is located more than ten miles away. Residents of John Day also contribute to travel between cities, as shown in Table 2. Nearly half of workers living in John Day commute to employment locations at least ten miles outside of the City.

# DKS

Table I: Where John Day Workers Live				
John Day workers who:	Percent of John Day Residents			
Live in John Day	23%	-		
Live outside John Day	77%	-		
Live in Canyon City	9%	2+ miles		
Live in Mount Vernon	6%	8+ miles		
Live in Prairie City	3%	13+ miles		
Live in Dayville	3%	30+ miles		
Live in Seneca	2%	25+ miles		
Live in Other Cities	54%	40+ miles		

Source: Home Destination Report, On The Map, US Census Bureau, 2015.

John Day residents who:	Percent of John Day Workers	Distance from John Day
Work in John Day	38%	-
Work outside John Day	62%	-
Work in Canyon City	11%	2+ miles
Work in Mount Vernon	2%	8+ miles
Work in Prairie City	2%	13+ miles
Work in Other Cities	47%	25+ miles

Source: Work Destination Report, On The Map, US Census Bureau, 2015.

## **Key Demographics**

Demographic characteristics such as age and income play a key role in determining mode of transportation. John Day residents with lower incomes, as well as the youngest and oldest residents, often account for more trips via walking, biking and public transportation.

As seen in Table 3, about 28 percent of John Day residents are school-age children, while 17 percent of residents are over 65. The median income of John Day is around \$44,000.

# DKS

Table 3: Key Demographics in John Day		
Demographic	John Day	
Age (by percent of residents)		
School-Aged (Under 18)	28%	
College-Aged (18-24)	7%	
Middle-Aged (25 to 64)	48%	
Retired-Aged (65+)	17%	
Median Household Income	\$44,432	
Source: US Census Bureau, 2013-2017 America	n Community Survey.	

## **Existing Transportation Infrastructure**

Much of the land within the study area is rural, with the exception of land surrounding W Main Street through downtown John Day. As a result, many roadways are not constructed to urban standards. Evaluating the transportation impacts of rezoning the land requires an understanding of the current transportation facilities in this area. This section includes descriptions of existing infrastructure to serve pedestrian, bicycle, transit and motor vehicle modes of travel in the immediate John Day Innovation Gateway Area Plan study area.

## **Roadway System**

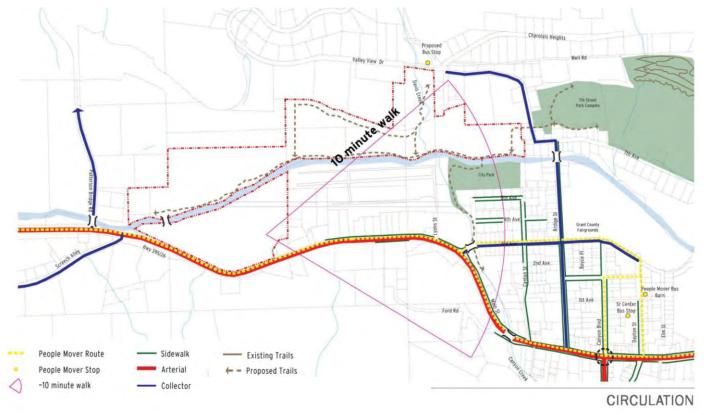
The major characteristics of the roadways in the study area are summarized in Table 4 and can be seen in Figure 2. The only streets providing for higher capacity motor vehicle movement through the study area is W Main Street and S Canyon Boulevard, which are classified by the state as a Statewide Highways. W Main Street runs east-to-west, and maintains a two-lane (i.e., one through lane in each direction) to three-lane cross-section (i.e., one through lane in each direction and a center turn lane) through the study area. Posted speeds along the highway in the study area range between 25 and 35 miles per hour. It is designated as a scenic byway and freight route, and some segments are within a special transportation area or urban business area.

S Canyon Boulevard runs north-to-south, intersecting with W Main Street. It maintains a two-lane cross-section with posted speeds between 25 and 35 miles per hour. It is designated as a freight route, and some segments are within a special transportation area or urban business area.

Bridge Street and Dayton Street run north-to-south, while 3rd Avenue runs east-to-west through the center of John Day, connecting to W Main Street. These streets are classified as collectors and



generally have lower vehicle-carrying capacity than the highway. Other key collector streets in the City include Patterson Bridge Road and Screech Alley. All other roadways in the study area are local streets and primarily serve local traffic traveling to and from the highway.



## Figure 2: Existing Transportation Facilities



Table 4: Study Area Roadway Characteristics				
Roadway (limits)	Functional Classification*	Cross section	Special Designations	
W Main Street - US 26 / US 395 (Patterson Bridge Road to 3rd Avenue)	Statewide	2 to 3 lanes	Scenic Byway; Freight Route; Urban Business Area (City limits to 3rd Avenue)	
W Main Street - US 26 / US 395 (3rd Avenue to US 395)	Statewide	2 lanes with on-street parking	Scenic Byway; Freight Route; Urban Business Area (3rd Avenue to Canyon Creek); Special Transportation Area (Canyon Creek to US 395)	
S Canyon Boulevard - US 395 (Canyon Boulevard to south UGB)	Statewide	2 lanes with on-street parking	Freight Route; Special Transportation Area (US 26 / US 395 to SW 3rd Avenue); Urban Business Area (SW 3rd Avenue to south UGB)	
Patterson Bridge Road (US 26 / US 395 to northern terminus)	Collector	2 lanes	None	
Screech Alley (US 26 / US 395 to Bench Road)	Collector	2 lanes	None	
7th Avenue (Bridge Street to western terminus)	Local Street	2 lanes	None	
3rd Avenue (US 26 / US 395 to Bridge Street)	Collector	2 lanes	None	
Bridge Street (US 26 / US 395 to 7th Avenue)	Collector	2 lanes	None	
Dayton Street (US 26 / US 395 to 3rd Avenue)	Collector	2 lanes	None	

#### \*Source: Oregon Highway Plan; John Day Transportation System Plan, December 1996

## **Pedestrian and Bicycle System**

Table 5 and Figure 2 shows the key roadways with pedestrian and bicycle facilities. Due to the rural nature of the abutting land uses, many streets have not been improved to urban standards and generally lack accommodation for pedestrian and bicycle users. The exception being segments of W Main Street, S Canyon Boulevard, 3rd Avenue, Bridge Street, Dayton Street and a few local streets (i.e., Canyon Boulevard, Canton Street, 2nd Avenue, 4th Avenue and 5th Avenue), which provides a sidewalk on at least one side of the street for pedestrians. Bike lanes are also provided for bicycle travel along S Canyon Boulevard between SW 2nd Avenue and SW 6th Avenue.



W Main Street and S Canyon Boulevard are important connections for pedestrian and bicycle travel in the City. They provide the only current route for pedestrians and bicyclists to the western, eastern and southern parts of the City. Those walking or biking along portions of these highways often have to walk along the edge or share the travel lane with motor vehicles. In addition, frequent driveways negatively impact the walking experience and introduce conflict points between pedestrians and motor vehicles. Motor vehicle traffic volumes along these highways are over 5,000 vehicles per day and the posted speeds range up to 35 miles per hour. These conditions are generally not conducive to comfortable shared walking and biking travel conditions. Main Street through John Day is also designated as part of the Old West Oregon Scenic Bikeway. Much of this route lacks accommodations for bicyclists.

Roadway (limits)	Pedestrian Facilities	<b>Bike Facilities</b>	
W Main Street - US 26 / US 395 (Patterson Bridge Road to 3rd Avenue)	Sidewalk on north side from 3rd Avenue to the west for 0.25 miles; Intermittent sidewalks on south side	None	
W Main Street - US 26 / US 395 (3rd Avenue to US 395)	Sidewalk on both sides from Ford Road to US 395; Sidewalk on north side only west of Ford Road	None	
S Canyon Boulevard - US 395 (Canyon Boulevard to south UGB)	Sidewalk on both sides north of SW 3rd Avenue; Sidewalk on west side only south of SW 3rd Avenue	Bike lanes south of SW 2nd Avenue; None north of SW 2nd Avenue	
Patterson Bridge Road (US 26 / US 395 to northern terminus)	None	None	
Screech Alley (US 26 / US 395 to Bench Road)	None	None	
7th Avenue (Bridge Street to western terminus)	None	None	
3rd Avenue (US 26 / US 395 to Dayton Street)	Sidewalk on both sides from US 26 / US 395 to Brent Drive; none east of Brent Drive	None	
Bridge Street (US 26 / US 395 to 7th Avenue)	Sidewalk on both sides from US 26 / US 395 to 3rd Avenue; Sidewalk on east side only north of 3rd Avenue	None	
Dayton Street (US 26 / US 395 to 3rd Avenue)	Sidewalk on west side south of 1st Avenue; None north of 1 <sup>st</sup> Avenue	None	

## Table 5: Study Area Pedestrian and Bicycle Characteristics



## **Highway Crossings**

As a major street connection through the area, W Main Street should not be a barrier to pedestrian and bicycle travel between the neighborhoods and businesses on the north and south side of the street. While most of the south side of the highway is undeveloped, especially areas towards the west side of the City, safe and comfortable pedestrian and bicycle crossings should be provided in convenient areas to encourage ease of access.

W Main Street is currently a two to three lane arterial street with a posted speed up to 35 miles per hour. The S Canyon Boulevard - US 395 intersection provides a signalized opportunity for pedestrians and bicyclists to cross W Main Street. Marked crosswalks are also available near the Riverside Mobile Home Park, Old West Federal Credit Union and at Dayton Street. The Riverside Mobile Home Park crossing includes signage. Curb ramps and street lighting are provided at the highway crossings, although the lighting is not pedestrian scaled, and the curb ramps are all in poor condition. Curb ramps at all other pedestrian crossings in John Day rate poorly or are missing. Several locations along the W Main Street with existing sidewalks lack curb ramps, including various approaches at the intersections of Lyons Street, 3<sup>rd</sup> Avenue, Ford Road, Canton Street, Bailey Place and Brent Drive.

## **River Crossings**

There are two bridges along public streets and one private bridge that cross the John Day River. The bridges are located at Patterson Bridge Road and Bridge Street, while the private crossing is located about 850 feet to the east of Patterson Bridge Road. These crossings are the only existing connections between the north and south side of the City. The bridge at Patterson Bridge Road does not provide facilities for pedestrian or bicycle travel, while the one at Bridge Street provides a sidewalk on one side.

There are also two bridges over Canyon Creek, providing the only existing improved connection between the east and west part of the City. The bridges are located at NW 3<sup>rd</sup> Avenue and W Main Street. These bridges provide sidewalks on both sides for pedestrian travel, but lack bicycle facilities.

## Transit

Transit service is provided in John Day and other nearby cities by the Grant County People Mover via several fixed bus routes, a Dial-a-Ride service and two deviated fixed route systems. The People Mover connects riders in John Day to nearby cities including Bend, Redmond, Prineville, Mount Vernon, Monument, Pendleton, Walla Walla, Burns, Prairie City and Baker City. The Bend, Redmond, Prineville, Mount Vernon route runs Monday, Wednesday and Friday; the Monument route runs on Thursdays; the Pendleton and Walla Walla route runs on Tuesdays; the Burns route runs on the 1st, 3rd, and 5th Thursday of the month; and the Prairie City and Baker City route runs on the 2nd and



4th Thursday of the month. Each of these routes typically depart John Day in the morning and return in the evening.

The deviated fixed route services in the John Day valley run Monday through Friday. The route will deviate ½ mile from the fixed route line. The PC MV Route runs between Prairie City and Mt Vernon three times a day, at 7 am, 12 pm and 6 pm. The JD CC Loop runs a set route in John Day and Canyon City every hour, beginning at 7 am and ending at 6 pm. Both the PC MV Route and the JD CC Loop are free.

The Dial-a-Ride, or Demand Response service runs Monday through Friday 8 am to 6 pm and Saturday 9 am to 4 pm. This service picks and drops off passengers at a location of their choosing. Anyone is eligible to ride this service, and it is available in John Day, Canyon City, Mt Vernon and Prairie City.

The deviated fixed routes have transit stops throughout John Day, with 56 stops in and around the City. Most of the intercity routes pick-up and drop-off passengers at the People Mover Bus Depot located on NE Dayton Street near at NE 1st Avenue. However, the Monument to John Day route pick-ups and drop-offs passengers at the Senior Center parking lot on NE Dayton Street south of NE 1st Avenue.

Transit users in the John Day Innovation Gateway Area Plan study area are generally less than one quarter mile from the closest bus stop (within the typical trip length for the average walking trip).

## **Transportation System Performance**

The transportation infrastructure in the study area was evaluated with a variety of measures in order to document the existing deficiencies of the transportation system. Information reviewed included safety of the roadways and a qualitative review of the pedestrian, bicycle networks, transit and motor vehicle networks.

## **Safety Evaluation**

Safety of the roadways in the study area was assessed through historic collision data to identify deficiencies. The data along the roadways was reviewed to identify potential patterns for motor vehicle, pedestrian, and bicyclist collisions.

Collision data from the past five years (January 2013 through December 2017) was obtained from the Oregon Department of Transportation (ODOT) for all roadways within the John Day Innovation Gateway Area Plan study area. Over the past five years, 41 collisions, or an average of about ten per year, were identified along study area roadways. A majority of these collisions (26 of the 41) involved



drivers running into fixed objects or failing to yield when making a turn. None of the collisions involved a pedestrian or bicyclist.

The severity of the collisions was generally low, with most (31 of the 41 collisions) involving property damage only (no injuries). There were three collisions involving major injuries, two involving moderate injuries and five involving minor injuries. There were no fatalities over the past five years. All of the collisions involving serious injuries occurred along Main Street, one each at S Canyon Boulevard and Gunther Street, when a driver rear-ended another driver, and another at Hillcrest Road, when a driver collided with an animal.

## **Roadway Segment Safety**

A segment collision rate along W Main Street was calculated to provide a picture of roadway safety. Segment collision rates are determined by dividing the number of collisions along the segment by the total vehicle traffic along the segment and are reported in crashes per million vehicle miles traveled (MVMT). Since W Main Street through the study area includes no major intersections, it was analyzed as a single segment and crash rates were compared to the five-year average of state highway crash rates published in Table II of the 2016 ODOT Crash Rate Book.

The collision rate calculated (based on the past five years of collision data) for the highway segment can be seen in Table 6. The segment collision rate was normal when compared to other similar highway segments across Oregon.

Table 6: Highway Segmen	t Collision	Analysis			
Roadway (limits)	Distance (miles)	Total Collisions (2013 to 2017)	Observed Crash Rate (per MVMT)	Statewide Collison Rate (per MVMT)	Over Statewide Collison Rate
W Main Street - US 26 / US 395					
(Patterson Bridge Road to S Canyon Boulevard-US 395)	1.23	16	1.17	1.37	No

## **Transportation Network Conditions**

To assess the pedestrian, bicycle, transit and motor vehicle network conditions within the study area, a high-level qualitative evaluation was conducted based on the ODOT Qualitative Multimodal



Assessment methodology<sup>1</sup>. The quality and availability of various characteristics are rated systemwide as "Excellent", "Good", "Fair", or "Poor". The intent of the analysis is to show the extent to which each network provides a level of comfort and safety for users. The analysis will be used to inform, create, and confirm recommendations for improvement projects.

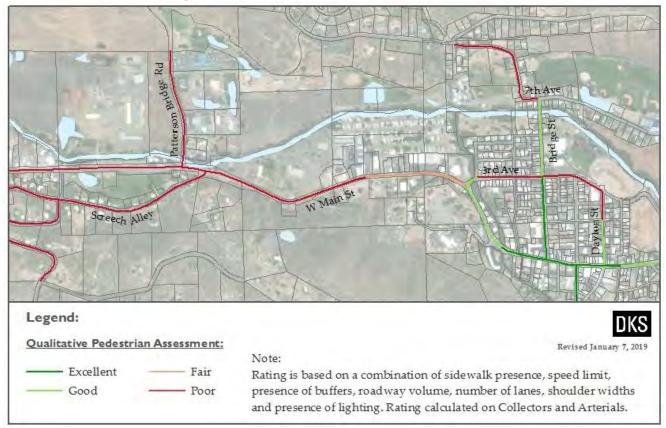
## **Pedestrian Network Conditions**

For the pedestrian network evaluation, consideration is given to the presence of a sidewalk or path, a buffer zone (i.e., bike lane, shoulder, landscape strip, or on-street parking) and street lighting, and the number of travel lanes and travel speeds along the adjacent roadway. In the study area, an "Excellent" rating requires sidewalks on both sides of the roadway, along with a buffer. A "Good" rating requires a sidewalk on at least one side of the roadway, along with a buffer. A "Fair" rating is given to a roadway with a sidewalk on at least one side, but without a buffer. A "Poor" rating denotes gaps within the sidewalks along that corridor.

Figure 3 summarizes the pedestrian network conditions in the study area. Overall, the network rates poorly in the study area. This result is not surprising given the rural nature of much of the area. The segment of Main Street between NW 3rd Avenue and NE 3rd Avenue rated as "Good" or "Excellent" since it has a sidewalk on at least one side of the roadway, along with on-street parking in some areas, while the segment west of NW 3rd Avenue rated as "Fair" since it has a curb-tight sidewalk. Bridge Street south of NW 3rd Avenue and S. Canyon Boulevard north of SW 3rd Avenue rated as "Excellent" since they have sidewalks on both sides, along with on-street parking.

<sup>&</sup>lt;sup>1</sup> Analysis Procedures Manual Version 2, Oregon Department of Transportation, November 2018.





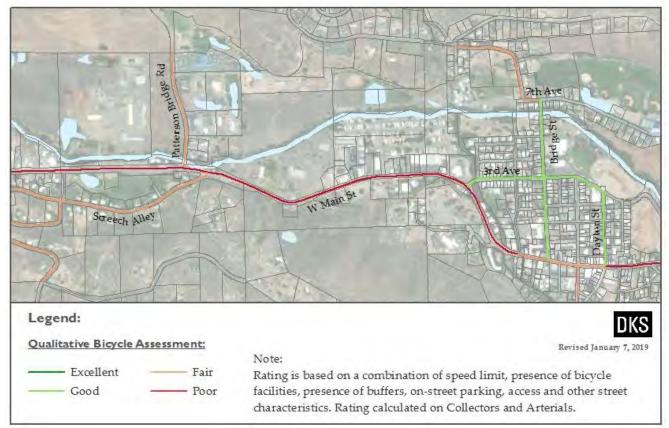
#### Figure 3: Qualitative Pedestrian Assessment

#### **Bicycle Network Conditions**

For the bicycle network evaluation, consideration is given to the presence and width of bike facilities (i.e., bike lane, shoulder, path, shared lane markings), grade and pavement conditions of the roadway, and the number of travel lanes, motor vehicle volumes, and travel speeds along the adjacent roadway. For the bicycle network evaluation of the study area, an "Excellent" rating requires separated bicycle facilities. A "Good" rating requires adequate bicycle facilities and width given the segment characteristics. A "Fair" rating is given to a roadway with bicycle facilities, but without the preferred facility type or width. A "Poor" rating denotes gaps within the bike network along that corridor.

Figure 4 summarizes the bicycle network conditions in the study area. Bridge Street, Dayton Street and 3rd Avenue rated as "Good" since they are level roadways with low traffic volumes, and slow motor vehicle travel speeds. Several streets rated as "Fair" since they have slightly higher motor vehicle travel speeds or volumes. Several segments of Main Street rated as "Poor" due to it having no bike facilities with higher motor vehicle volumes.





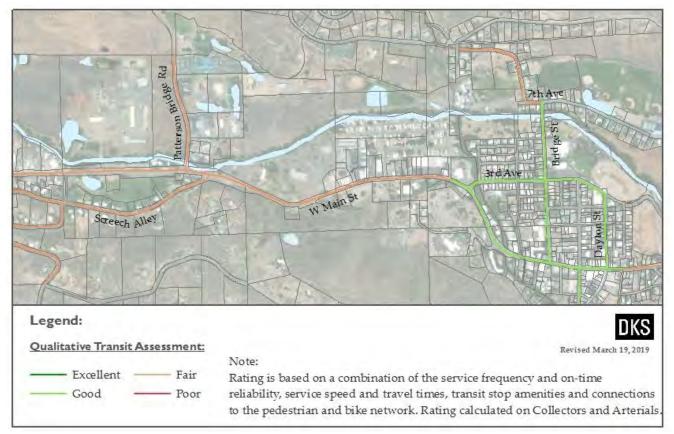
## Figure 4: Qualitative Bicycle Assessment

#### **Transit Network Conditions**

For the transit network evaluation, consideration is given to the service frequency and on-time reliability, service speed and travel times, transit stop amenities and connections to the pedestrian and bike network. In the study area, an "Excellent" rating requires a transit stop within a quarter-mile that is well connected to the pedestrian and bike network, with amenities and frequent service. A "Good" rating requires a transit stop within a quarter-mile that is well connected to the pedestrian and bike network, but without amenities or frequent service. A "Fair" rating is given to a roadway with a transit stop within a quarter-mile but is not connected to the pedestrian and bicycle network and lacks amenities or frequent service. A "Poor" rating denotes no transit service along that corridor.

Figure 5 also summarizes the transit network conditions in the study area. Overall, the network rates highly in central John Day since the transit stops are well connected to the pedestrian and bike network. The roadway segments outside of central John Day rated "Fair" since they are within ½ mile of a transit stop, but lack connections to the pedestrian and bike network.





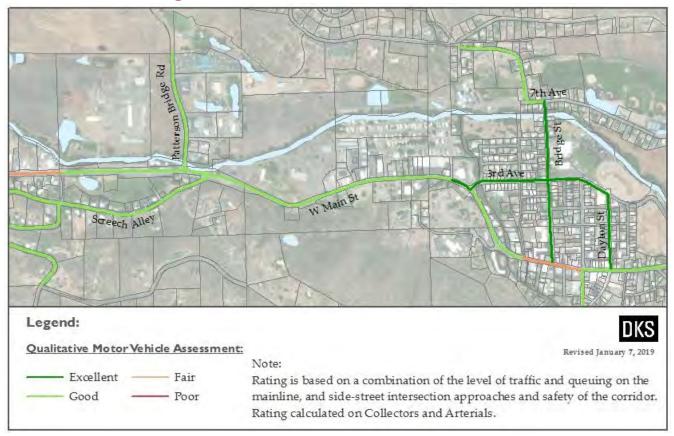
## Figure 5: Qualitative Transit Assessment

#### **Motor Vehicle Conditions**

For the motor vehicle network evaluation, consideration is given to the level of traffic and queuing on the mainline, and side-street intersection approaches and safety of the corridor including presence of street lighting, driveway density, intersection spacing, posted speeds, number of fixed objects, presence of a median or traffic separator and turn lanes. For the motor vehicle network evaluation of the study area, an "Excellent" rating requires no congestion and safe roadway conditions. A "Good" rating requires minimal congestion and reasonably safe roadway conditions. A "Fair" rating is given to a roadway with minimal congestion, but with less than desirable roadway conditions. A "Poor" rating denotes congestion or unsafe conditions along that corridor.

Figure 6 also summarizes the motor vehicle network conditions in the study area. Overall, the network rates highly in the study area. Most segments rated "Excellent" or "Good" since they have low levels of traffic and safe roadway conditions. A few segments of W Main Street rated "Fair" since they have a greater density of driveways.





#### Figure 6: Qualitative Motor Vehicle Assessment

#### **Motor Vehicle Volumes**

Average annual daily traffic (AADT) volumes along W Main Street were obtained from ODOT<sup>2</sup>. The data indicates AADT volumes range from around 4,800 near Patterson Bridge Road, 5,900 near NW 3<sup>rd</sup> Avenue, to 6,200 near S Canyon Boulevard-US 395.

#### Trucks

Within John Day, W Main Street - US 26 / US 395 and S Canyon Boulevard - US 395 are classified as Oregon Freight Routes. Most of the local freight generators are located at the west and south end of the City. Heavy vehicles account for approximately four percent of the traffic on W Main Street west

<sup>&</sup>lt;sup>2</sup> Based on counts obtained from the ODOT Traffic Volumes and Vehicle Classification system, Effective Date 12/31/2017.



of NW 3rd Avenue and seven percent of the traffic on W Main Street east of NW 3rd Avenue through John Day.

## **Opportunities and Needs**

Below is a summary of key opportunities and needs identified in the 1996 John Day Transportation System Plan (TSP), 2009 John Day Local Street Network Plan, or resulting from the analysis of baseline transportation conditions.

## **Pedestrian and Bicycle Network**

The pedestrian and bicycle network in the study area is undeveloped and disconnected from the rest of the City. Identified opportunities and needs include:

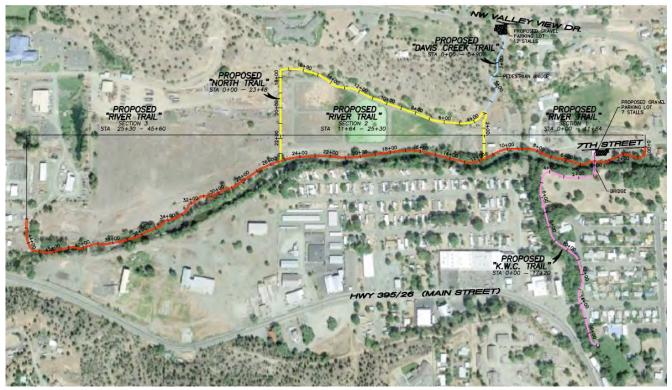
- Sidewalks on W Main Street. This route is needed to link Patterson Bridge Road with NW 3<sup>rd</sup> Avenue. The existing sidewalk on the north side of W Main Street between NW 3<sup>rd</sup> Avenue and west of NW Lyons Street is not constructed to ODOT standard and should be reconstructed. Topography constraints will hinder the ability to construct a sidewalk on the south side of W Main Street through the study area, but a continuous sidewalk on the north side is feasible. The ODOT standard for sidewalk width is six feet, with a minimum width of five feet acceptable on local streets. The unobstructed travel way for pedestrians should be clear of utility poles, sign posts, fire hydrants, vegetation and other street furnishings.
- Curb ramps. The existing curb ramps along highways in John Day all rate poorly or are missing. All intersections are crosswalks in Oregon and if sidewalks exist at intersections, curb ramps are required for each crossing. Improvements are needed to bring the substandard and missing curb ramps into Americans with Disabilities Act (ADA) compliance.
- Bikeway on W Main Street. This route is needed to link to surrounding areas of the City. Currently the narrow shoulders are generally not suitable for bike travel. ODOT standard width for a bicycle lane is six feet. The minimum width of a bicycle lane against a curb or adjacent to a parking lane is five feet. A bicycle lane may be as narrow as four feet, but only in very constrained situations. Bike lanes are most appropriate on arterials and collectors, where high traffic volumes and speeds warrant greater separation of travel modes.

Paved roadway shoulders not specifically designated for bicycle travel often accommodate bicyclists traveling along rural routes in Oregon. ODOT recommends a six-foot paved shoulder to adequately provide for bicyclists, and a four-foot minimum width in constrained areas.



Shared-use Path Network. A shared-use path along the north side of the John Day River linking Patterson Bridge Road with Bridge Street is envisioned to provide a more comfortable alternative route to W Main Street (see Figure 7). A pedestrian/bicycle crossing of the John Day River and a path along the east side of Canyon Creek is also planned to connect the John Day River Trail with the Kam Wah Chung site. An additional trail network is envisioned to the north, linking Valley View Drive with the John Day River Trail.

Shared-use paths are typically separated from the street and used by a variety of nonmotorized users, including pedestrians, bicyclists, skateboarders, and runners. Shared-use paths are typically paved (asphalt or concrete), but unpaved smooth surfaces may also meet ADA standards. Shared-use paths are usually wider than an average sidewalk (i.e. 10 - 14feet). The width may be reduced to as little as eight feet where bicycle and pedestrian volumes are expected to be low, good passing opportunities can be provided, and maintenance vehicle loads are not expected to damage the pavement.



## Figure 7: Planned Shared-Use Paths

Image Source: Sisul Engineering



Pedestrian facilities on Patterson Bridge Road.
 Patterson Bridge Road has low enough traffic volumes (expected daily volume less than 1,000 vehicles) and travel speeds (expected 85th percentile speed 28 mph or less), and enough pavement width that it could be suitable to designate a section of the existing street for walking. Striping a portion of the street could provide a dedicated area for walking without physically separating the facility from the roadway. Striped shoulders visually narrow the roadway and may slow traffic, making it more pedestrian friendly. The designated walking area can be painted to increase visibility.

## Striped Area of Roadway for Pedestrian Use



Advantages:

- 1. Cost-effective and easy to implement
- 2. No additional pavement or street widening needed
- 3. Provides stable surface for pedestrian travel
- 4. Striping will help alert drivers to expect pedestrians along the route
- 5. Ease of maintenance with ordinary street cleaning equipment

#### Disadvantages:

- 1. Would require improved street lighting in some areas, increasing utility costs
- 2. Less comfortable than separated sidewalks or shared-use paths
- 3. Increased maintenance with additional striping and/or painted street surface
- 4. No on-street parking on the side with the striped walking area.
- Bicycle facilities on Patterson Bridge Road. Designate Patterson Bridge Road as a shared roadway to connect W Main Street with a potential shared-use path adjacent to the John Day River. Shared roadways are facilities where bicyclists and motorists share the same travel lane. The most suitable roadways for shared bicycle use are those with low speeds (25 mph or less) and low traffic volumes (3,000 vehicles per day or fewer). Signed shared roadways are shared roadways that are designated and signed as bicycle routes and serve to provide continuity to other bicycle facilities (e.g. bicycle lanes) or to designate a preferred route through the community.



Common practice is to sign the route with standard Manual on Uniform Traffic Control Devices (MUTCD) green bicycle route signs with directional arrows. However, these facilities can be improved with the addition of yellow bicycle warning signs (MUTCD, W11-1) and Share the Road signage (MUTCD, W16-1P). An effective strategy for enhancing Shared Roadway facilities involves placing Shared Lane Markings (SLMs) on the roadway surface. Shared Lane Markings (also known as "sharrows") are high-visibility pavement markings that delineate where bicyclists should operate within a shared vehicle/bicycle travel lane (outside of the "door zone" on streets with on-street parking) and alert motorists to expect bicyclists on the roadway. Shared roadways can also be signed with innovative signing that highlights a special touring route (e.g. Old West Oregon Scenic Bikeway) or provides directional information in bicycling minutes or distance (e.g., "Library, 3 minutes, <sup>1</sup>/<sub>2</sub> mile").

#### Advantages:

- 1. Cost-effective and easy to implement
- 2. No additional pavement or street widening needed
- 3. Striping will help alert drivers to expect bicyclists along the route

Disadvantages:

- 1. May need improved street lighting in some areas, increasing utility costs
- 2. Less comfortable than bike lanes or shared-use paths

## **Motor Vehicle Network**

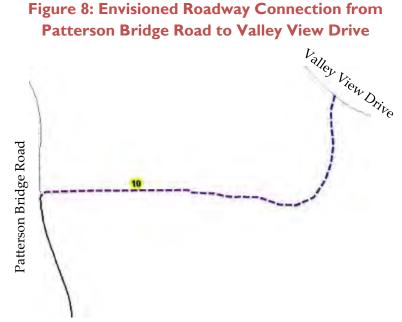
The downtown area of John Day has a grid-like pattern of streets, while the surrounding areas are generally disconnected and follow the natural topographic layout of the hillsides. W Main Street provides the only current motor vehicle connection from the study area to the rest of the City. Identified opportunities and needs include:

## Shared Roadway for Bicycle Travel





Roadway connection from
 Patterson Bridge Road to Valley
 View Drive. The TSP and Local
 Street Network Plan envision a
 roadway connection along the
 existing private road alignment
 between Patterson Bridge Road
 and Valley View Drive (see Figure
 8). This alignment will provide a
 connection between the study
 area and the north part of the
 City.



7th Street Extension to Patterson
 Bridge Road. There is no direct
 east to west roadway connection

on the north side of the City, with the only existing connection being W Main Street on the south side of the study area. An extension of 7th Street will provide an alternate alignment to the highway between Patterson Bridge Road and Bridge Street (see Figure 9). This will reduce reliance on the highway for travel within the City.



#### Figure 9: Planned 7th Street Extension

Image Source: Sisul Engineering



## **Transit Network**

While transit users in the John Day Innovation Gateway Area Plan study area are generally less than a quarter mile from the closest bus stop (within the typical trip length for the average walking trip), the transit stops lack pedestrian and bike network connections. Redevelopment in the study area will attract more people and could warrant the need for an additional bus stop or amenities. While the development may set the stage for future transit, the type and extent of service improvements will play out over time. Specifics of transit service will depend on the actual rate and type of development, City and County resources and policies, and consideration of local options. The John Day Innovation Gateway Area Plan will provide walking and biking accommodations and connections that enhance the future viability of potential transit service along study area streets.

	Nick Green, City of John Day Aaron Lieuallen, City of John Day Cheryl Jarvis-Smith, ODOT			FINAL Technical Memo #3: Existing Conditions	<b>Date:</b> 02/28/2019	
From:	Ken Pirie		Project:	John Day Innovation Gateway	Project #:	P3465
Distrib	ution:				_	
Basec	amp	Technical Memo #3: Existing Conditions				
	The following memorandum serves as a summary of known existing co Innovation Gateway study area and includes a summary of smart growt existing zoning and city policies and opportunities and constraints for ch study area. SECTION 1 Review of existing background information An extensive library of previous studies and existing data has been pro- project team, listed on page 12 of the Statement of Work. These docum been reviewed by the project team and they provide useful context and the project under several broad categories:					growth concepts,
						ocuments have
		<b>Transportation</b> As an ODOT TGM project, a certain baseline level of analysis is required project, as described in Technical Memo #2, in order to meet State transpolicies. This includes an analysis of transit and active transportation pot project, to ensure that City and County TSPs are updated. Relevant doct provided are:				transportation on potential for th
		City	<ul> <li>Roa</li> <li>1996</li> <li>2009</li> <li>Cap</li> <li>Trail</li> <li>Pend</li> </ul>	d Standards 5 TSP 9 John Day Local Street Network ital Improvement Plan plans and location of informal/ur ding plans within Project Area mapping data		thways
		Cou	unty • Grai • Grai	nt County TSP nt County Human Services Coord ple Mover transit route	linated Plan	
		State		gon Transportation Plan gon Highway Plan		

WALKER MACY

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- Oregon Bicycle and Pedestrian Plan
- 2012 HDM
- Transportation Planning Rule and No Reduction of Vehicle-Carrying Capacity (ORS 366.215)

#### Land Use Planning (and Smart Growth)

This project will resolve a number of land use challenges through smart growth planning and will be implemented by amending the Comprehensive Plan and potentially proposing updated development standards. Relevant documents provided are:

City

- 2009 Comprehensive Plan
- Zoning, Land Use and Development Code
- GIS mapping data

#### County

- Comprehensive Plan
- County Zoning, existing land uses, and data related to Project Area.

#### **Environmental Planning and Public Utilities**

The project will envision the John Day River as a central, cohesive element for the Innovation Gateway. Through integrated site planning and green infrastructure design, we believe the river can become a beautiful, green corridor through the city - a recreational and environmental resource that attracts visitors and improves pedestrian safety and public health. The innovative reuse of treated wastewater is an important resource in realizing the vision. Relevant documents provided to guide this vision are:

City

- Water and Sewer Plan
- Wastewater Facilities Plan Update
- Reclaimed Water Feasibility Study
- FEMA flood remapping project information
- Historic photos
- Topography maps
- Oregon Pine environmental review
- GIS data

#### **Economic Development and Workforce Housing**

A parallel study performed by ECONorthwest will focus on economic redevelopment aspects of the City and Innovation Gateway. The two studies will be well-integrated, on similar schedules. Relevant documents provided to the team are:

City

- 2017 Housing Needs Analysis
- 2017 Oregon Kitchen Table Survey Summary
- City Economic Opportunities Analysis
- Chamber Strategic 3-Year Plan

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#### SECTION 2 Development Potential and Feasibility

#### 2.1 City Strategy for Growth

The City of John Day and Grant County is a distressed rural area that has experienced a number of socio-economic problems including the highest unemployment rate in Oregon since 2012 (Grant County 6.5% percent as of June 2018); low real market property values and assessed values and struggles to find sufficient revenue to fund basic public services. Over 60% of working John Day residents work outside the City. John Day's population, which peaked in the 1990s, is now at the same level it was in the 1970s and is primarily white with a median age of 40.5. Title VI population figures are as follows: White (93%); Latino (2.7%); Native American (1.5%); Asian (0.7%); Black (0.3%) and two or more races (1.6%). One of the primary drivers for this study and other related projects is to slow and reverse population decline. In terms of age, the population is 28% School-Aged (under 18), 7% College-Aged (18-24), 48% Middle-Aged (25-64) and 17% Retired (65+).

The John Day & Canyon City Income Survey 2018 Report (Portland State University) found that the most common response to the question *"Which of the following best describes your family's total gross income, before taxes, for the previous 12 months?"* was less than \$29,900 with 43.4% (n=157) of respondents reporting this. The \$56,301 or more category was the second most common with 22.4% (n=81), but overall there is a bi-modal income distribution with 57.3% of the population making a low to moderate income.

#### As stated by the City in their "Strategy for Growth":

The intent of the strategy is to reverse the pattern of population and economic decline by achieving a modest and sustainable growth rate. While no specific population targets have been set, an annual growth rate of 0.5% would add nine new residents the first year. Even at an accelerated growth rate of 1.5%, the city would only add 26 new residents in the first year (roughly 8 new families). At the accelerated rate it would take over ten years to recover the population we've lost in the past three decades.

The strategy also includes an understanding of and sensitivity to our community's traditional values and culture. Our desire is to grow in a balanced way so we can preserve our rural lifestyle and quality of life for generations to come. The strategy for growth focuses on recruiting and retaining three core demographics:

- 1. Digital commuters who have a choice about where they work;
- 2. Active retirees with disposable income; and
- 3. Young, working families that contribute to the local economy and tax base.

Attracting these demographics will result in a more diversified and more resilient economy and will promote growth in supporting infrastructure and agencies, including the local hospital and schools. Targeting these demographics will require the City to focus its investments in four main areas:

- 1. Digital marketing and branding that promotes our rural quality of life;
- 2. Recreational amenities that make for attractive, active-lifestyle communities;
- 3. Competitive broadband infrastructure enabling residents to be digitally connected to friends, family and co-workers; and
- 4. Housing and community development initiatives that create more housing options across a wider and more balanced price range and community spaces that enhance the economic value of our city.

#### 2.2 Study Area Location and Parcel Sizes

The Innovation Gateway study area encompasses 90 acres on the west and north edges of the city of John Day and includes the 53-acre former Oregon Pine mill site on two parcels. One of these parcels (tax lot 300) includes the John Day River (meaning, the river is entirely within the parcel) and a smaller 2-acre parcel is located on the western edge of the study area. The southeastern parcel line was recently adjusted to reflect property line adjustments with Mills Building Supply and JD Rents. To the east of the mill site is the 30 acre City wastewater treatment plant property on 5 parcels. An additional 2 parcels

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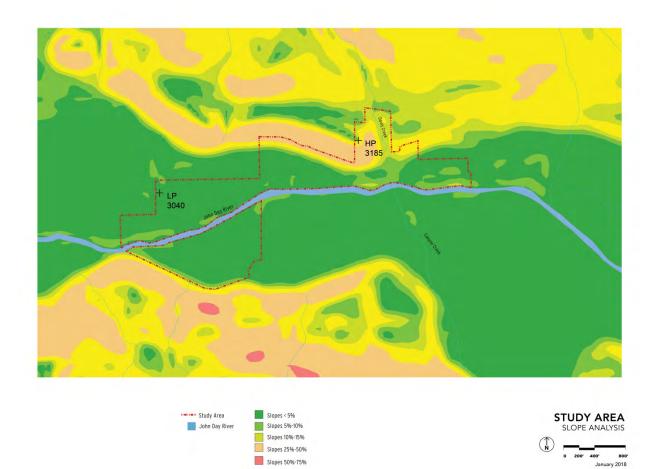
adjoining Davis Creek south of Valley View Drive have also been acquired by the City and are part of the study area.





#### Figure 2.1. Study area

The study area sits in the valley bottom of the John Day River and slopes gently toward the John Day River. The elevation of the study area is approximately 3,040 feet above mean sea level, rising to 3140' where Davis Creek flows into the area from the north. The steep slopes in the vicinity of Davis Creek are the only topographic challenge in the study area, but this area will likely only have trail and other low-impact recreational improvements.





#### 2.3 Utilities and Urban Services

The former Oregon Pine mill site was fully served with potable water supply with lines extended from a trunk line under US 26 and these can potentially be reused. Electrical service is available from the east and southern edges of the property. It is assumed that wastewater produced by future development will be conveyed to the proposed new wastewater treatment plant. Gas and telecom service to the site is not currently known. The City will be seeking grant funding in 2019, building on state seed money to enhance the currently limited broadband service to the John Day area, partnering with Ortelco through the Grant County Digital Network Coalition. The existing wastewater plant is fully served with utilities.

#### 2.4 New Wastewater Facility and Use of Treated Effluent

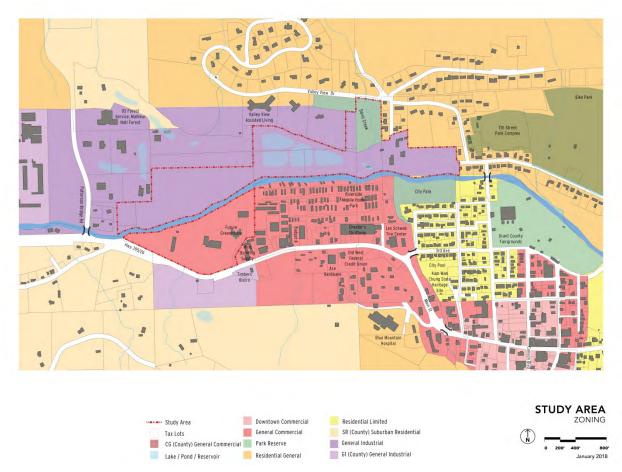
A primary tactic towards achieving the City's Growth Strategy at the Innovation Gateway project is the replacement of John Day's Wastewater Treatment Plant. The 2018 Wastewater Treatment Plant Plan Update proposes an innovative new strategy of constructing a membrane bioreactor with aerobic digestion and anticipated supply of treated effluent to new parks, gardens and most importantly, greenhouses. This treated water will be piped to a 6,200sf pilot-scale greenhouse, located east of the planar shed on the Oregon Pine mill site. The final design and manufacturing of the greenhouse has been awarded to EuroMex, with produce harvests beginning in 2019. With two full-time employees, the pilot-scale greenhouse should generate roughly 1,200 pounds of fresh produce per week. Local restaurants and grocers such as Chester's Thriftway intend to purchase produce from the City at wholesale prices. Revenue from the greenhouse will accrue to the Sewer fund to offset its operating expenditures and ultimately the cost of wastewater treatment. The innovative project will also become a tourist attraction in

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its own right, while portraying evidence of an entrepreneurial public sector for companies potentially interested in investing in John Day.

#### 2.5 Existing Zoning

The study area is now entirely within the urban growth boundary and city limits of John Day. The City recently annexed remaining portions of the Oregon Pine mill site which were previously outside of the city limits and still under county zoning. The City also applied city zoning designations to these areas which are generally consistent with the vision for the project area. A significant portion of the study area (primarily north of the John Day River) is zoned for industrial use. The majority of the project area south of the river, including the portion of the area recently annexed into the City, is zoned for commercial use. The area southeast of the confluence of the John Day River and Canyon Creek (planned for future river recreation access) was recently rezoned to the City's Park Reserve zone to ensure consistency with current use and future plans for the property. Existing zoning designations are shown in Figure 2.3 below, and allowed uses (permitted outright and conditionally) in the applicable industrial and commercial zones are summarized in Table 1.





In addition to establishing allowed uses in each zoning designation, the city and county development codes outline other design and development standards. In both the city and county industrial zones, development standards are generally limited to building setbacks and heights, and requirements associated with parking, landscaping fences, signs and stream setbacks. The city's commercial zones

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include a variety of additional standards related to building orientation, block and lot layout, architectural design and pedestrian amenities.

Table 1:	
Land Use Designation	Allowed Uses
General Industrial (City)	Residential: Existing as of 11/24/2005 <u>Commercial</u> : Drive-Up/Drive-In/Drive-Through, Offices, Parking lot (CU), Vehicle servicing and repair, Retail sales under 10,000 sf, Self-service storage <u>Industrial</u> : Industrial services, Manufacturing, Warehouse and freight movement, Waste-related (CU), Wholesale sales <u>Institutional</u> : Utilities, Public safety facilities, Parks and open space (CU) <u>Other</u> : Accessory structures, Agriculture (existing as of 11/24/2005 and as an accessory to a permitted industrial use), Mining (CU)
General Commercial (City)	Residential: Existing as of 11/24/2005 and/or (?) not occupying a ground floor space on Main Street Commercial: Drive-Up/Drive-In/Drive-Through (CU), Bed and breakfast inn, Educational services, not a school, Entertainment – major event (CU), Offices, Outdoor recreation (CU), Parking lot (CU), Vehicle servicing and repair – fully enclosed (CU), Retail sales and commercial services Industrial: Industrial services – fully enclosed, Manufacturing – fully enclosed, Wholesale sales – fully enclosed and under 20,000 sf (CU) Institutional: Utilities, Community service, Family day care, Adult day care, Day care subject to state licencing (CU), Parks and open space (P/CU), Religious institutions – new (CU), Schools – new (CU) Other: Accessory structures, Radio frequency transmission facilities (CU), Utility corridors (CU), Temporary uses (P/CU), Transportation facilities

#### 2.6 Potential Development Code Changes

As part of the process of adopting and implementing this plan and given the recent annexation and rezoning of remaining portions of the study area, the primary remaining land use actions for the study area will be to review and adjust specific development code requirements that apply to the area. For example, some future uses envisioned in the study area could be incompatible with existing standards for building height, setbacks, landscaping requirements, or architectural design standards. If so, modifications to those requirements may be needed. This could be done by amending the base zoning designations or by establishing an overlay zone or other mechanism that allows for variations in the standards applied within the study area.

#### 2.7 Intergovernmental Agreements and Special Districts

Scarce planning resources need to be integrated to allow for appropriate transportation networks such as multimodal trails, sidewalks, and other important infrastructure to be developed in a coordinated fashion. Since 2011, Grant County and the City of John Day have had an Urban Growth Management Agreement, which stipulates that Grant County is responsible for administering land use for projects outside the John Day city limits, but within the UGB. Such areas are subject to the City Comprehensive Plan and when development is proposed, in order to receive city services, annexation and city zoning will be initiated.

Given the recent annexation of the portions of the study area that were previously located outside the city limits, this issue will no longer be directly applicable to development or provision of services within the study area. However, to the extent that future development occurs in areas outside the city limits but within the UGB adjacent to the area, the City will be responsible for commenting on proposals, since city development standards will be applied. In doing so, it will be important to ensure that such proposals are compatible with and support the vision of the project area. The City will be the preferred provider of urban services in such areas. Extensions will be permitted when consistent with the Comp Plan and city standards. New roads will also be built to city standards.

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Park and recreation facilities and services are provided by the John Day Canyon City Parks and Recreation District. This district was established to help provide a dedicated funding source and agency to serve regional park and recreation needs. The City will need to continue to coordinate with the District for future planning of capital facility improvements and recreational programming that meets the needs of John Day residents and the goals for the project area.

No other special districts are located in the area. However, emergency services (fire and medical) and law enforcement are provided by Grant County. Similar to the JDCC Parks District, the City will need to continue to coordinate with the County regarding future provision of these services.

#### 2.8 Urban Context and Existing Land Uses

Situated on the western outskirts of the City of John Day, the 83-acre study area is surrounded by a mixture of land uses, from residential to light industrial. US Highway 26 borders the 53-acre Oregon Pine mill portion of the study area on the south. The mill site has recently been annexed into the City and rezoned for consistency with the vision for the project area. One single-family residence is the only adjacent development on the south side of the highway. To the west, the study area is accessed from Patterson Bridge Road and along the west property line the adjacent properties are a residence, Clark's Disposal Services, Oregon Department of Forestry, and the Oregon Department of Fish and Wildlife. The Iron Triangle Logging Company offices, truck shop, and storage yard occupy the property adjacent to the north and on the hillside beyond is the Valley View Assisted Living facility and 7<sup>th</sup> Day Adventist Church. The adjacent land east of the study area, south of the John Day River, is occupied by Mills Building Supply, J D Rents, a warehouse, a UPS distribution site, storage units, and the Riverside Mobile Home Park. The study area includes a 30-acre City-owned property used for the current wastewater treatment facility. Other components of the planning area and adjacent uses include the area southeast of the confluence of the John Day River and Canyon Creek (within the study area currently identified as a city park and planned for future river recreation access). Existing land uses are illustrated in Figure 2.4.

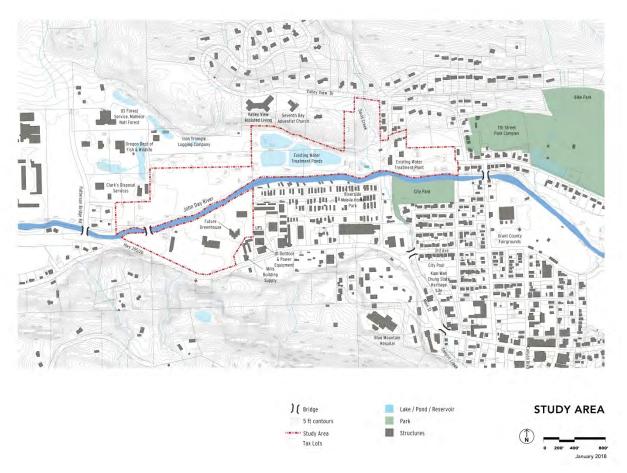


Figure 2.4. Study area with adjacent existing businesses and uses.

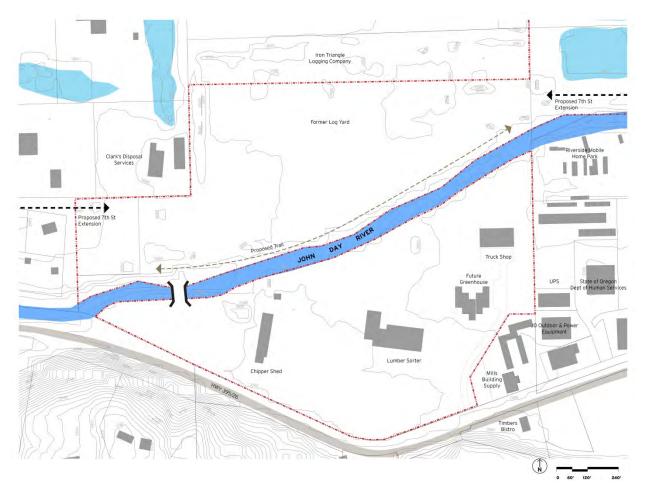
The study area has been vacant for 20 years, with no recent development on the site itself. A variety of uses are envisioned within the study area in the future, including relocation of the City's wastewater treatment plan, an aquatic and recreation center, an academic and research campus, hydroponic agriculture and greenhouses, a renovated public works shop, mixed-use commercial development, and a system of trails and open spaces including botanical gardens. There are parallel efforts to protect and enhance downtown John Day and this study will recognize the importance of providing complementary land uses that don't compete with the strengthening of the downtown core.

#### 2.9 Existing Structures

The study area includes three buildings that were in the past actively used as part of the Oregon Pine mill lumber production (see Figure 2.5: Oregon Pine site map). The three buildings are the Chipper Shed, the Planer Shed and Lumber Sorter, and the Truck Shop. The majority of the machinery and equipment formerly used by the mill have been sold and removed. The Chipper Shed and the Planer Shed are open timber and steel structures with concrete floors. Some siding remains. The structural condition of these two shed buildings is unknown. The Truck Shop is a simple 4-bay garage shed with concrete floors, insulation and corrugated metal siding which is being converted for use as a City of John Day Public Works facility. Elsewhere in the study area are structures and sheds associated with the existing wastewater treatment plant, which is aging and will be demolished. The 2018 Wastewater Facilities Plan Update (Anderson Perry) notes that:

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A portion of the existing components and treatment units were constructed during the original 1949 plant construction. Due to these units being approximately 70 years old, they are showing severe degradation and will not serve the long-term treatment needs of the City and need replacement. Additionally, most of the existing facilities were constructed as part of the 1978 construction project and have been in service for approximately 40 years. These 40-year-old components are at the end or have surpassed their expected service life and need rehabilitation and/or replacement.



#### Figure 2.5. Oregon Pine mill site.

#### 2.10 Parks and Open Space

The John Day-Canyon City Parks and Recreation District currently manages the ballfields at the 7<sup>th</sup> Street Parks Complex, the new adjacent mountain bike trail park and Gleason Pool, a facility which is 60 years old and open only 12 weeks per year. A 2017 survey of County residents indicated strong public support existed for a new aquatics center and more outdoor recreational opportunities. The Oregon Parks and Recreation Department owns about 1.5 acres at the Kam Wah Chung site, and a master plan was developed in 2009 and updated in 2012. Ultimately, the state wants to build a new interpretive facility at the Kam Wah Chung site to replace the current museum across the street. The plan calls for the state to acquire the three acres of city park land surrounding Kam Wah Chung in exchange for cash that could be used to leverage grants for a new city pool. A feasibility study by Counsilman-Hunsaker and Opsis Architects has been commissioned to study options for a new aquatics and recreation facility, with two potential sites, at 7<sup>th</sup> Street Complex or on the former Oregon Pine mill site.

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A 10-acre parcel between Valley View Drive and the river, including the Davis Creek ravine, has been acquired for open space uses related to the Innovation Gateway and could include a trail from Valley View Drive to an overlook with dramatic views of the city and mountain ranges to the south. The land could also be home to a future botanical garden. The newly-acquired land also includes 4 acres along the south side of river that could become a new City Park, served by the Canton Street extension. From this new park a trail will be extended along Canyon Creek to connect this new city park with a trail system to Kam Wah Chung. Currently, a rough discontinuous trail can be found in places along both sides of the John Day River, a result of informal efforts to access the river. This project will seek to formalize this trail alignment and include connections around the current wastewater evaporation ponds and to the proposed Davis Creek trail.

# 2.11 Smart Growth in John Day

The following is a list of 8 potential smart growth-oriented actions that should be considered as part of this study, and specifically relate to the challenges and opportunities in John Day. These refer broadly to the 10 Principles advocated by Smart Growth America (<u>https://smartgrowthamerica.org/our-vision/what-is-smart-growth/</u>) which are actions that inherently support sustainable development and the intelligent use of scarce public resources.

#### 1. Strengthen Downtown

John Day's core is a distinctive, compact mixed-use area, at the crossroads of Main Street and US 395, with hotels, restaurants and small businesses in aging, relatively narrow structures. The businesses benefit from the visibility to traffic at this crossroads. Given the challenges of surviving in a small market, future redevelopment on the Innovation Gateway site should strive to complement and support Downtown, not compete with it.

#### 2. Promote mixed-use, compact infill development

Any complementary new development on the Gateway site should strive to mix uses vertically or within the same structure to reduce the need for auto circulation and parking. The study area is a great opportunity for infill development on a 'brownfield' site within close proximity to existing urban services and with good potential for active transportation connections to the rest of John Day.

# 3. Create a range of housing options, including affordable, workforce housing

If housing is determined to be a feasible use in the study area, within the constraints of the floodplain, there should be opportunities for housing to support existing John Day residents with pricing that reflects local incomes.

#### 4. Foster a strong sense of place

The John Day region has a very distinctive identity as a rural 'frontier' town with a long history of self-reliance and connection to natural resources. New development should recognize and strengthen the region's physical and social character. It may be appropriate to establish architectural design standards that promote this sense of character.

#### 5. Capitalize on views and connections to John Day's natural assets

The John Day river flows through the study area and the City has long 'turned its back' to the river, an attitude which probably stems from the past degraded nature of the riverbanks due to dredge gold mining and subsequent use of the banks for lumber storage. The river can become a positive asset for the community and a focal point for new development and recreation that attracts visitors, employers and new residents. Views of the Strawberry Mountains and Aldrich Range are reminders of John Day's position within a remarkable context of natural beauty.

6. Build new complete streets

New street extensions in the study area should be designed for all potential uses, not just automobiles and should include safe walking and biking facilities, on-street parking, use by people with mobility limitations, and allow for future transit service.

# 7. Encourage walking and biking

In addition to the design of complete streets, new trails should be designed throughout the study area to link destinations and connect to the existing city trail system.

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8. Coordinate City and County actions and foster collaboration and partnerships A variety of local, regional and state organizations can partner with the City to help ensure implementation of the JDIG area. These include, among others, the John Day-Canyon City Parks and Recreation District, Oregon Parks and Recreation Department, Grant County, the Oregon Department of Transportation, the Oregon Department of Land Conservation and Development, Oregon State University, the Grant County School District, Business Oregon, local employers and others.

#### 2.12 Local energy development

The John Day area has limited potential for traditional energy development. According to BLM resource mapping, there is no potential for oil and gas development. DOGAMI mapping suggests that there are untapped geothermal resources in the region. The City's electrical grid is served by OTEC, using Bonneville Power Administration energy from Columbia and Snake River dams and increasingly, wind power. Local winds are not strong enough to support wind energy generation. Solar power has potential, with over 200 sunny days in the City but generation will depend on the costs of installing infrastructure. The most promising use of locally-generated energy is the production of biomass by Ochoco Lumber's Malheur mill, which is replacing natural gas at several large local facilities such as the Blue Mountain Hospital. This biomass is created from the harvest of smaller trees in public forests, which helps reduce forest fire risk while supporting local mill employment.

#### 2.13 Cultural Resources

A cultural resources assessment will be required for the site to demonstrate conformance with state and federal cultural resources requirements. At the federal level, this includes review under Section 106 of the National Historic Preservation Act. This requirement is triggered via a federal nexus, such as projects on federal land, with federal funding, or that require federal licensing or permitting. For the JDG Project, this would most likely be triggered through the application for the Section 404 (dredge and fill) permit through the US Army Corps of Engineers. The State of Oregon also requires that projects on public land, such as the City of John Day property, comply with Oregon Revised Statute 358.653, which pertains to cultural resources.

Cultural resources evaluation would be handled through the Oregon State Historic Preservation Office (SHPO). The survey would require a "below-ground" (i.e. archeological) survey for resources that have been abandoned for over 50 (federal) or 75 (state) years or more; and may also require an "above-ground" (e.g. for historic structures) survey for resources that are either still in use or have only been abandoned for less than 50 years and that may have historic significance. The cultural resources survey would need to be performed by professionals that meet minimum qualifications for archeology and historic preservation, as defined by SHPO. The evaluation first includes researching background information on what resources are potentially located at the site, and then follow-up field surveys.

The below-ground survey would require an archeological permit through SHPO to perform the survey because it would include shovel probes for resources, and if resources are found, the permit would specify the protocol required for their collection and curation. A permit application would need to be prepared, including providing an Area of Potential Effects (APE) map. The permit normally takes up to 33 days to complete the review process. Landowners, tribes, the local planning department, and the University of Oregon Museum of Natural and Cultural History are provided the opportunity to review the application. Once the permit is acquired, the survey can proceed. If an archeological site is found during the survey, then discussions will occur with SHPO on how to proceed. Ideally, archeological sites can be avoided. If avoidance is not possible, then a follow-up evaluation, issued under a separate permit, may be required to determine the significance of the site and any mitigation or documentation measures that would be necessary.

An initial conversation with a professional archaeologist (personal communication with Zach Windler, Dudek Inc, Dec 21, 2018) indicates that no below-ground survey or identification of archeological

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resources appear to have yet occurred in the project footprint (as outlined in the June 5, 2018 Concept Plan). Based on the heavy impact from the entire site from dredge-mining during the 1930s, it is unlikely that below-ground resources would be found or impacted by project activities.

An above-ground survey and evaluation may also be required for the project, the scope of which would be determined in consultation with SHPO. This evaluation also includes background research as well as possibly field surveys. This evaluation could include areas beyond the APE map and that are outside but nearby the project footprint; the spatial extent is typically determined via consultation with SHPO. This evaluation is to ensure that components of the project would not detract (e.g. visually or otherwise) from eligible or listed historic buildings or other built features. For eligible buildings within the agreed-upon area, the evaluation would identify the type of historic value, the potential for the project to impact it, and if necessary, recommendations for how to configure the project or mitigate accordingly. An initial conversation with a professional archeologist (personal communication with Zach Windler, Dudek Inc. Dec 21, 2018) indicates that no above-ground survey or identification of historic structures appear to have yet occurred in the project footprint. However, there are several eligible and listed buildings in town. Based on a preliminary review of the National Register Database, there are a few buildings in John Day listed on the National Register of Historic Places, including the Kam Wah Chung Building, the Advent Christian Church (downtown), and the Supervisor's House and Warehouse just to the west of town. Although none of these are within the actual project footprint of the project, the Kam Wah Chung Company Building is the closest and is anticipated to be linked to the trail network associated with the project. This building is also designated a National Historic Landmark and is an important cultural component to the town, especially the associated Kam Wah Chung museum.

In general, cultural resources evaluation will be an important permitting consideration for this project, as review and consultation times can affect project implementation timing; and there are budget considerations as well. It is recommended to initiate consultation with SHPO as soon as possible and hire a professional archeologist to acquire the archeological permit and begin performing initial background assessments and site surveys. We also recommend consulting with the Warm Springs Tribe early on in the process. They may have particular interest (and likely support) of river habitat restoration work that would provide benefits to salmon and steelhead.

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# **SECTION 3**

# John Day River Conditions and Opportunities

#### 3.1 Introduction and Background on River Conditions

Approximately one mile of the John Day River flows through the project area. Canyon Creek, which is a significant tributary to the upper John Day, flows north through town and enters the mainstem within the project area. A smaller tributary, Davis Creek, flows from the north and enters the mainstem just across from the Canyon Creek confluence. The river and tributaries provide important aquatic and riparian habitats for fish and wildlife species, and also potential recreational benefits for people. The river, riparian zone, and adjacent floodplain areas have been heavily impacted by past and current land uses, which have substantially changed conditions compared to those that existed historically. The Innovation Gateway Project provides an opportunity to improve some of the river-related functions and features, including fish habitat, aesthetics, and recreation/access.

This portion of the upper John Day River is designated Critical Habitat for Middle Columbia Steelhead, which are listed as Threatened under the Endangered Species Act. The reach that flows through the project area is used for steelhead "rearing/migration" (*Carmichael et al. 2010*). Canyon Creek is also designated as Critical Habitat, with use type listed as "spawning/rearing". Steelhead habitat in the upper mainstem John Day River was evaluated as part of the Middle Columbia Steelhead Recovery Plan (*Carmichael et al. 2010*). The plan lists the following as "Major Limiting Factors" for the Upper Mainstem John Day Population: degraded floodplain and channel structure (loss of cover, pools, LWD, overall habitat diversity, connectivity); water quality (temperature); altered sediment routing; degraded riparian communities; altered hydrology; and impaired fish passage. The listed "Threats" to habitat are agricultural practices, livestock grazing, removal of large trees from the riparian corridor, wetland draining and conversion, stream channelization and diking, mining and dredging, and irrigation withdrawals. These limiting factors and threats have all occurred to some degree within the study area, either in the past and/or currently.

This reach of river, and the surrounding valley bottom, was especially impacted by dredge mining for gold and silver in the early 1900s. This was bucketline dredge mining, which is extremely damaging to the river, riparian zones, and floodplains. It involved a barge-mounted dredge that sits within a pond excavated in the floodplain. The dredge was winched back and forth across the valley bottom, excavating, sifting, and depositing material as it went. This area is within an area known as the "Canyon District" mining area (*Brooks and Ramp 1968*). Prior to 1916, most of the mining occurred further up Canyon Creek near Canyon City, which was one of the earliest mining camps in eastern Oregon. In 1916, a dredge was installed near the town of John Day by the Empire Dredge Company (Brooks and Ramp 1968). The dredge operated almost continuously until 1929, when it was moved up to Prairie City. New dredging began again in 1935 and in 1937 and ran until 1942. Heavy dredge impacts can be seen in the earliest aerial photos (Figure 3.1) and in the historic site photo from 1939 (Figure 3.2). The piles of material seen in the photos are the dredge spoils placed by the barge-mounted dredge as it moved back and forth across the valley floor dredging and then re-depositing material as it went.

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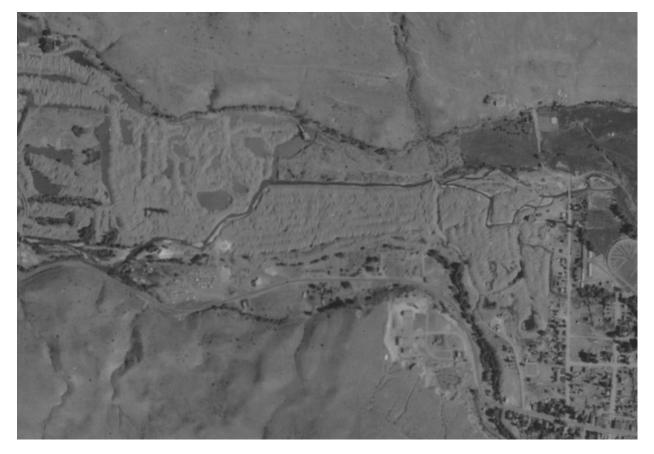


Figure 3.1. 1939 aerial photo showing ditched stream channel and floodplain dredge mining impacts throughout the study area.

As can be seen in the photos, the dredge mining severely damaged river, riparian, and floodplain habitats. The river was relocated from its original position and essentially left in a ditch through the project area. Dredge mining not only directly damages habitat, but it also changes the way that the river and floodplain function. The river was straightened, and so no longer has the meandering pattern that helps maintain pools and riffles in natural rivers. Much of the habitat is now comprised of straightened sections with very little variation in habitat types that are necessary to support multiple life stages of salmonids and other aquatic species. The connectivity of the river with its floodplain was also impaired. The river was left in a ditched condition, in a deep channel. The river now inundates its floodplain much less frequently than it would have prior to dredging impacts. Natural rivers of this type typically inundate their floodplains at least every year or two, and sometimes more frequently. Floodplain inundation is important in that it provides access to off-channel slow water rearing habitat for fish during high flows, and results in dynamic channel changes that are necessary for creating and maintaining habitat over time. Reducing floodplain inundation results in a much less complex channel with poorer habitat conditions for fish. Furthermore, reducing floodplain inundation can also increase flow velocities, potentially increasing flood levels and associated damage to human infrastructure in downstream areas.

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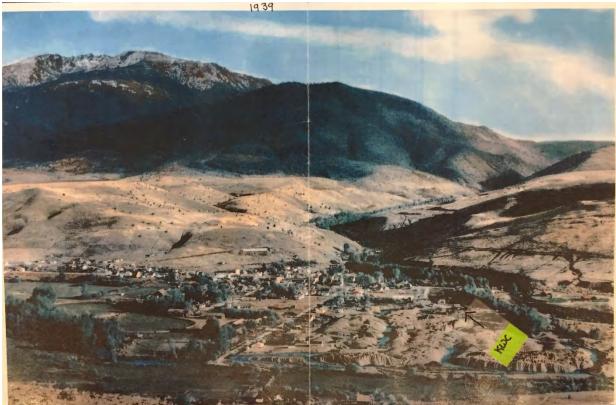


Figure 3.2. 1939 photograph of the site looking south towards town and up Canyon Creek. This is a photo of a hard copy photograph located at the Kam Wah Chung museum. The note with "KWC" is the current location of Kam Wah Chung.

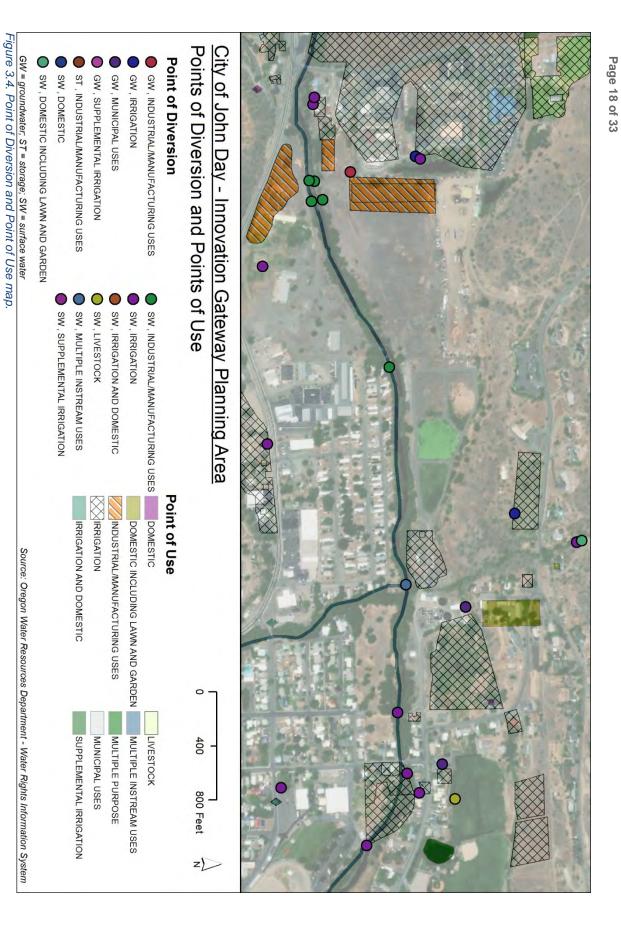
Following dredge mining, the dredge spoils were gradually flattened as the City of John Day, the Oregon Pine Mill, and other commercial, industrial, and residential uses expanded into the formerly dredge-mined areas; areas that were historically occupied by river channels, vegetated riparian zones, and extensive lowland floodplain areas (Figure 3.3). Water withdrawals were also put in place, primarily to support agricultural uses. There is currently one major diversion within the project area and a diversion upstream of the project area that reduces instream flows in portions of the study area. A Point-of-Diversion and Point of Use map based on data from the Oregon Water Resources Department (WRD) is included in Figure 3.4.

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Figure 3.3. Photo of historical photograph displayed at The Corner Cup coffeeshop, John Day, OR.

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Contemporary river conditions include an incised (i.e. deepened) river channel with a lack of habitat complexity (Figure 3.5). Many of the components that are important for fish habitat are missing. This includes deep pools with cover, instream large wood, and vegetated riparian zones. A lack of shading from streamside vegetation results in higher stream temperatures, which negatively impact steelhead. The riverbanks have also been treated with bank armoring in numerous locations. This riprap and concrete armoring reduces channel complexity, limits the ability of vegetation to establish, and prevents the river from being able to migrate (i.e. move around as natural rivers do) and develop a natural meandering pattern. Overall, aquatic habitat has been significantly impaired in this once dynamic and complex reach of river.



Figure 3.5. Typical section of the John Day River at the site. The channel has been straightened, there is only a narrow forested riparian buffer, and there is a lack of pools, riffles, instream large wood, and overall habitat complexity.

At the downstream end of the site, there is a low-head boulder and concrete dam that diverts flow into a buried pipe that feeds an irrigation canal. This structure, and the associated bank armoring, fixes the river in place and limits the growth of a forested riparian zone. Although adult steelhead can ascend the dam, the structure likely affects passage for juvenile steelhead and other fish.

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Figure 3.6. Low head boulder and concrete weir and irrigation diversion at downstream end of site.

This section of the John Day, as well as lower Canyon Creek, also have impaired water quality conditions. The reach of the John Day that flows through the project area is on the State's list of impaired waterbodies (OR DEQ 2012). This list includes impaired streams that fail to meet basic water quality criteria. It is listed because it does not meet standards for Dissolved Oxygen, Biological Criteria, Temperature, Fecal Coliform, and E. Coli. The reach is also listed as impaired due to Flow Modifications. Lower Canyon Creek is on the list for Biological Criteria and Temperature.

# 3.2 Opportunities for River Restoration

The Innovation Gateway Project offers an opportunity to integrate river habitat improvements with other components of the waterfront redevelopment plan. These improvements could be mutually beneficial to fish/wildlife and human recreational uses. From a habitat perspective, actions could improve components of the natural river system that will provide important benefits to ESA-listed steelhead and other aquatic, avian, and terrestrial species. From a human perspective, actions could benefit recreational uses, including wildlife/bird watching, swimming and floating, and aesthetics. There have been many river habitat improvement projects in the John Day Basin and throughout the region that can be used to help guide river improvement efforts, including other projects in previously dredge-mined valleys. However, actions here will be unique because of the location adjacent to the City of John Day urban area and also the integration with the larger Innovation Gateway project.

River habitat improvement can be thought of as two interrelated approaches: 1) long term process-based approaches, and 2) short term habitat-based approaches. Both of these are important with any habitat

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improvement effort, and should be integrated to achieve multiple objectives over multiple time scales. The first one restores natural ecosystem processes that will help the system to create and maintain habitat on its own over the long term, essentially improving how the river functions. The second provides immediate habitat benefits that will address critical short-term needs. These two approaches are described in more detail below.

# **3.3 Restoring River Functions**

This includes restoring ecosystem processes that will help to create and maintain habitat conditions over the long term. This includes restoring river, floodplain, and riparian functions that are compatible with the site and with the other goals of the Innovation Gateway Project. Because of the past impacts, on-going uses, and future development plans, there is a limit to the degree to which these functions can be reasonably improved, but they should nevertheless be considered, and there may be opportunities for at least partial restoration and for improving human recreational uses as part of it. There are 3 primary considerations: 1) Reconnect portions of the channel migration zone (CMZ) – This could occur by removing streambank armoring in select locations where the river can be allowed to move around and recreate a meandering pattern, which is important for the long-term creation of new habitats and for other related functions. The CMZ will obviously be limited in width and will be much narrower than under historical conditions. Set-back protections will likely be necessary to protect existing or proposed infrastructure; 2) Improve floodplain connectivity – This will most likely be accomplished by lowering the floodplain in certain areas and planning uses for those areas to be compatible with seasonal inundation; this may include trails, parks, or natural areas, and could incorporate creation of bird and wildlife habitat. Improving floodplain connection benefits aquatic habitat through creation of new habitats in the floodplain over time (e.g. off-channel wetland habitat) and it also provides area for velocity refuge for fish during floods. There can also be a reduction in downstream flood impacts; and 3) Re-establish native vegetation conditions - This will include planting streambanks, riparian zones, and floodplain areas with native vegetation appropriate for site conditions. Over time, native vegetation communities will provide bank stability; stream shade to lower temperatures: habitat and food for fish and wildlife; and a future source of large wood (i.e. important fish habitat) to the channel.

# 3.4 Direct Habitat Improvements

Direct habitat improvements are performed to make sure that important habitat types are provided for sensitive species over the short-term. These include addressing spawning, rearing, and migration conditions for steelhead, as well as addressing habitat for other species. Habitat improvement elements may also be configured to benefit human uses.

First, the type and distribution of habitat types can be improved. If space allows, this could be accomplished by increasing the sinuosity of the main channel. This would involve creating more meanders (e.g. curves) in the river. There are a few locations where this might be accomplished at the site and where it could be integrated with other components of the Innovation Gateway Project. This action would help to create pool and riffle sequences that are used by both juvenile and adult steelhead. Creating meanders could also be combined with lowering/reconnecting the floodplain (described previously) at the inside of the meander bends – this action would also allow for some narrowing and raising of the channel, which would increase ease of public access and possibly make the river more floatable by tubers/rafters during the summer. Pools could be further enhanced by deepening them to improve juvenile salmonid rearing habitat, which would also enhance swimming opportunities by humans.

Side channels and alcoves could be created. These are features that are connected to the mainstem at their upstream and downstream ends (side channels) or just at a single location (alcoves). Off-channel areas provide important rearing, feeding, and flood refuge habitat for juvenile salmonids; they also can provide high quality wetland habitat for numerous other species. Portions of the existing wastewater treatment lagoons at the site could potentially be converted to floodplain wetlands or connected side

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channel or alcove habitat; and Davis Creek could possibly be routed into this floodplain wetland complex to provide a source of cool water and to increase the amount of available tributary habitat.

Large wood could also be added to the stream. Large wood in rivers is a critical component of salmonid habitat in Pacific Northwest streams. It provides cover from predators, helps to retain spawning gravels, creates velocity shadows (e.g. eddies) for fish to rest, and increases the overall complexity of the stream system, which is good for fish and other aquatic species. Large wood could be added in various locations to serve multiple purposes. This includes within pools to provide cover and complexity, and along stream margins, especially where complexity can be added to or replace existing bank armoring. Wood can also be used in select areas to trap and sort spawning gravels, particularly in Canyon Creek, which is likely to be used more heavily for steelhead spawning than the mainstem. Wood can also be used to increase the stability of rapidly eroding streambanks that are providing inputs of harmful fine sediment into the channel.

There may be opportunities to improve fish passage and habitat at the diversion structure at the downstream end of the site. Assuming this water right will remain, this point of diversion will need to remain operable. This low-head dam structure could be replaced with a constructed riffle (aka "roughened channel") in order to maintain the grade of the streambed to maintain the diversion; but this alteration would provide a more natural looking and natural functioning streambed that is better for fish passage. The condition of screening, if any, and the potential for fish entrainment into the irrigation canal should be investigated, and addressed if necessary as part of the project.

# 3.6 Project Examples

A few project examples are provided below that demonstrate approaches that could be employed at the City of John Day site. The first is the Middle Fork John Day Oxbow Project, located approximately 21 miles northeast of John Day, OR on the Middle Fork John Day River. This project was led by the Warm Springs Tribe, with design assistance from the US Bureau of Reclamation and Inter-Fluve. Similar to the City of John Day site, the valley bottom had been dredge-mined in the 1930s and the stream had been relocated into a straightened channel. The project re-created a meandering pattern (Figure 3.7), reconnected the floodplain, created off-channel habitat features, and installed large wood for habitat. This project is different in that there was very little infrastructure that needed protection from flooding, but it demonstrates some of the types of actions that could be implemented at the Innovation Gateway site.

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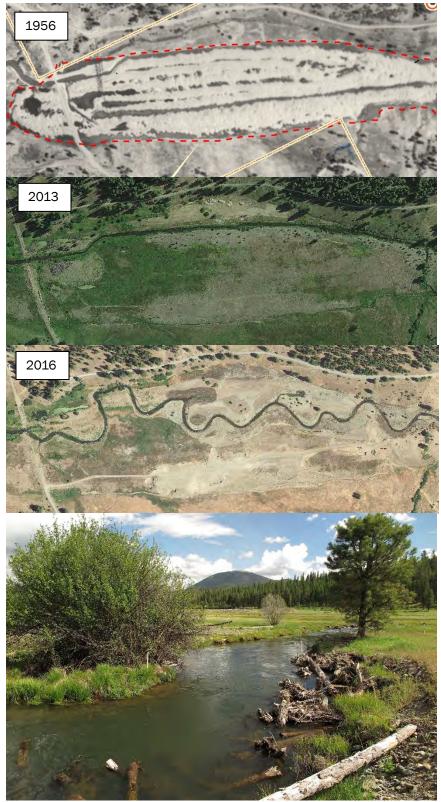


Figure 3.7. Aerial views of the Middle Fork John Day Oxbow Project showing dredge mine tailings (1956), pre-project conditions (2013), post-project conditions (2016), and a created pool with large wood (2015 photo).

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Another example is the Johnson Creek Schweitzer Natural Area Project in Portland, OR. This project, led by the City of Portland and designed by Inter-Fluve, is located adjacent to Powell Butte in suburban Portland along the Springwater Corridor Trail. In the 1930s, the stream was straightened, ditched, and lined with stone armoring. The project re-created a meandering pattern, created pools and riffles, added large wood for habitat, and created off-channel alcoves, wetlands, and side channels. The project also included extensive planting of native vegetation. The project also included extensive excavation of material to lower and reconnect the floodplain, which was not only good for habitat but also provided 108 acre-feet of additional flood storage to lessen downstream nuisance flooding in residential areas. This approach to floodplain reconnection could potentially be used at the City of John Day site to accomplish multiple ecological and human objectives.



Figure 3.4. Aerial photos of the Johnson Creek Project, pre-project (left, 2006) and post-project (right, 2014).

Inter-Fluve has worked with the Minnehaha Creek Watershed District and various partners to perform restoration design work on Minnehaha Creek in a highly urbanized area of Minneapolis, MN (Figure 9). Similar to the previous project examples, this project re-meandered and added habitat enhancement features to a formerly straightened and impaired stream. However, this site is more urban than the previous examples, and therefore had more constraints, similar to the situation with the City of John Day project. There was significant nearby infrastructure to work around, including residential and commercial development. It was also highly integrated with recreational uses, including a pedestrian/bike trail, a new city park, and river recreational floating during the summer; all of which will be encountered with the City of John Day effort.

A final example worth considering is the success of the City of Pagosa Springs, CO in re-connecting the small city of 1700 people to its San Juan riverfront, creating a new recreational destination for fishing and floating from a formerly degraded and hidden channel. A city that was oriented solely to timber production was able to build on a growing attractiveness as a second-home market 280 miles from the nearest large city, Denver and attract new employment and infill development, while marketing its riverfront to recreational tourists and fishing enthusiasts.

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Figure 3.9. Minnehaha Creek in Minneapolis, MN. Pre-project (top), past-project (middle), and a photo of the pedestrian boardwalk trail along a restored section of Minnehaha Creek.

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# 3.7 Restoration Design Considerations

The considerations described in Sections 3.3 and 3.4 can be used as a basis to develop more detailed river improvement objectives for the site. In order to develop a plan for river improvements, more detailed analysis will be required. This will include various types of engineering, biologic, and geomorphology analyses. One of the primary requirements for design will be the collection of topographic and bathymetric survey data and development of a hydraulic model for the site. The survey data will be used to run the hydraulic model. Combined with proposed conditions, the hydraulic model will be used to evaluate how potential restoration actions would affect habitat as well as flooding and erosion that could affect human infrastructure. The model will be a critical design tool and will be used in an iterative fashion to explore the effects of various alternatives. The survey would also be used to develop a grading plan for the design. We recommend performing an alternatives analysis that evaluates multiple potential approaches. For this effort, an alternatives analysis would provide an explicit way to evaluate project options against potential benefits, constraints, and compatibility with other components of the Innovation Gateway Project.

The plan for the river should be viewed as a core component of the Innovation Gateway Project, especially since other features associated with the plan may be affected by what happens with the river. For example, creating new meanders could affect the location of other project features, such as trails or infrastructure; and changes to the river depth and width could affect flooding conditions in nearby areas. For these reasons, the plan for the river should be developed as early as possible. The cost of design, construction, and the timing for permitting are also important considerations. Costs can vary considerably depending on the final approach, and could range from approximately \$200,000 for very simple habitat improvements (e.g. adding large wood to the channel) to over \$2,000,000 for creating new meanders, side-channels, and improving floodplain connectivity. Engineering design costs typically range from 15-30% (or more for very complex projects) of construction costs for river restoration projects.

# 3.8 Environmental Permitting

There are several environmental permits that are likely to be required for the project. These are described in the subsections that follow. Environmental permitting can be a long and potentially costly process and it is therefore recommended to begin consultations with permit agency staff early in the planning process to make sure that the project is designed with the permitting requirements in mind.

Permitting agencies typically prefer that projects are permitted as a whole, rather than breaking up into components. However, for large complex efforts where there are very distinct pieces, or where the project will be phased over several years, it may be possible to permit separate components. Separating out the permitting could be considered with respect to permitting the river habitat improvement work versus other components of the Innovation Gateway Plan. This is because, oftentimes, habitat restoration work will fall under more streamlined permitting processes. If the restoration work is to happen in the early phases of the effort, this work could be permitted first and implementation could potentially move forward on a more expedited schedule.

#### 3.8.1 US Army Corps of Engineers 404 Permit

A US Army Corps of Engineers 404 Permit is required when there will be removal or placement of material within "waters of the US". This includes wetlands, rivers, and other waterways that are present at the John Day project area. Where there is the potential for wetlands, a wetland delineation is required, which will be the case for the John Day site. Information on wetlands delineation is included below in Section 0. It will also be necessary to delineate the boundaries of other waterways, including the John Day River and Canyon Creek. This is performed by conducting a delineation of the Ordinary High Water (OHW) line on either side of the stream or waterbody. The OHW defines the extent of jurisdictional waterbodies that are subject to the requirements of the 404 permit.

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The 404 permit application is through the Joint Permit Application (JPA) process through the Oregon Department of State Lands (DSL). The USACE will issue either an Individual permit or a Nationwide permit for the project. Nationwide permits are programmatic (i.e. more streamlined) permits for certain types of pre-approved activities, such as river habitat restoration. For the JDG project at a whole, falling within a Nationwide Project may not be possible, but if the river habitat work is split out as a separate effort, then the project may be able to fall under Nationwide Permit 27, which covers stream habitat restoration work. Falling under Nationwide 27 also helps to streamline the 401 Certification process (described below). A qualified archaeologist should review the Gateway Study Area for the presence of known cultural resources and plan on completing a survey of the project during development.

#### 3.8.2 Oregon Removal-Fill Permit

Similar to the 404 permit, a Removal-Fill permit is required from Oregon DSL for work within waters of the state, which includes the river and wetland areas at the site. The wetlands and OHW delineations described above for the 404 permit will also satisfy the requirements of the Removal-Fill permit, and the application for a Removal-Fill permit is also through the JPA process.

#### 3.8.3 401 Certification

The Oregon Department of Environmental Quality (DEQ) will review the project for conformance with the Clean Water Act's Section 401 requirements related to water quality impacts, and if required, will issue a 401 Certification. As with the 404 and Removal-Fill permit, this also occurs through the JPA process. If the activities being permitted go through a USACE Nationwide 27 permit, then 401 certification is not required as long as the project involves less than ½ acre of fill in waters of the US. There is also a new (pending) DSL requirement to complete a Stream Function Assessment Method (SFAM) analysis of waterways as part of their permitting process.

#### 3.8.4 Construction Stormwater Permit

The City of John Day Development Code regulates storm drainage (Chapter 5-3.4.040). The code states that

"The rate of stormwater runoff leaving a development site during and after development (post-development) shall not exceed the rate of stormwater runoff leaving the site before development (pre-development). The Public Works Director may require an applicant for development to provide a storm drainage analysis prepared by a qualified professional engineer registered in the State of Oregon to examine pre- and post-development stormwater runoff conditions and any required mitigation."

The project will also likely require a construction stormwater permit as part of the National Pollution Discharge Elimination System (NPDES). This would apply to work that is outside the river and riparian zone but that may affect water quality via stormwater runoff. This will likely be a 1200-C permit, issued through the Oregon DEQ. The application requires a description of the project and an Erosion and Sediment Control Plan (ESCP) for how runoff will be addressed and managed during construction to protect waters of the state.

#### 3.9 Floodplain Regulations

A study was recently completed to revise the existing FEMA flood maps for the John Day River and lower Canyon Creek within the study area. This was performed by the Corps of Engineers in support of the City of John Day, Canyon City, and Grant County. The flood mapping is used by the City to administer the City's code related to development in flood hazard areas and to manage their responsibilities under the National Flood Insurance Program. The study used updated survey data and hydraulic modeling to adjust the boundaries of the Special Flood Hazard Area (SFHA), which is the area that is subject to flooding

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during the Base Flood, also known as the 100-year flood, or the flood that has a one-percent chance of occurring every year. The SFHA includes two sub-units, the regulatory Floodway and the Floodway Fringe. The Floodway is defined as "The channel of a river or other watercourse and the adjacent land areas that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one foot" (City of John Day Code of Ordinances, Title 4, Chapter 2). This is essentially the portion of the river and adjacent floodplain where floodwaters are deepest and swiftest and where features would be in the greatest danger during a flood. The Floodway Fringe is the remaining area within the SFHA that is typically in less danger during floods and where reasonable development can occur so long as certain measures are taken.

Information on the revised flood mapping is shown in Figure 3.10, which shows the changes from the old mapping. On this map, the old SFHA is referred to as the "effective" area and the Floodway Fringe is denoted as Zone AE. A few significant changes can be seen in this map as it relates to the project area. First is the removal from the SFHA of the mobile home park neighborhood just north of the supermarket. There are also some significant changes to the mapping at the west end of the study area, with a reduction of area in the SFHA on the north side of the river at the former Pine Mill log yard site, and then a substantial increase in area within the SFHA further to the north and west, including privately owned land as well as the US Forest Service, OR Dept of Fish and Wildlife, and OR Dept of Forestry compounds. Other areas have seen minor additions or reductions of land within the SFHA. Many of the proposed features associated with the Innovation Gateway Project are within the Floodway Fringe, and some may be within the Floodway.

Typical of most jurisdictions throughout the country, development activities are allowed within the SFHA, but activities that occur within the SFHA must adhere to the rules specified for Flood Hazard Areas in the City's Code of Ordinances. Following these rules also allows for the City to participate in the National Flood Insurance Program, which provides federally-backed flood insurance for residents in participating communities. For development projects within the SFHA, the City requires a Development Permit that shows the elevation of structures in relation to the Base Flood Elevation (BFE), floodproofing methods and elevations, and the extent of any alteration to a watercourse. This information must be provided by a registered professional engineer.

There is also a provision in the City Code that prohibits any encroachments within the regulatory Floodway that results in any increase in flood levels during the occurrence of the base flood discharge. This is commonly referred to as a "no-rise" condition. This no-rise condition will apply to any of the proposed features associated with the Innovation Gateway Project that are located within the Floodway. which could include roadways, trails, and portions of the new park. These features are likely to be low profile and to not provide encroachments that cause a rise in the BFE, but this will nevertheless be an important consideration. Any changes to the river, including potential restoration work that has been described previously, will also need to consider flood rise. For river restoration projects, FEMA Region 10 has developed a "Policy on Fish Enhancement Structures in the Floodway" (FEMA 2009). This policy recognizes that a strict interpretation of the no-rise standard could incur costly analyses for restoration projects that are providing an overall benefit to the river and to aquatic species, and therefore allows for floodplain managers to rely on hydraulic or hydrologic professionals to provide a less formal "feasibility analysis and certification that the project was designed to keep any rise in 100-year flood levels as close to zero as practically possible and that no structures would be impacted by a potential rise" (FEMA 2009). For more significant alterations that go beyond what this policy covers, then a more formal no-rise analysis may be required for the river restoration work. This could even potentially include a revision to the flood maps through the Letter of Map Revision (LOMR) process. As mentioned previously, hydraulic modeling will be necessary for design of river restoration features. This model can also be used in the evaluation of the effects of proposed alternatives on flood levels.

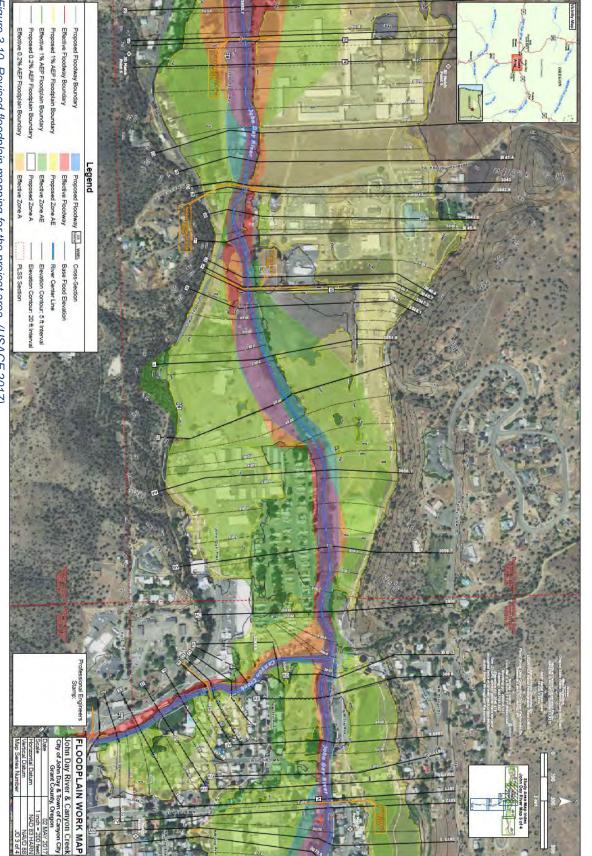


Figure 3.10. Revised floodplain mapping for the project area. (USACE 2017)

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#### 3.10 Wetlands

A wetland delineation and assessment will be necessary for the site since river modifications and other project features have the potential to affect wetlands. A wetlands field delineation will need to be performed and a report prepared that satisfies US Army Corps of Engineering and Oregon DSL requirements. This would include completion of the OR Wetland Rapid Assessment Protocol (ORWAP) forms. The delineation and assessment will need to be performed by someone with approved qualifications, ideally a Professional Wetlands Scientist. The wetlands delineation and assessment work would likely cost \$15,000-\$20,000 for the site. The site is large (80-90 acres), but there is easy access and many areas that are unlikely to have potential for wetlands. These costs do not include additional field time to obtain regulatory concurrence for the assessment results, which is unlikely to be required, but is a possibility.

If the project has the potential to impact wetlands, wetland mitigation may be required. If needed, this would ideally occur on-site to reduce costs, but could occur off-site if necessary. A wetland mitigation plan would be required, which varies in level of effort depending on the amount, type, and location of mitigation that is required.

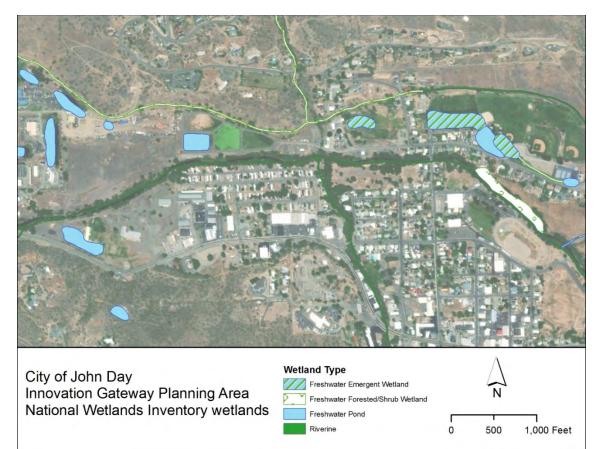


Figure 3.11. Location of wetlands from the National Wetlands Inventory. These represent a coarse-scale estimate of wetlands at the site. The actual location and type of wetlands will need to be identified through performance of a wetlands delineation.

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#### 3.11 Endangered Species Act Compliance

The project will need to be reviewed with respect to the impact on ESA-listed species. For the river-related work, this will primarily apply to Middle Columbia River Steelhead, which are listed as Threatened under the ESA. This project will likely require ESA consultation with NOAA Fisheries (NOAA) under Section 7 of the ESA, which applies to projects either on federal land, funded with federal dollars, or that go through federal permitting. The USACE 404 permit is the likely nexus for Section 7 consultation for this project, thus the USACE will consult with NOAA regarding the impact of the project on ESA-listed steelhead. ESA consultation typically results in the issuance of a Biological Opinion (BO) from NOAA, which specifies whether the project can proceed as planned and any specific measures that need to be taken for protection of ESA-listed species. For river restoration projects, activities can typically follow a programmatic consultation, which is a more streamlined review process for pre-approved restoration activities. In Oregon, the USACE has a pre-approved BO for restoration, termed the Standard Local Operating Procedures for Endangered Species (SLOPES; USACE 2013). There may also be other potential programmatic consultation pathways available depending on funding sources and other factors.

If the project cannot fall within a programmatic BO, then informal or formal consultation will be required and a BO may need to be prepared. Early communication with the USACE and NOAA Fisheries staff is recommended to determine what will be required with respect to ESA consultation, especially since preparation of a BO and moving through the consultation process could affect costs and construction timing.

#### 3.12 Environmental Contamination

The former Oregon Pine Mill site was evaluated and treated for contaminated soils multiple times since 2009 and has been given a determination of No Further Action (NFA) needed (letter from OR DEQ to Nicholas Green, dated February 14, 2018). This site was originally identified by OR DEQ as potentially having contaminated soils, leading to the investigations, treatments, and eventual NFA determination. We are unaware of other sites within the Innovation Gateway plan area that would be expected to have contamination. If further investigations suggest the presence of contaminated soils, or if there are signs of potential contamination found during site surveys, then additional Phase 1 assessment may be required.

#### 3.13 Water Withdrawals

Any actions proposed for the site must consider potential effects on existing water rights and points of diversion. The location of points of diversion and use in the study area are included in Figure 3.4. The primary diversion point in the study area is near the downstream end of the site near the former Pine Mill site where there is a diversion dam and intake on river-left that feeds an irrigation ditch with multiple downstream users. This is described previously in Section 3.1.

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# Section 4 Opportunities and Challenges

#### 4.1 Sustainable and Interpretive Features

There is an inherently sustainable nature to smart growth-oriented planning efforts described earlier in this memo, which aim to focus urban development on efficientlyserved infill sites and encourage walking to reduce carbon emissions from private auto use and improve public health. The strategy of restoring the heavily degraded John Day River to promote habitat and floodplain protection is also inherently sustainable, enhancing citizen's daily access to nature. Perhaps the best illustration of sustainable innovation can be found in the City's strategy to build a new membrane bioreactor, outside the floodplain, pipe the treated water to greenhouses and build local produce that is then sold in John Day, creating a positive internal support loop for the City.

Educating visitors and residents on these sustainable features and on aspects of John Day's natural and cultural history is an excellent way of strengthening the City's sense of place and encouraging people to stay in the study area longer. Specific interpretive opportunities include:

- Riverbank restoration
- History of gold dredge mining and effects on the river
- Oregon Pine Mill history
- Native American history
- The biota (birds, animals, plants) of the John Day River
- Innovative wastewater treatment and greenhouse development

#### 4.2 Opportunities & Challenges

As the consulting team begins to consider potential alternatives for change at the Gateway site, the following is a list of Opportunities and Challenges that will drive and guide concept plan development. This list will be refined in Technical Memo #5:

#### **Opportunities:**

- Building on John Day's existing identity, values and resources
- Access to the John Day River
- A sustainable local economy, based on competitive advantages of John Day's location and resources
- Improved aesthetics for this western gateway to John Day
- Creating room for natural floodwater storage, reducing flood impacts
- Health benefits from improved access to nature and opportunities for active recreation
- Utility and public infrastructure upgrades
- Encouraging community connection and interaction

#### Challenges:

- Costs of improvements and limited public resources
- State and National competition for grant funding
- Maintaining local community support through time needed to realize improvements
- Creating a careful phasing strategy to ensure efficiency in new improvements
- Market feasibility of supporting envisioned hotel and public market uses
- The John Day River floodplain and restraints on development

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https://www.nwp.usace.army.mil/Portals/24/docs/regulatory/SLOPES\_V\_restoration.p

То:	City of John	Aaron Lieuallen, Day -Smith, ODOT	Topic:	Technical Memo #4: Vision Statement and Guiding Principles	Date:	01/18/2019
From:	Ken Pirie		Project:	John Day Innovation Gateway	Project #:	P3465
Distrib	ution:					
Basec	amp	John Dov	Innov	ation Cotoway Area Blan		

# John Day Innovation Gateway Area Plan

**Technical Memo #4: Vision Statement and Guiding Principles** The following brief memorandum provides a proposed vision statement and guiding principles for the project, and includes a set of evaluation criteria with which to assess concept plans for the Gateway project.

#### **Proposed Vision Statement**

The John Day Innovation Gateway Project is an integrated community development, transportation and design plan that will envision and support the revitalization of the former Oregon Pine mill site and adjacent properties as a dynamic, thriving and welcoming public space. The Gateway will be connected with public trails through restored floodplain habitat along the banks of the John Day River. New land uses, including recreational facilities and an exciting vision for the use of reclaimed wastewater, offer significant public investments for John Day's future. These support the City's initiatives to maximize innovation and efficiency, build partnerships and spur a sustainable and resilient economy that retains and attracts a range of residents and businesses to John Day.

#### **Guiding Principles**

As stated in the project statement of work, the following objectives are guiding the project's development:

- Support development of the former Oregon Pine mill site and adjacent City-owned land to unify design, land uses and transportation connections.
- Rebrand John Day as a thriving rural community to retain residents, attract visitors and sustainable commerce.
- Identify an area for the new wastewater treatment plant to be constructed outside the 100-year floodplain.
- Create a community destination attraction focused on rural innovation and rural value creation to support infill development and a multimodal transportation system.
- Create a beautiful and scenic environment to promote inviting community gathering places and launch new opportunities through transit supportive urban designs.

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- Provide active transportation choices to serve all ability levels and provide a healthy, safe and comfortable user experience.
- Restore public access to the riverfront on both sides of the John Day River and enhance the greenbelt along Canyon Creek to link recreational areas and neighborhoods.
- Improve efficiency in use of land and public infrastructure investments so they become vibrant places.
- Enhance planned and existing amenities which support conservation and renewable energy.
- Apply smart growth development strategies to optimize collaboration and leverage public-private partnerships.
- Capitalize on natural assets that improve livability and health of the community.

The consultant team proposes a <u>synthesis</u> of these detailed objectives into more concise guiding principles for a future Innovation Gateway, as follows, under an overarching vision of *"keep what is great about our community and enhance it"*:

# The Gateway Project should:

- 1. Create a thriving destination
- 2. Attract investment and jobs
- 3. Honor John Day's identity and character
- 4. Promote a connected and healthy community
- 5. Create opportunities for walking and biking
- 6. Provide public access to the John Day River and Canyon Creek
- 7. Efficiently use public resources and land; and
- 8. Support innovation in conservation

# **Evaluation Matrix**

Concept plans for the Gateway will be prepared with close consideration of background information provided to the consultant team, as summarized in Technical Memo #3. As a method of comparing and evaluating the concept plans and draft preferred concept, the following set of 13 criteria are proposed. The team suggests that a concept that scores 6 or fewer points will require further revision or will not be considered as a preferred alternative. The Project Advisory Committee will review all "scores" but public input will be critical in evaluating the first three: Community Support, Complementary Design, and Identity and Character.

Table	1:	Evaluation	Criteria
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Criteria	<b>Evaluation Range</b> (potential score of +1, 0, -1)	Evaluation Score
Community support	<ul> <li>+1 Recommendations and findings complement and support other pertinent plans</li> <li>0 No opportunity to implement in coordination with other projects or partners</li> <li>-1 Poses barriers to implementing other projects</li> </ul>	

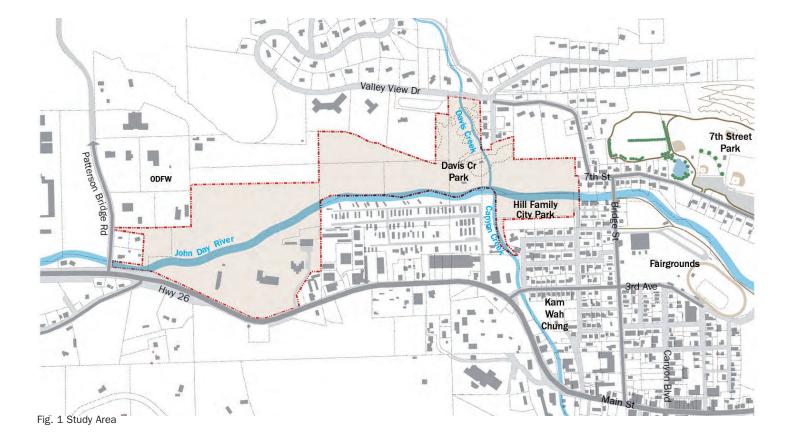
Identity and character	<ul> <li>+1 Plan honors John Day identity</li> <li>0 Some elements of plan context-appropriate</li> <li>-1 Plan doesn't feel appropriate for regional context</li> </ul>	
Complementary design to City as a whole, "enhances existing community"	<ul> <li>+1 Provides opportunity to connect residents to downtown and provides an attractive and welcoming gateway into the City</li> <li>0 No real change - doesn't change relationship of study area to downtown</li> <li>-1 Draws focus away from downtown, feels disconnected from City.</li> </ul>	
Trail system for active transportation	<ul><li>+1 Clear, connected trail system</li><li>0 Some trail connections</li><li>-1 Disconnected, incomplete trails</li></ul>	
Public access to the John Day River	<ul><li>+1 Access provided on both banks</li><li>0 Access provided to at least one bank</li><li>-1 Incomplete access to riverbank</li></ul>	
Smart location for wastewater treatment plant	<ul> <li>+1 Plant is located out of floodplain</li> <li>0 Plant is partially in floodplain</li> <li>-1 Plant is within floodplain and requires mitigation</li> </ul>	
Accommodates mix of uses	<ul> <li>+1 Plan proposes sites for range of land uses</li> <li>0 Plan provides sites for at least 2 land uses</li> <li>-1 Most of site dedicated to single land use</li> </ul>	
Street network and connections	<ul><li>+1 Fully connected, improved complete streets</li><li>0 Some connections, incomplete streets</li><li>-1 Inadequate connections</li></ul>	
Use existing rights of way (ROW)	<ul><li>+1 Plan uses existing ROW</li><li>0 Plan requires some ROW acquisition</li><li>-1 Plan requires significant ROW acquisition</li></ul>	
Potential for future bus transit	<ul> <li>+1 Potential for improved transit service</li> <li>0 No improvement on existing limited transit</li> <li>-1 Transit access is constrained or reduced</li> </ul>	
Flood hazard mitigation for new buildings	<ul> <li>+1 All buildings avoid or raised above floodplain</li> <li>0 Most buildings avoid or raised above floodplain</li> <li>-1 Flood hazard not considered or avoided</li> </ul>	
John Day River habitat and function	<ul><li>+1 River habitat is protected and enhanced</li><li>0 Some river restoration and protection</li><li>-1 River conditions not improved</li></ul>	
Plan ensures implementing of plan in future land use and development actions	<ul> <li>+1 Policy and code amendments provided/adopted</li> <li>0 Policies and code amendments not provided</li> <li>-1 Current policies and code conflict/not resolved with new or amended policies and code</li> </ul>	
Cost of public improvements *Cost estimating is not part of this planning process.	<ul> <li>+1 Plan proposes reasonable improvements within existing funding or future grant potential</li> <li>0 Plan will require additional public investment</li> <li>-1 Plan may entail higher costs; sources of funding unclear</li> </ul>	

End

# Technical Memo #5 Draft Innovation Gateway Area Design Concepts







#### Introduction

Technical Memo #5 is an illustrated report of the draft concepts for the Innovation Gateway Area. It summarizes materials presented on 3/12/2019 at the Advisory Committee Meeting and City Council Meeting. The following topics are covered in narrative, photographs, and illustrative graphics: John Day River history, river restoration alternatives, Oregon Pine site conditions, proposed trail system, 7th St extension, street standards and two concepts for the study area.

The two concepts for the study area were developed in response to a number of planned projects (Davis Creek Trails, Hill Family City Park, Aquatic Center, Wastewater Treatment Plant), site constraints and assets. Overall, the concepts were guided by the overarching vision statement that emerged from community outreach: "keep what is great about our community and enhance it."

# Existing Site Conditions

1 Planer shed 2 Chipper shed 3 Planer shed and adjacent buildings





As noted in Tech Memo #3, the Innovation Gateway Site currently contains Oregon Pine Mill structures in varied conditions surrounded by native and opportunistic shrubs and grasses. Further east on the John Day River, there are current wastewater treatment buildings, ponds, and various industrial materials stored in scattered stacks. The study area is surrounded by striking views of the Aldrich and Strawberry Mountains.





1 Davis Creek Park 2 Hill Family City Park site 3 Current Wastewater Treatment Ponds 4 Future location of 7th St extension, looking west





# John Day River

1 Historic photo of gold dredging near John Day 2 Historic photo of John Day River area during mining activities



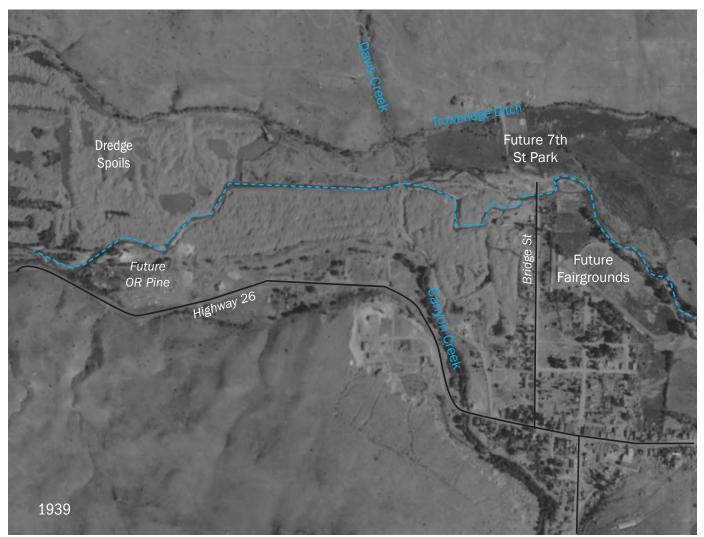


Fig. 2: River History

#### **River History**

The John Day River, where it flows through town, has been drastically altered by past gold mining activities. Bucket dredge mining occurred throughout the river corridor and floodplain in the early 1900s. The natural river course was straightened, narrowed, and re-routed. The valley bottom soils, including the river sand and gravel, were essentially turned upside down, and all of the native vegetation was removed. The dredging activities had severe impacts on river and floodplain function, as well as on fish and wildlife habitat. Fish passage was also disrupted by the irrigation diversion structure at the downstream end of the project area.



#### 1 River channel with no sinuosity 2 River channel near Fairgrounds 3 Canyon Creek 4 Irrigation diversion on Oregon Pine Mill site

#### **Existing Conditions**

As a result, the current John Day River corridor and floodplain are not optimal for fish habitat, recreation, or flood storage. The 100-year floodplain (large flood that occurs, on average, once every 100 years) covers much of the study area except the north west corner (see Fig. 3).

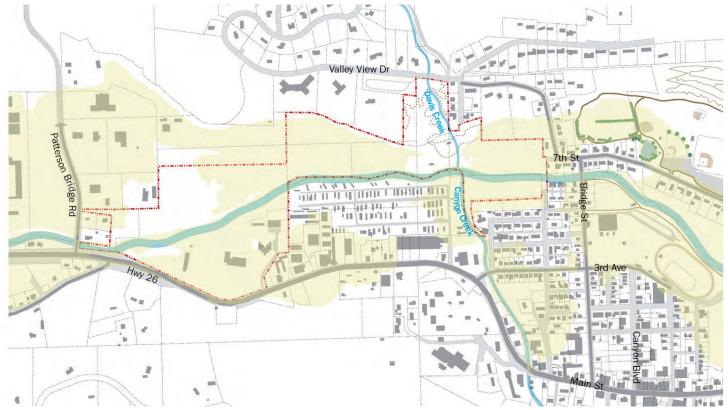


Fig. 3: 100-yr floodplain

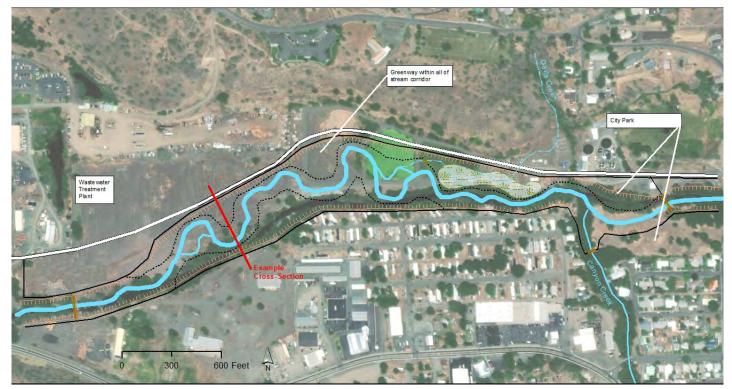
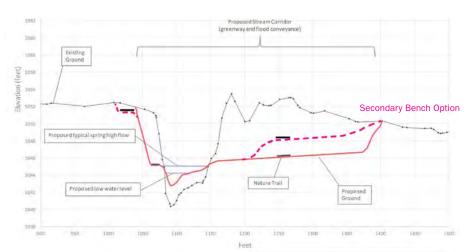


Fig. 4: Concept 1 "Wide Corridor" (Note: this is an initial planning-level concept rendering; actual pattern and location of river would be determined based on further engineering and ecological analysis)



#### Concept 1 "Wide Corridor"

This more extensive river restoration concept creates meanders and side channels throughout the study area. A new wetland could connect Davis Creek to the John Day River on the north side. The river banks are set back to create additional space for flood conveyance, habitat, and recreational facilities such as nature trails on both sides of the river (see Fig. 6).



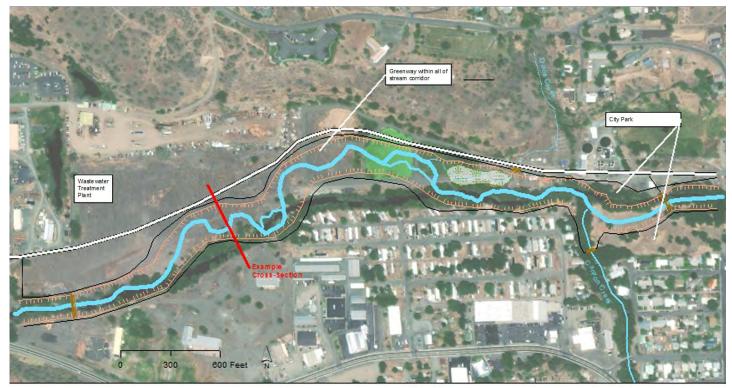


Fig. 5: Concept 2 "Narrow Corridor" (Note: this is an initial planning-level concept rendering; actual pattern and location of river would be determined based on further engineering and ecological analysis)



## Concept 2 "Narrow Corridor"

This concept contains all the same elements (meanders, side channels, wetland, trails) but does so in a narrower stream corridor to reduce the amount of affected land adjacent to river.



1 Pre-project, Minnehaha Creek, MN 2 Post-project, Minnehaha Creek, MN 3 Restored meandered and pedestrian boardwalk, Minnehaha Creek, MN

#### **Benefits of River Restoration**

• Overall increased flood conveyance is likely to reduce flood impacts to nearby infrastructure during large flood events (more analysis will be required to determine specific impacts on flooding)

• New inset floodplain surfaces would have more frequent inundation to better mimic natural river conditions, and can also be utilized for multiple recreational uses

- · Potential to create constructed wetlands to reduce need for City stormwater treatment
- · More recreational trails and access to natural area
- Improved fish habitat
- Additional access for fishing
- · Enhanced land values for neighboring properties
- · Improved visual identity for City
- Maintain existing function of irrigation diversion

#### Considerations for Future Study / Challenges

- $\boldsymbol{\cdot}$  Current wastewater ponds need analysis and permitting to be integrated with river restoration
- The effects of the past gold dredging on future ecological restoration needs further study
- · Needs additional hydrologic engineering feasibility study

 $\cdot\,$  City could still build the proposed improvements without river restoration, but there would be less visual and recreational benefit







1 Side channel, habitat and pathway, La Center 2 Restored river floodplain, La Center 3 Restored river habitat and pathway, Red Tail

# **Concept Overview**

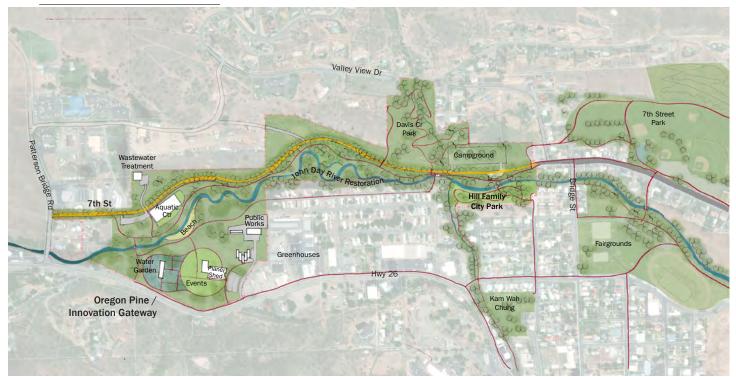


Fig. 7: Concept 1

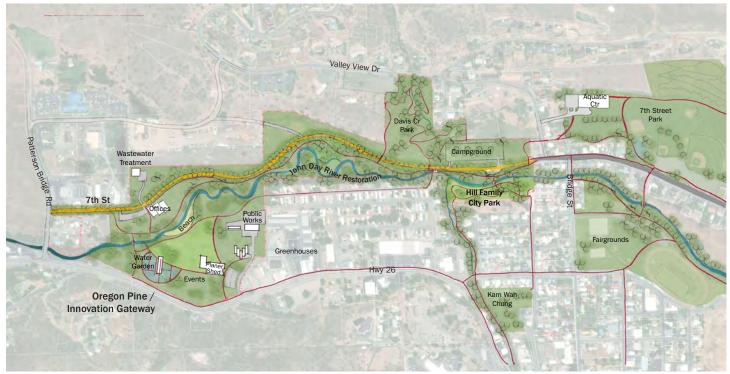


Fig. 8: Concept 2

#### **Overview**

Activity is concentrated in the Oregon Pine / Innovation Gateway site to create a welcoming first impression of John Day and signal development activity and momentum for the city. The anticipated program emphasizes public amenities that bring the John Day community and visitors together. Mill structures will be renovated to host events such as farmers markets, classes, events and communicate the history and identity of John Day. Water sourced from the new state-of-the-art wastewater treatment plant will be showcased in the water garden and greenhouses. The site will also serve as a summer time hub of recreation with a beach for swimming, tubing, and small boat access, aquatic center, and trails for jogging, biking, or walking. The site may also become a node of employment, with a potential office development, vendors in the Planer Shed, greenhouse employees, and public works facilities.

Both concepts (see Fig. 7 and 8) create a strong linkage between the Oregon Pine / Innovation Gateway site and the existing and planned open spaces of 7th St Park, Hill Family City Park, Fairgrounds, and Kam Wah Chung. The new 7th St extension is a 'park street' featuring gentle curves, dense tree canopy, and ample space to walk and bike along the river. A series of greenway paths and footbridges over the John Day River allow for exploration of the restored river and safe access to the town open spaces.

	Concept 1	Concept 2	
Pros	Aquatic Center activates Innovation Gateway	Includes office space	
	<ul> <li>Synergy between Aquatic Center and River</li> <li>Beach</li> </ul>	<ul> <li>Synergy between 7th St Park rec fields and Aquatic Center</li> </ul>	
	• Larger Water Garden	Maintenance efficiencies for Aquatic Center	
		• Larger, river facing event lawn	
Cons	Smaller Event Lawn	Smaller water garden	
	<ul> <li>No office space in study area</li> </ul>	$\cdot$ Less public activation on north side of river	
Acreage			
Event Lawn	2.6 acres	3.8 acres	
Water Garden	2.3 acres	1.2 acres	
Campground	.7 acres	.7 acres	
Office	n/a	.4 acres	
Aquatic Center	1.8 acres	1.8 acres	

# Oregon Pine / Innovation Gateway

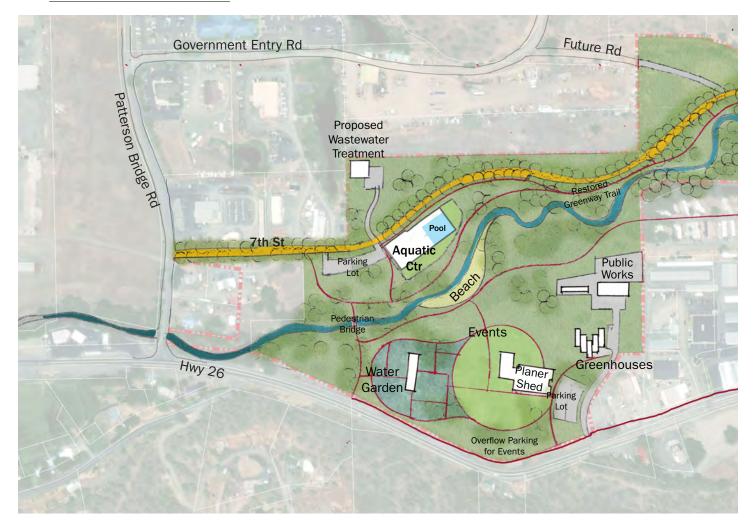


Fig. 9: Concept 1

### Concept 1

This concept includes a smaller, circular event lawn around the renovated Planer Shed, which allows for multiple indoor/outdoor events. This concept will be particularly active in the summer, with the Aquatic Center and beach bringing people to both sides of the river.

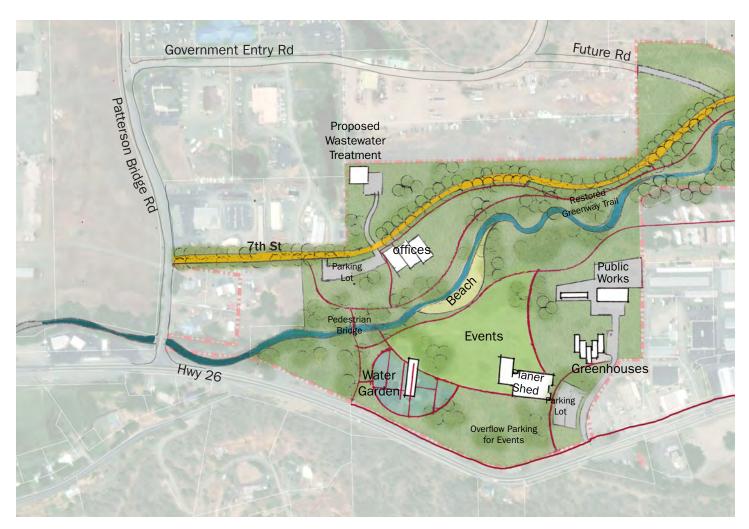


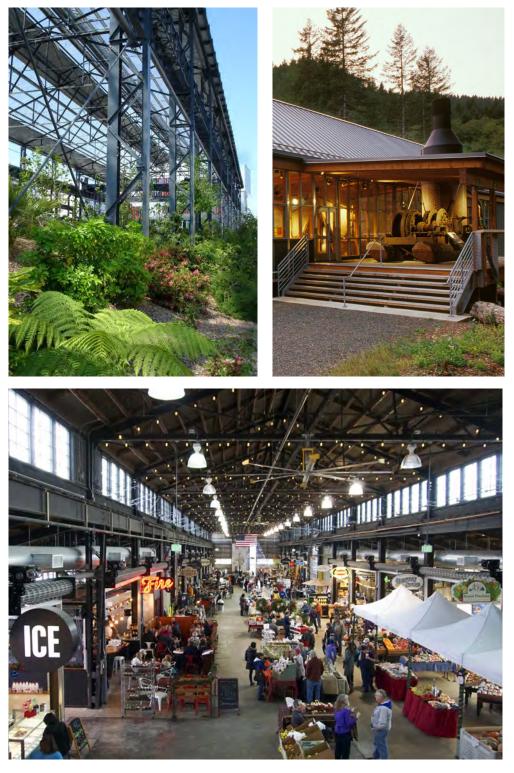
Fig. 10: Concept 2

#### Concept 2

This concept features a larger, river-facing event lawn space and more flexible overflow parking space near Hwy 26. An office development anchors the north side of the river and 7th St extension, with the Aquatic Center located east of the Oregon Pine / Innovation Gateway Site at the 7th St Park.

## **Key Design Elements**

1 Precedent for water garden around adapted Chipper Shed 2 Historic machinery displayed at Tillamook Forest Center, OR, precedent for activation of mill structures 3 Pybus Public Market, Wenatchee, WA, precedent for adaptive reuse of Planer Shed



#### **Adaptive Reuse of Mill Buildings**

Mill structures are renovated to celebrate their unique form and to provide space for events, overlooks for the water garden, interpretive/educational elements, and a farmer's market.



1 Heyburn Idaho Boardwalk, precedent for riverfront trail with overlooks and wayfinding / interpretive signage 2 Festival at 7th St Park, John Day, OR 3 Lawn event in Bend, OR, precedent for flexible lawn space in Oregon Pine Site



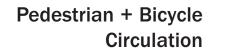


#### **Riverfront Trails**

A robust system of multi-use riverfront trails on both sides of the river provides access for bicycles and pedestrians. This trail system may incorporate boardwalks, paving, and more rustic gravel paths. Three additional pedestrian/bicycle bridges improve river crossings across the study area.

#### **Flexible Event Space**

A flexible lawn adjacent to the renovated Planer Shed allows for a variety of events, such as food and beverage festivals, movie nights, concerts, and fairs. Parking overflow can be accommodated between the event lawn and Hwy 26.



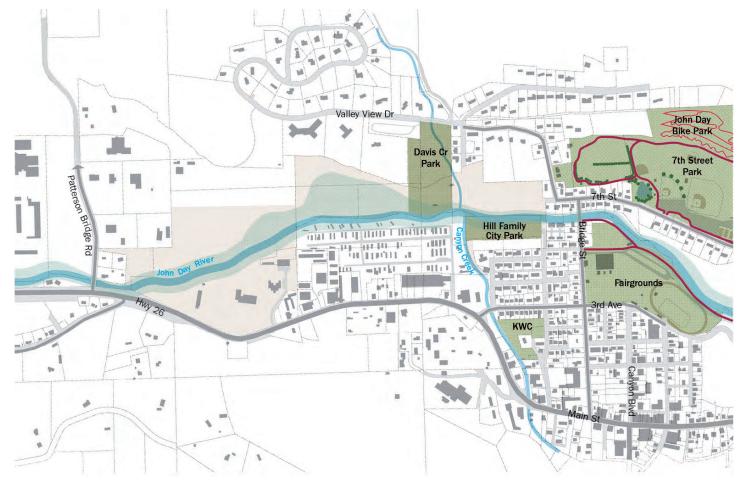


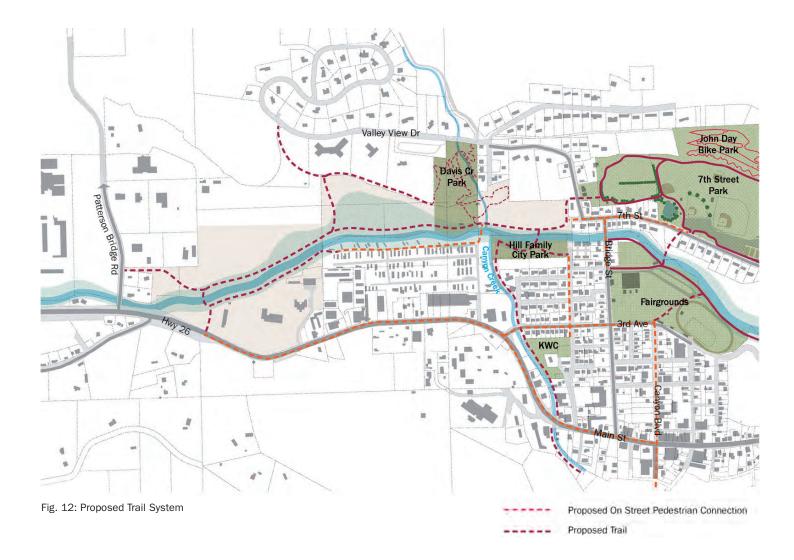
Fig. 11: Existing Trail System



1 Existing trail, 7th St Park

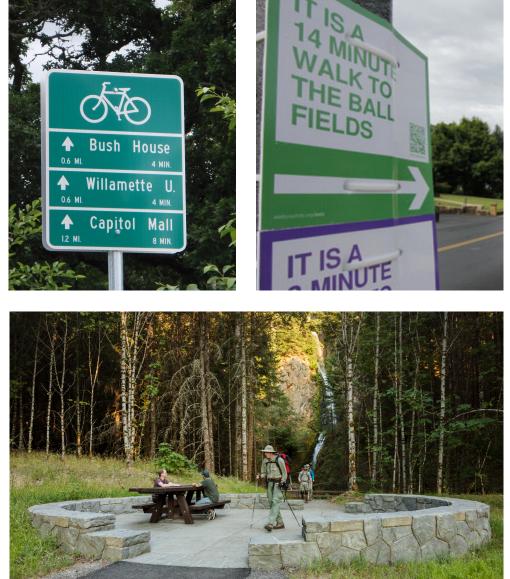
## **Existing Trail System**

The existing trail system is concentrated east of the study area in 7th St Park and the Prospector Trail at the Fairgrounds. These trails provide access to these recreational spaces but lack connections to other key destinations in town.



#### **Proposed Trail System**

The proposed trail system includes new trails on the north and south banks of the John Day River, and new park trails that connect and circulate within Kam Wah Chung, Hill Family City Park, and Davis Creek Park. On-street pedestrian connections link these new trails with town destinations. These on-street pedestrian connections will include new sidewalks or improvements such as pedestrian scale lighting, enhanced crossings, bulb-outs, street trees, and signage.



 Bike wayfinding signage with mileage and estimate travel time, Salem, OR
 Temporary, low cost wayfinding signage to encourage walking and highlight town destinations
 Overlook with rustic wood picnic table and stone walls, Historic Columbia River Highway, OR

#### Wayfinding / Public Art

Wayfinding and public art are incorporated into the proposed trail system to orient and communicate John Day's identity. Comments at the PAC #1 suggested that John Day trails need better signage to encourage exploration by visitors and people relocating to the area. Community members also expressed interest in highlighting the local history of the mill and river in the study area.



#### 1 Interpretive Signage and weathered wood overlook, Minnehaha Creek Preserve, Minneapolis, MN 2 Industrial style pedestrian bridge, Blue Basin, OR 3 Weathered wood and metal wayfinding and interpretive signage, Cottonwood Canyon, OR

#### **Overlooks**

Overlooks along the trail system will capitalize on views of the restored river and the surrounding Aldrich and Strawberry Mountains. Overlooks may include seating, shade, lighting, wayfinding or interpretive signage.

#### Lighting

Lighting along the proposed trail and on street pedestrian connection system is crucial to provide safe, welcoming and pleasant access between town destinations. Lighting near the restored river areas should balance human safety with impacts to wildlife habitat.

# 7th St

 Curving, lush views on the Natchez Trace Parkway, Tennessee
 Robust tree canopy and pocket parking, Yosemite National Park
 Riverside multi-use trail in Truckee, CA



#### 7th St "Park Street"

The 7th St extension will be pleasant for cars, bicyclists, and pedestrians. The street's gentle curves capitalize on views of the restored river and surrounding landscape. Parking pockets interspersed along the street reduce the need for large surface parking lots (see Fig. 13). A multi-use path and sidewalk between the river and street provide ample space for all modes of transportation. Street furniture, such as bicycle racks, pedestrian scale lighting, and benches, will be located at key nodes along the street and reflect the palette of materials on the Oregon Pine / Innovation Gateway site.



1 Native planting and unfinished wooden fencing border a multiuse path 2 Bike racks and street furniture materials inspired by mill materials 3 Bulb-outs for safer pedestrian crossing, rustic lighting, and streetscape planters, Sisters, OR



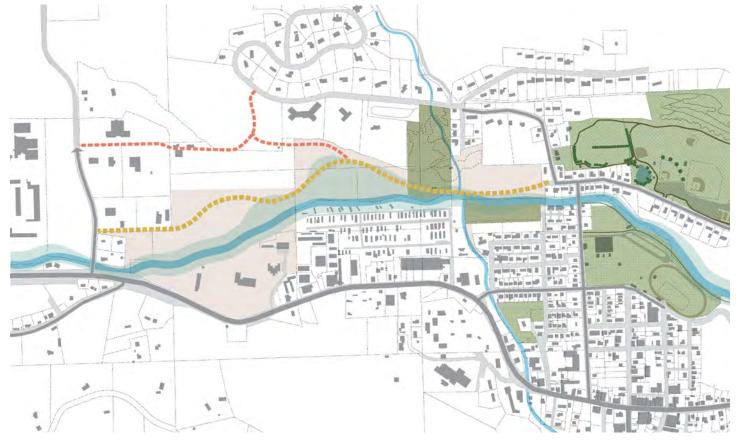


Fig. 14: 7th St Extension, Government Entry Rd, and Future Rd

#### **Future Streets + Extensions**

The 7th St Extension, Government Entry Rd, and Future Rd will provide access to future development on the north side of the river, such as residential or office, and provide a grid of 'complete street' circulation in an area of the city that is currently underserved.

#### **Existing Street Standards**

The typical roadway cross-section standards recommended for streets in the Innovation Gateway Area are summarized below and can be seen in Figures 15 through 19.

**Collector Roadways** 

The 7th Street extension (see Figure 13) and Patterson Bridge Road (see Figure 15) are recommended to include shared travel lanes (versus bike lanes in the current standard) given the expected low speeds (25 mph or less) and low traffic volumes (3,000 vehicles per day or fewer). A sidewalk is recommended on only one side of the 7th Street extension given the proposed multi-use path that will parallel the roadway (consistent with the City collector option B standard). A continuous sidewalk is recommended on the east side of Patterson Bridge Road and on the west side north of the 7th Street extension.

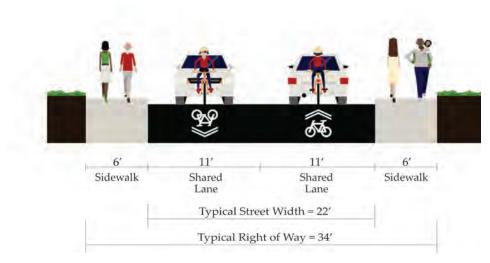


Fig. 15: Recommended Cross-section for Collector Roadways

#### Local Streets

Local streets are recommended to be consistent with the current standard. This includes ten-foot travel lanes, optional eight-foot parking lanes (depending on the parking need of adjacent development) and five-foot sidewalks on both sides. In areas with topographic or other constraints where the recommended section cannot be reasonably constructed, a sidewalk can be constructed on one-side only (consistent with the City local street option B or C standard).

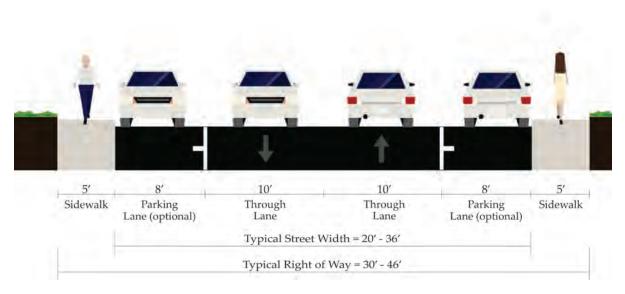


Fig. 16: Recommended Cross-section for Local Streets

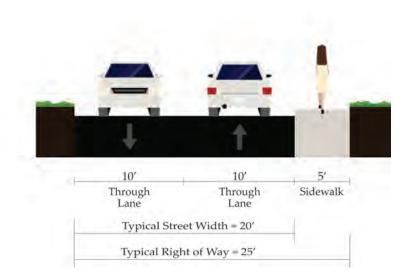


Fig. 17: Recommended Cross-section for Constrained Local Streets

#### W Main Street

W Main Street is recommended to be consistent with the standard in the Highway Design Manual (HDM) between NW 3rd Avenue and the proposed driveway to the site (see Figure 18). This segment will include bike lanes (6-feet wide) and reconstructed sidewalks (6-feet wide) on both sides. W Main Street between the proposed driveway to the site and Screech Alley is constrained by upward slopes. The south side of the highway along this segment has no development potential and is recommended to include a sidewalk on the north side only (see Figure 19). The sidewalk on the north side is recommended to be wider (8 feet versus 6 feet in the HDM standard) and include a four-foot buffer.

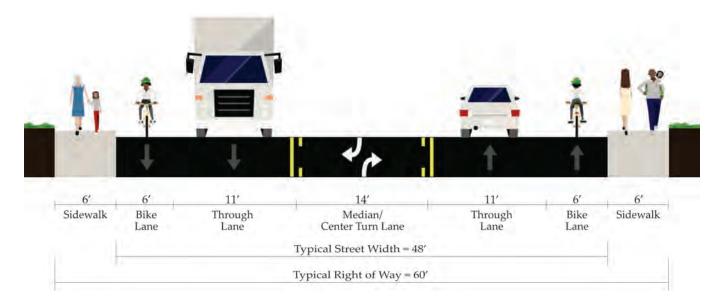


Fig. 18: Recommended Cross-section for W Main Street

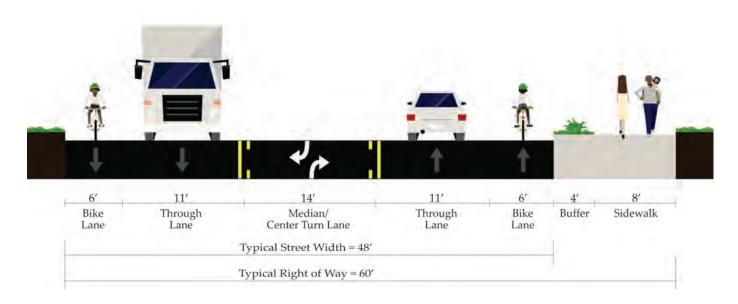


Fig. 19: Recommended Cross-section for Constrained Segment of W Main Street

An Integrated Park System

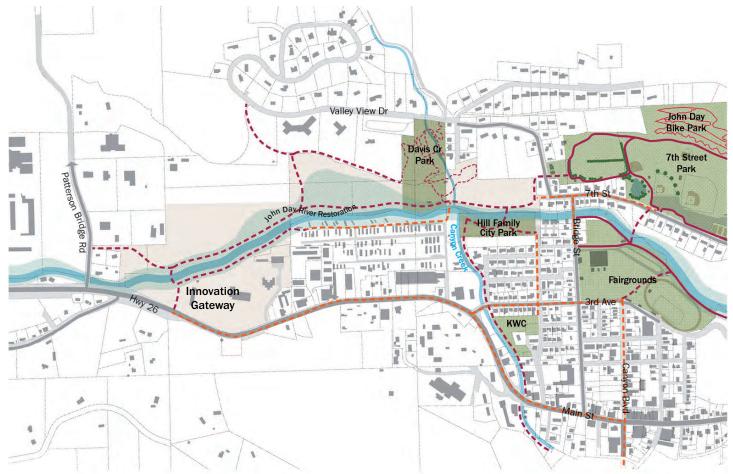


Fig. 20: John Day's Integrated Park System



7th Street Park



Davis Creek Park



Kam-Wah Chung State Heritage Site

A proposed integrated park system will help create a walkable community with connections to the John Day River, downtown John Day and the John Day Innovation Gateway. John Day's five individual parks are used daily by residents throughout Grant County and Oregon. The JDCC Parks & Recreation District services 2,440 residents in John Day and Canyon City at the 7th Street Complex. Day use visitors from neighboring cities frequently visit the parks, with over 40% of patrons at the community pool visiting from outside the district. The Kam Wah Chung Heritage Site had 8,851 visitors in 2018 with guides conducting 16-20 visitors per hour through the store and interpretive center. John Day is establishing a sister city relationship with Sijiuzhen, in the Guangdong Province from which the original Chinese immigrants to the area originated. This will further boost tourism once the state completes construction of its proposed heritage site improvements at the location of the current city park and pool.

Active recreation areas at each facility include:

- 7th Street Complex: Bike park, Ball fields, Skate Park, Splash Pad, Tennis, Pickleball courts, Basketball courts, Playground, Trail system, Cross-fit equipment, Fishing pond, Disc golf, Picnic areas, Future pool and rec center.
- Hill Family Park: Future botanical gardens, Picnic areas, Trails, Open space and free play areas, Future river tubing and beach play area.
- Davis Creek Park: Viewpoints and scenic overlooks, Stream and rock gardens, Biking, Hiking trails, Bird watching, Riverfront trail system.
- Kam Wah Chung: Interpretive center, Kam Wah Chung store, Parks and open spaces.
- Grant County Fairgrounds: Overnight camping (RV and tent), Riverfront trails, Pavilion and Arena, Open spaces.

An integrated park system will tie together the active participation features from multiple existing parks and will create the infrastructure needed for future active recreation facilities, like the new city pool. It will also create a walkable, bikeable trail, bridge and parking system to connect downtown John Day and surrounding residential areas to the John Day Riverfront and to the park system. New infrastructure and restroom facilities will bring these two investments together as a single, integrated parks system.

The proposed parks and recreation improvements are part of a broader economic redevelopment plan to improve access to the John Day River and restore 100-acres of brownfield along the John Day River. This strategy will revitalize the City of John Day and will feature an innovative use of reclaimed water to irrigate the project area. Among the notable environmental benefits of the project is the city's ability to reclaim over 90 million gallons per year of wastewater and put it to beneficial reuse irrigating the integrated park system and providing non-potable water for the facilities.

The integrated park system is a placemaking approach to improve the overall health of the community. These infrastructure investments will open access for residents and visitors alike to the John Day River. It extends the existing trail system and create access to parks for under-served and economically distressed neighborhoods. It creates options for kids to get to multiple recreation sites without using surface streets, creates needed parking to support visitors to the area, and lays the groundwork for future amenities like the new community pool, a new Kam Wah Chung interpretive center, in-city camping and a restored riverfront.

More information can be found at the City's website:

http://www.cityofjohnday.com/planning/page/john-day-integrated-park-system-new-city-parks

# Final Technical Memo #6 Revised Innovation Gateway Area Design Concepts





# "Keep what is great about our community and enhance it"

The Gateway Project should:		Create a thriving destination
	2	Attract investment and jobs
	3	Honor John Day's identity and character
	4	Promote a connected and healthy community
	5	Create opportunities for walking and biking
	6	Provide public access to the John Day River and Canyon Creek
	7	Efficiently use public resources and land
	8	Support innovation in conservation

Fig. 1 Overarching Vision & Guiding Principles

#### Introduction

Technical Memo #6 is an illustrated report of the refined concepts for the Innovation Gateway Area. It summarizes materials presented June 9-10, 2019 at the & PAC/TAC meeting & Regional Economic Development Summit.

The concept for the study area was refined in response to feedback from the PAC/TAC, online survey, and recent developments in city land acquisition and planned projects. The refined concept is guided by the overarching vision statement and guiding principles that emerged from community outreach (see Fig. 1).



Attendees touring the Innovation Gateway at the Regional Economic Development Summit

## Refined Concept Overview

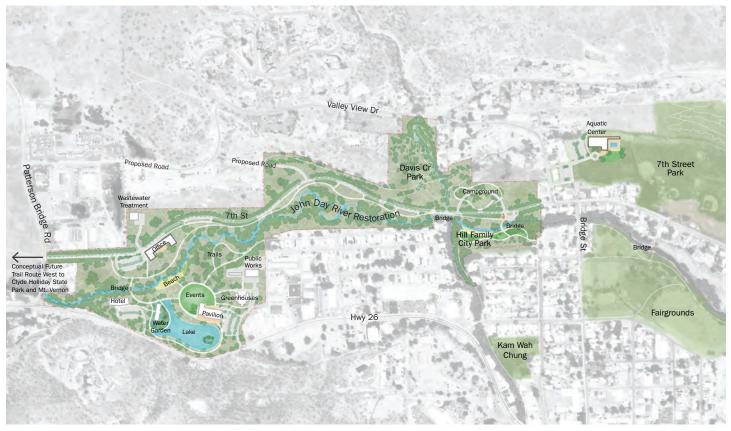


Fig. 2: Refined Oregon Pine / innovation Gateway Concept Plan



#### **Overview**

Activity is concentrated in the Oregon Pine / Innovation Gateway site to create a welcoming first impression of John Day and signal development activity and momentum for the city. The anticipated program emphasizes public amenities that bring the John Day community and visitors together. Former mill structures will be renovated to host events such as farmers markets, classes, events as well as communicate the history and identity of John Day. Water sourced from the new state-of-the-art wastewater treatment plant will be showcased in the water garden and hydroponic greenhouses. The site will also serve as a summer time hub of recreation with a beach for swimming, tubing, and small boat access, and trails for jogging, biking, or walking. The site can also become a node of employment, with a potential office development, vendors in the Planer Shed Pavilion, greenhouse employees, and public works facilities.

The concept creates a strong linkage between the Oregon Pine / Innovation Gateway site and the existing and planned open spaces of 7th St Park, Hill Family City Park, Fairgrounds, and Kam Wah Chung. The new 7th St extension will serve as a minor arterial but will be designed as a 'parkway street' featuring gentle curves, dense tree canopy, and ample space to walk and bike along the river. A series of greenway paths and footbridges over the John Day River allow for exploration of the restored river and safe access to the city's open spaces.

This concept incorporates several program and circulation refinements. A new element is a 100-room hotel south of the John Day River and adjacent to the Water Garden. Parking and circulation have been refined to create a more welcoming entry to the Aquatic Center in the 7th St Park as well as the Planer Shed Pavilion. North of the Hill Family City Park, the campground circulation has been reconfigured to connect to the adjacent Davis Cr Trails.



Fig. 3: Detail of Refined Oregon Pine / innovation Gateway Concept Plan

400

200

# **Concept Renderings**



Fig. 4: A multi-use asphalt trail along the John Day River will connect the Innovation Gateway to the 7th Street Park complex and downtown John Day along the route of the 7th St extension. An interim trail will be built first and the final trail completed in coordination with future river restoration activities. The trails provide safe, comfortable places to walk, bike, and jog in addition to access to the restored John Day River, benches and overlooks, and sites for interpretive elements.



Fig. 5: The Planer Shed is renovated and adapted into a multifunctional community space anchored by an event lawn and access to the restored John Day River via a network of walking and biking trails.

## Consistency with Evaluation Criteria

As a method of comparing and evaluating the draft preferred concept, the following set of 13 criteria were proposed. The team suggested in Technical Memo #4 that a concept that scores 6 or fewer points will require further revision. This scoresheet suggests that the preferred concept meets and exceeds evaluation criteria.

Criteria	Evaluation Range	Evaluation Score
Community Support	+1 Recommendations and findings complement and support other pertinent plans	+1
	0 No opportunity to implement in coordination with other projects or partners	
	-1 Poses barriers to implementing other projects	
Identity and Character	+1 Plan honors John Day identity	+1
	0 Some elements of plan context-appropriate	
	-1 Plan doesn't feel appropriate for regional context	
Complementary design to City as a whole, "enhances existing community"	+1 Provides opportunity to connect residents to downtown and provides an attractive and welcoming gateway into the City	+1
	0 No real change - doesn't change relationship of study area to downtown	
	-1 Draws focus away from downtown, feels disconnected from City.	
Trail system for active	+1 Clear, connected trail system	+1
transportation	0 Some trail connections	
	-1 Disconnected, incomplete trails	
Public access to the John	+1 Access provided on both banks	+0.5
Day River	0 Access provided to at least one bank	
	-1 Incomplete access to riverbank	
Smart location for	+1 Plant is located out of floodplain	+1
wastewater treatment	0 Plant is partially in floodplain	
plant	-1 Plant is within floodplain and requires mitigation	
Accommodates mix of	+1 Plan proposes sites for range of land uses	+1
uses	0 Plan provides sites for at least 2 land uses	
	-1 Most of site dedicated to single land use	
Street network and	+1 Fully connected, improved complete streets	+1
connections	0 Some connections, incomplete streets	
	-1 Inadequate connections	

Use existing rights of way (ROW)	<ul> <li>+1 Plan uses existing ROW</li> <li>0 Plan requires some ROW acquisition</li> <li>-1 Plan requires significant ROW acquisition</li> </ul>	+1 (ROW for 7th Street is entirely public today)
Potential for future bus transit	<ul> <li>+1 Potential for improved transit service</li> <li>0 No improvement on existing limited transit</li> <li>-1 Transit access is constrained or reduced</li> </ul>	+1
Flood hazard mitigation for new buildings	<ul> <li>+1 All buildings avoid or raised above floodplain</li> <li>0 Most buildings avoid or raised above floodplain</li> <li>-1 Flood hazard not considered or avoided</li> </ul>	+1
John Day River habitat and function	<ul><li>+1 River habitat is protected and enhanced</li><li>0 Some river restoration and protection</li><li>-1 River conditions not improved</li></ul>	+1
Plan ensures implementing of plan in future land use and development actions	<ul> <li>+1 Policy and code amendments provided/adopted</li> <li>0 Policies and code amendments not provided</li> <li>-1 Current policies and code conflict/not resolved with new or amended policies and code</li> </ul>	+1
Cost of public improvements	<ul> <li>+1 Plan proposes reasonable improvements within existing funding or future grant potential</li> <li>0 Plan will require additional public investment</li> <li>-1 Plan may entail higher costs; sources of funding unclear</li> </ul>	0
	Total Evaluation Score	+11.5

## **Online Survey**

As of June 6, 2019, the online survey posted on the City's website had 80 respondents, with 40% living in John Day and 70% working in or near John Day. The online survey confirms the city's overall direction for the Innovation Gateway. The majority of respondents support the key elements of the concept: 7th St extension, trail network, improved river access and river restoration. In terms of types of development within the gateway site, there is interest in event space, recreation, hotel or other tourism destinations.

Survey results suggest the proposed trails and improved walking and biking connections to the rest of the city would be well-used; nearly half of respondents walk, hike, or run for recreation or getting around at least a few times per week. Reasons to not walk or bike primiarly focused on the safety of walking and biking facilities.

#### Fig. 6: Survey landing page

## Survey on the Preliminary Concept for the John Day Innovation Gateway and Aquatic Center

#### Tell us what you think of the concept

The City of John Day owns nearly 90 acres of undeveloped land along the John Day River, from Bridge Street to Patterson Bridge Road. At a Community Advisory Committee meeting in March 2019, early concepts were shared for how the site might be redeveloped. An updated concept, shown below, is now being reviewed.

Please take this 10-15 minute survey to share what you think of the concept for the area. Your input will help guide future planning and funding efforts.

This concept has not yet been funded, but much funding could come from grants or private investment.

Image: Concept for the Oregon Pine/Innovation Gateway site.



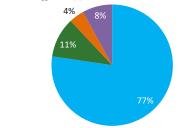
# What do you think about the trail network, including types of trails and uses or the connections to places in town?

Fig. 7: Perception of the trail network is positive.

- I like this and I plan to use any new trails. (58)
- I wouldn't use the trails much, but think residents and visitors would use and benefit from these trails. (8)

#### I don't have an opinion. (3)

I have concerns and/or suggestions. (6)



# What do you think about additional and improved river access for people at the site?

- I like this a lot and would access the river. (60)
- I wouldn't use the river access, but think people and visitors would use it a lot. (7)
- I don't have an opinion. (1)
- I have concerns and/or suggestions. (5)

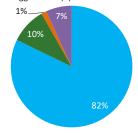
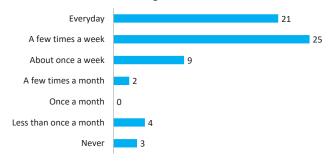


Fig. 8: improved river access is strongly supported

# Tell us about you...

Fig. 9: Survey respondents frequently walk, hike, or run.

How often do you walk, hike, or run for recreation or to get around?



# Preliminary Transportation Cost Estimates

ITEM	COST *
	Note: All costs include varying soft costs and continencies. See City of John Day's 2019 BUILD grant Appendix A for detail. Items marked with <sup>1</sup> include 30% soft costs and a 40% contingency.
Study Area Roads	
7th Street West Extension	\$4,826,779
7th Street East Extension	\$324,182
Government Entry Road Extension	\$652,363
Gateway Drive	\$684,097
Johnson Drive	\$131,080
Wastewater Treatment Plant Facility Access	\$165,968
Campground Road	\$57,000 <sup>1</sup>
Study Area Roads Subtotal	\$6,841,469
Note: Hwy 26 improvements not included in this estimate	
Other new City Roads	
3rd Avenue Bridge and Charolais Heights Street Extension	\$6,257,636
Charolais Heights Intersection Improvements	\$244,143
Other City Roads Subtotal	\$6,501,779
Trails and Recreation	
Oregon Pine-area Trails (decomposed granite / gravel)	\$136,000 <sup>1</sup>
Oregon Pine Bridge (Johnson Bridge) Improvements	\$90,000 <sup>1</sup>
Oregon Pine-area Concrete Paths	\$122,000 1
Multi-Use Trail along John Day River (asphalt)	\$460,000 <sup>1</sup>
Campground Trails (gravel nature trail)	\$34,150 <sup>1</sup>
Integrated Park Transportation Infrastructure (@ Hill Family Park)	\$555,864
Interpretive Overlooks on river	\$204,000 <sup>1</sup>
Gravel Parking Lot/Trailhead (between 7th St + river )	\$23,970 <b>1</b>
Gravel Parking Lot (Planer Shed)	\$153,000 <sup>1</sup>
Trails & Recreation Subtotal	\$1,778,984
Grand Total	\$15,122,232

#### Notes:

The costs on the previous page are preliminary, based on current (2019) cost ranges for construction in eastern Oregon.

The City of John Day included preliminary cost estimates in two past transportation documents:

#### 1996 TSP

A new Collector was proposed, between Patterson Bridge Road and NW Bridge Street. The route would have extended from just south of the USFS offices, uphill to the west of the existing wastewater treatment ponds, and the proposed ROW was 60'. A total cost of \$429,000 was estimated for this Collector.

#### 2009 Local Street Network Plan

The new 7th Street connection was not included in this plan update, but instead, a multi use bicycle/ pedestrian path was proposed on the north bank of the John Day River, at an estimated cost of \$2,085.000.

Technical Memo #7 includes a description of proposed changes to the 2009 Local Street Network Plan.







720 SW Washington St. Suite 500 Portland, OR 97205 503.243.3500 www.dksassociates.com

# MEMORANDUM

DATE:	September 16, 2019
TO:	John Day Innovation Gateway Area Plan Team
FROM:	Kevin Chewuk, DKS Associates
SUBJECT:	Technical Memo #7:
	Transportation Solutions Analysis

P18194-000

This memorandum summarizes the multimodal transportation solutions needed to support new proposed development within the John Day Innovation Gateway Area Plan study area.

# **Proposed Concept Plan**

The proposed Concept Plan is shown in Figure 1. Most of the study area north of the John Day River is zoned for industrial use, while most of the area south of the river is zoned for general commercial use. The proposed Concept Plan includes offices, a wastewater treatment plant, parks and open space and a campground north of the river, and a hotel, parks and open space, public works facilities and greenhouses south of the river.



# Figure I: Proposed Concept Plan



# **Proposed Circulation System**

The following sections summarize the project site's proposed circulation system for pedestrian, bicycle, transit and vehicular travel, and proposed sections for newly constructed or reconstructed streets.

# **Pedestrian Circulation System**

Pedestrians approaching and traveling within the project site will be able to safely and efficiently walk between destinations using a proposed system of sidewalks, multi-use paths and trails. As a primary pedestrian thoroughfare, an improved W Main Street section will include a continuous sidewalk on the north side from downtown John Day to Patterson Bridge Road, and from downtown John Day to the proposed Johnson Drive on the south side. No sidewalk is recommended on the south side west of the proposed Johnson Drive due to the lack of adjacent development and topography constraints. A network of walkways and trails is recommended just to the west of Johnson Drive that would connect to recommended sidewalks on W Main Street at the Johnson Drive intersection. In addition, curb ramps are recommended at each intersection crossing along W Main Street to bring them into Americans with Disabilities Act (ADA) compliance.

# DKS

The proposed 7<sup>th</sup> Street extension will serve as the primary pedestrian route north of the John Day River. It will provide a continuous pedestrian connection between Patterson Bridge Road and Bridge Street. This street is proposed to include a 5-foot sidewalk and a multi-use path will parallel the roadway on the south side.

A 5-foot sidewalk is recommended on the east side of Patterson Bridge Road between W Main Street and the proposed Government Entry Road. The proposed Gateway Drive will also provide a local walkway linking the proposed 7<sup>th</sup> Street with the proposed Government Road extension. This street is proposed to include a 5-foot sidewalk on the north side. Government Entry Road is also proposed to include a 5-foot shoulder on each side for pedestrian travel from Patterson Bridge Road to Valley View Drive.

The proposed multi-use path on the south side of the proposed 7<sup>th</sup> Street will provide a primary walkway along the John Day River. This multi-use path will provide for convenient and comfortable travel and recreation between Patterson Bridge Road, the Oregon Pine and Innovation Gateway areas and the proposed Aquatic Center and existing multi-use pathway network within 7<sup>th</sup> Street Park, east of Bridge Street. In addition, a proposed network of multi-use paths and trails will link the proposed multi-use path along the John Day River with Hill Family City Park, Davis Creek Park and Campground, Oregon Pine and Innovation Gateway areas and Valley View Drive.

An improved Oregon Pine Bridge and a bridge adjacent to Hill Family City Park will provide new pedestrian crossings of the John Day River, in addition to the existing crossings at Patterson Bridge Road and Bridge Street. This network of river crossings will provide shorter block lengths for the pedestrian system, will increase pedestrian access to destinations, and will also provide a recreational loop trail.

Safe and comfortable pedestrian crossings will be provided where facilities cross streets. This will include curb extensions and marked cross-walks where appropriate. A pedestrian wayfinding system for the site and the entire downtown area should also be developed.

# **Bicycle Circulation System**

The proposed 7<sup>th</sup> Street extension will serve as a local bikeway, serving those traveling from downtown John Day and the neighborhoods to the north and east. Given the relatively slow vehicular speeds along the proposed street, bicyclists will share travel lanes with vehicular traffic. It is recommended to include sharrows to alert drivers to share the street and be designed to allow bicyclists to travel outside of the door zone of parked vehicles.

In addition, cyclists can travel along the proposed multi-use path along the John Day River between the Oregon Pine and Innovation Gateway areas and Bridge Street. A potential link to Hill Family City



Park and 7<sup>th</sup> Street Park will also connect the site with the multi-use path networks in these parks. This multi-use path will provide for convenient and comfortable bicycle travel between the Oregon Pine and Innovation Gateway areas and Bridge Street.

W Main Street will serve as the primary bikeway south of the John Day River. It is recommended to include 6-foot bike lanes between downtown John Day and Patterson Bridge Road. The recommended bike lanes will also enhance the Old West Oregon Scenic Bikeway through the project area.

The proposed Johnson Drive will connect cyclists from the bike facilities on W Main Street to the Oregon Pine and Innovation Gateway areas. Since it will be a local street and expected to have low traffic volumes and slow vehicular speeds, cyclists will share the travel lanes with vehicular traffic.

Government Entry Road is proposed to include a 5-foot shoulder on each side for bike travel from Patterson Bridge Road to Valley View Drive. Sharrows are also recommended on Patterson Bridge Road between W Main Street and the proposed Government Entry Road.

In addition, it is recommended that bike racks and bike storage zones be incorporated in strategic locations along streetscapes and within future development to encourage bicycle use. A bicycle wayfinding system for the site and the entire downtown area should also be developed. This system could also consider routing to mountain biking opportunities, to enhance the city's reputation for the sport.

# **Transit Circulation System**

The proposed 7<sup>th</sup> Street is recommended to serve as the primary pedestrian and bicycle path to bus service. Bus pull-outs are recommended in strategic locations along the proposed alignment to serve future bus service. The on-street sidewalk and multi-use pathway network will connect transit users from these facilities to other key destinations. Within reasonable proximity to the project site, pedestrians and cyclists can also access the existing bus stops on W Main Street.

The project site will be served by high quality pedestrian/bicycle connections. A network of river crossings will provide shorter block lengths and is oriented towards pedestrian and bicycle users, with active and inviting pathways and public walkways on both sides of the river. It is the intended that the project site will also include a supportive mix of uses and amenities for encouraging transit ridership. Future bus stop locations should also include necessary infrastructure (e.g., shelter, bench, signage) to encourage transit ridership.



# Vehicular Circulation System

W Main Street provides primary access to the project site south of the John Day River, with two proposed accesses. One access is proposed to be located approximately 1,100 feet east of Patterson Bridge Road, serving the proposed hotel. The primary site access is proposed to be Johnson Drive, which will be located approximately 900 feet east of the proposed hotel driveway. Left-turn lanes are recommended on W Main Street at both proposed access points, and the Johnson Drive approach to W Main Street is recommended to include separate left-turn and right-turn lanes for exiting traffic.

Another key assumption of the proposed vehicular circulation system is the extension of 7<sup>th</sup> Street to Patterson Bridge Road north of the John Day River. 7<sup>th</sup> Street's proposed alignment will provide an alternative route to the highway and a means of vehicular site access to areas north of the river. This new street connection will provide circulation between Patterson Bridge Road and Bridge Street and offer drivers from downtown John Day and neighborhoods to the north and east another option to access the site.

North-south running cross-streets, including Patterson Bridge Road and Bridge Street, will link the proposed 7<sup>th</sup> Street and W Main Street. The proposed Government Entry Road and Gateway Drive will connect 7<sup>th</sup> Street and Patterson Bridge Road with neighborhoods to the north, further enhancing connectivity north of the river.

The proposed street system modifies some of the classifications of the John Day Transportation System Plan (TSP). Given the City's standards, the estimation of traffic volumes on area streets and overall circulation needs, recommended classification/reclassification is as follows:

- W. Main Street will continue to serve as an Arterial Street
- Patterson Bridge Road, Bridge Street and the proposed Government Entry Road will continue to be Collector Streets
- 7<sup>th</sup> Street is recommended to be a Minor Arterial Street, modified from a Collector Street in the TSP
- Gateway Drive and Johnson Drive are newly identified streets that were not in the TSP and are recommended as a Collector Street and Local Street, respectively

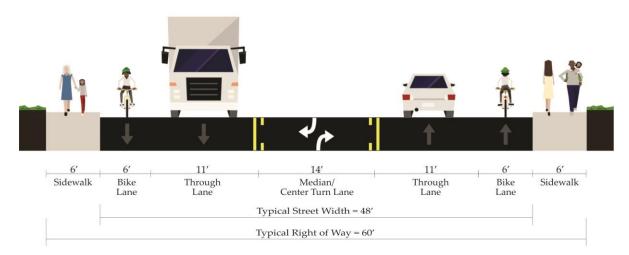
# **Street Sections**

The recommended street sections are shown in Figures 2 through 10. Typical streets in the project site will consist of two lanes of traffic and occasional on-street parking. Landscape strips and lighting will enhance the pedestrian experience, while cross-walks at strategic locations will create safer, more fluid circulation opportunities.



# W Main Street between NW 3<sup>rd</sup> Avenue and Johnson Drive

W Main Street is recommended to be consistent with the standard in the Highway Design Manual (HDM) between NW 3<sup>rd</sup> Avenue and the proposed Johnson Drive. This segment will include bike lanes (6-feet wide) and reconstructed sidewalks (6-feet wide) on both sides (see Figure 2) and would fit within the existing 60-foot highway right-of-way. Note that more right-of-way would need to be obtained (beyond the existing 60-feet) if a buffer is desired between the roadway and the sidewalk.



# Figure 2: Recommended Design for W Main Street between NW 3rd Avenue and Johnson Drive

# W Main Street between Johnson Drive and Patterson Bridge Road

W Main Street between the proposed Johnson Drive and Patterson Bridge Road is constrained by upward slopes on the south side of the highway. The south side of the highway along this segment also has no development potential and is recommended to include a sidewalk on the north side only (see Figure 3). The sidewalk on the north side is recommended to be wider (8 feet versus 6 feet in the HDM standard) and include a four-foot buffer. A left turn lane is only recommended at the proposed hotel driveway, Screech Alley and Patterson Bridge Road intersections with W Main Street. Otherwise, the center turn lane can be narrowed at mid-block locations as feasible.



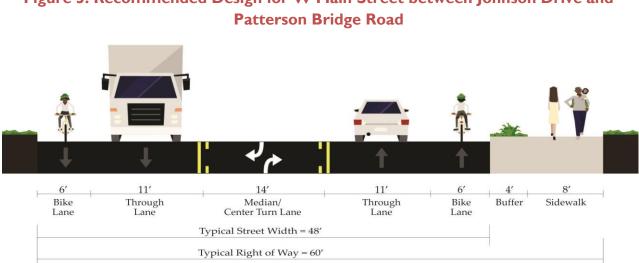


Figure 3: Recommended Design for W Main Street between Johnson Drive and

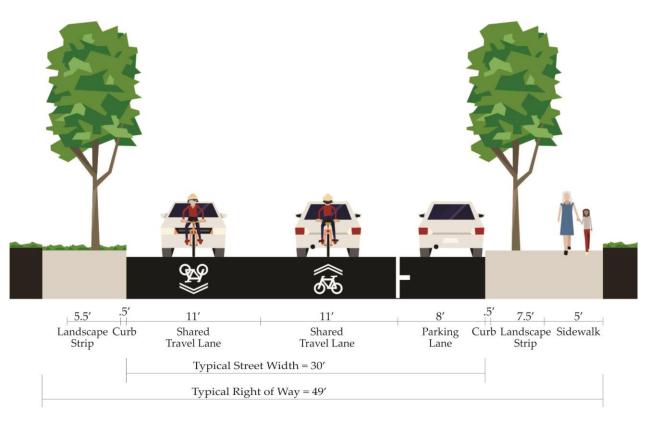
## 7<sup>th</sup> Street

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for an Arterial would require 7th Street to include at least 62-feet of right-of-way. This includes two 14-foot travel lanes and a 12-foot center turn lane, two 5-foot bike lanes and a 6-foot sidewalk on each side.

Narrower travel lanes are recommended along 7<sup>th</sup> Street (11 feet versus 14 feet) to encourage slower vehicular travel speeds. Given the relatively slow vehicular speeds expected, it is recommended to include sharrows instead of bike lanes. In addition, no center turn lane is recommended given the lack of driveways and slow travel speeds. A sidewalk is recommended on only one side of 7th Street given the proposed multi-use path that will parallel the roadway.

The recommended design for 7<sup>th</sup> street is shown in Figure 4a, with a conceptual view shown in Figure 4b. It includes 49-feet of right-of-way, consisting of two 11-foot travel lanes, a 6-foot landscape strip on the north side and an 8-foot landscape strip on the south side, and an 8-foot intermittent parking lane and 5-foot sidewalk on the south side. A sidewalk is recommended on the north side only in areas adjacent to bus stop locations.





# Figure 4a: Recommended Design for 7<sup>th</sup> Street

Figure 4b: Conceptual view of 7<sup>th</sup> Street



**Transportation Solutions Analysis | Page 8** 

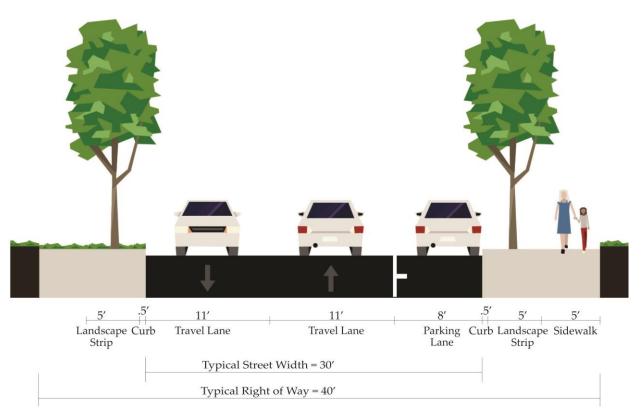


# **Gateway Drive**

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for a Collector would require Gateway Drive to include at least 40-feet of right-of-way. This includes two 11-foot travel lanes, two 5-foot bike lanes and a 6-foot sidewalk on one side.

Wider landscape strips are recommended to enhance the pedestrian experience and allow for occasional on-street parking. No bike facilities are recommended since facilities are recommended on adjacent routes (i.e., 7<sup>th</sup> Street, Patterson Bridge Road and Government Entry Road).

The recommended design for Gateway Drive is shown in Figure 5. It includes 40-feet of right-of-way, consisting of two 11-foot travel lanes, a 5.5-foot landscape strip on each side and an 8-foot intermittent parking lane and 5-foot sidewalk on the north side.



# Figure 5: Recommended Design for Gateway Drive

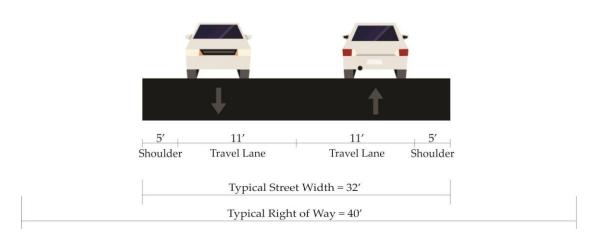
# **Government Entry Road**

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for a Collector would require Government Entry Road to include at least 40-feet of right-of-way. This includes two 11-foot travel lanes, two 5-foot bike lanes and a 6-foot sidewalk on one side.



Since Government Entry Road is expected to have low traffic volumes, slow vehicular speeds and given rural nature of the surrounding uses, it is recommended to include shoulders in place of bike lanes and a sidewalk for pedestrian and bike travel.

The recommended design for Government Entry Road is shown in Figure 6. It includes 40-feet of right-of-way, consisting of two 11-foot shared travel lanes and a 5-foot shoulder on each side.



# Figure 6: Recommended Design for Government Entry Road

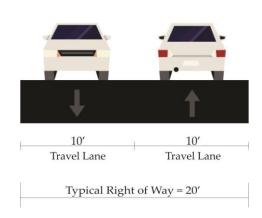
# Johnson Drive

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for a Local Street would require Johnson Drive to include at least 40-feet of right-of-way. This includes two 10-foot travel lanes, an 8-foot parking lane and a 6-foot sidewalk on one side.

Parking is proposed to be provided in lots surrounding Johnson Drive, so no on-street parking is recommended. In addition, a network of walkways and trails is recommended just to the west of Johnson Drive that would connect to recommended sidewalks on W Main Street. Therefore, no sidewalks are recommended along the alignment.

The recommended design for Johnson Drive is shown in Figure 7. It includes 20-feet of right-of-way, consisting of two 10-foot travel lanes.





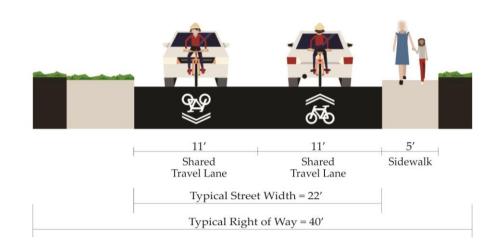
# Figure 7: Recommended Design for Johnson Drive

# **Patterson Bridge Road**

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for a Collector would require Patterson Bridge Road to include at least 40-feet of rightof-way. This includes two 11-foot travel lanes, two 5-foot bike lanes and a 6-foot sidewalk on one side.

Given the relatively slow vehicular speeds expected along Patterson Bridge Road, it is recommended to include sharrows instead of bike lanes and a slightly narrower sidewalk on one side (5 feet versus 6 feet).

The recommended design for Patterson Bridge Road is shown in Figure 8. It includes 40-feet of rightof-way, consisting of two 11-foot shared travel lanes and a 5-foot sidewalk on the east side.



# Figure 8: Recommended Design for Patterson Bridge Road



# **Oregon Pine Bridge Improvements**

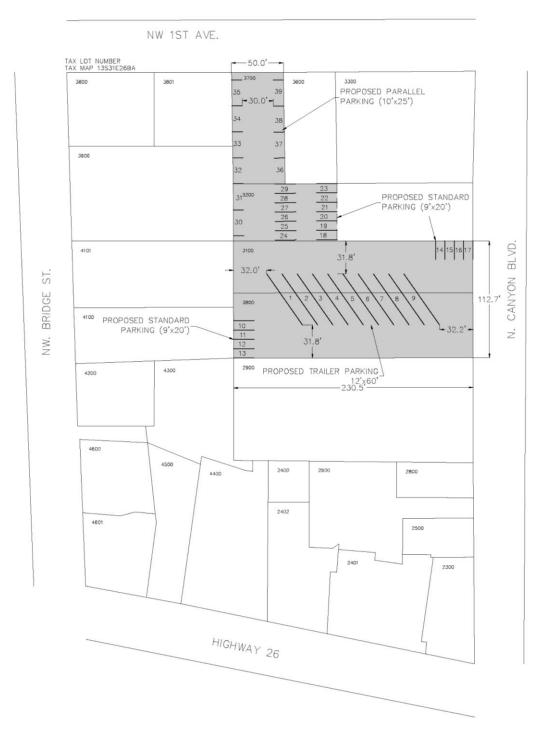
The proposed Oregon Pine Bridge will be an adaptive reuse of the existing but dilapidated bridge that crosses the John Day River on the Oregon Pine site. With financial and material assistance from the DR Johnson Lumber Company, the bridge will be rehabilitated for use by pedestrians and bicycles and will serve as an access bridge for Public Works or emergency vehicles. The existing bridge is structured on two railroad car bases, which will remain, with improved reinforced concrete abutments and a new cross-laminated timber surface. For additional detail, please refer to the appendix.

# **Parking Facilities**

The recommended vehicle parking facilities will be developed on a project-by-project basis to support the proposed development. Shared parking strategies will be integrated as a key element of future development.

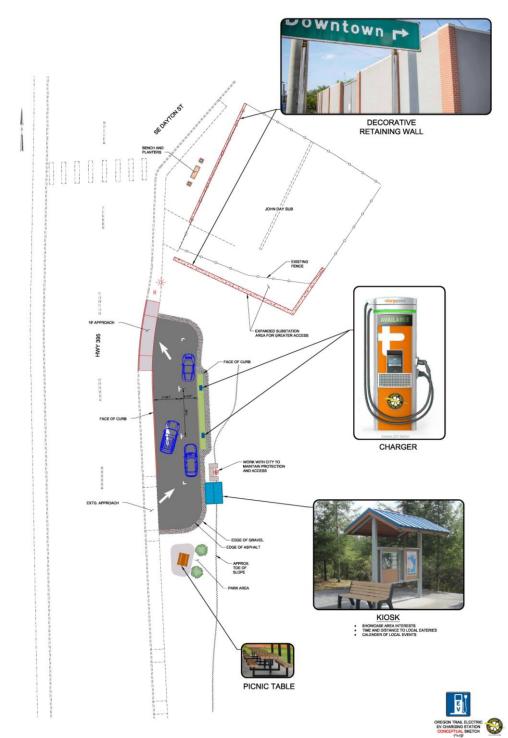
Intermittent on-street parking will be provided along one side of 7<sup>th</sup> Street and Gateway Drive. Potential locations for off-street parking are identified in Figure 1, although it may vary depending on development. This includes parking lots near the Oregon Pine and Innovation Gateway Area and Hill Family City Park. In addition, a parking lot is proposed downtown near the Canyon Boulevard and 1<sup>st</sup> Avenue intersection (see Figure 9) and a lot providing an electric vehicle charging station is proposed near the SE Dayton Street and Hwy 395 intersection (see Figure 10).





# Figure 9: Downtown Parking Lot





# Figure 10: Downtown Electric Vehicle Charging Station



# Summary of Transportation System Recommendations

Table 1 and Figure 111 summarize transportation improvements needed to support future growth and new development within the John Day Innovation Gateway Area Plan study area, including projects previously identified in the TSP or Local Street Network Plan.

Not all recommended improvements need to be in place prior to developing land within the John Day Innovation Gateway Area Plan study area. Upgrade of the existing streets will be driven by the multi-modal access needs of the adjacent properties. Many of the street construction projects, such as 7<sup>th</sup> Street and Johnson Drive, will be dependent on new development.

# Projects Outside of the Plan Area

In addition to the projects within the John Day Innovation Gateway Area Plan study area, a few projects are needed to support vehicular, pedestrian and bicycle circulation outside of the plan area. This includes street extensions west of Valley View Drive and Government Entry Road, improvements at the Valley View Drive, Bridge Street, Charolais Heights and Boulder Lane intersection, and extensions of Charolais Heights to 3rd Avenue, and 7th Street to the Charolais Heights extension. These projects are also shown in Table 1 and Figure 11.



Table I: Recommended Transportation System Improvements in the John Day
Innovation Gateway Area Plan Study Area

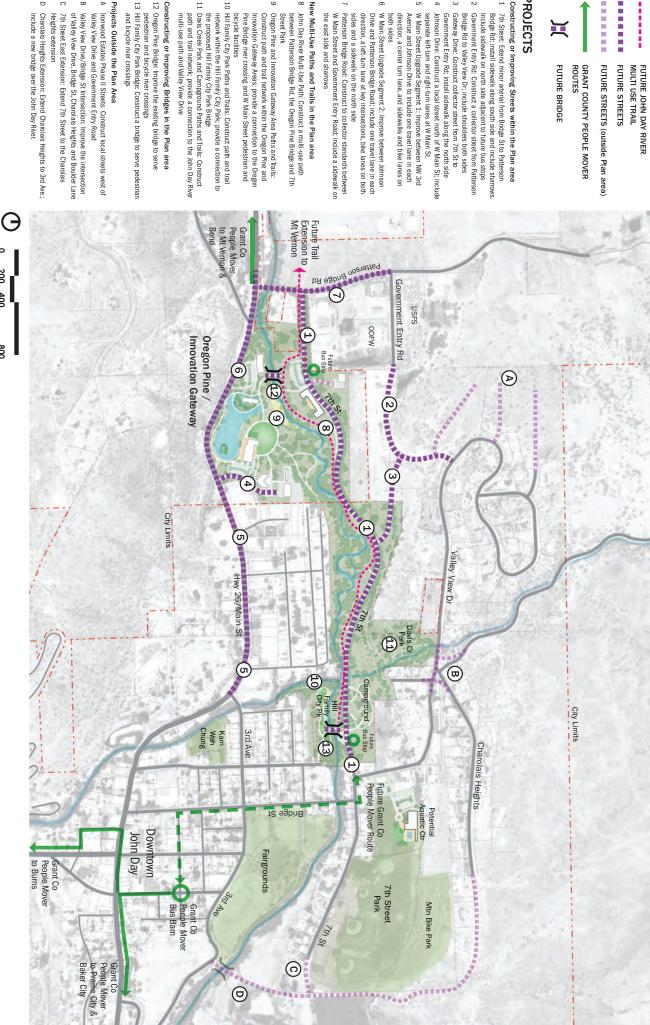
	Project Description	Project Summary	Project Source			
Proje	cts Constructing or Im	proving Streets within the Plan area				
1	7th Street	Extend a minor arterial from Bridge Street to Patterson Bridge Road; install a sidewalk on the south side and include sharrows. A sidewalk should be installed on the north side adjacent to future bus stop locations	Modified version of TSP Project Alternative 2 (modified roadway design and classification)			
2	Government Entry Road	Construct a collector street from Patterson Bridge Road to Valley View Drive; include shoulders on both sides	Modified version of Local Street Network Plan Project 10 (modified roadway design)			
3	Gateway Drive	Construct a collector street from 7 <sup>th</sup> Street to Government Entry Road; install a sidewalk along the north side	New Project			
4	Johnson Drive	Construct a local street north of W Main Street; include separate left-turn and right- turn lanes at W Main Street	New Project			
5	W Main Street Upgrade Segment 1	Improve between NW 3 <sup>rd</sup> Avenue and Johnson Drive to include one travel lane in each direction, a center turn lane, and sidewalks and bike lanes on both sides	Local Street Network Plan Projects 38 and 39			
6	W Main Street Upgrade Segment 2	Improve between Johnson Drive and Patterson Bridge Road; include one travel lane in each direction, a left-turn lane at key intersections, bike lanes on both sides and a sidewalk on the north side	Modified version of Local Street Network Plan Projects 38 and 39 (modified roadway design)			
7	Patterson Bridge Road	Construct to collector standards between W Main Street and Government Entry Road; include a sidewalk on the east side and sharrows	New Project			
Proje	Projects Constructing Multi-Use Paths and Trails in the Plan area					
8	John Day River Multi-Use Path	Construct a multi-use path between Patterson Bridge Road, the Oregon Pine Bridge and 7 <sup>th</sup> Street Park	Local Street Network Plan Project 5			
9	Oregon Pine and Innovation Gateway Area	Construct path and trail network within the Oregon Pine and Innovation Gateway Areas; provide a connection to the Oregon Pine	New Project			

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	Paths and Trails	Bridge river crossing and W Main Street		
		pedestrian and bicycle facilities		
	Hill Family City	Construct path and trail network within the		
10	Park Paths and	Hill Family City Park; provide a connection	New Project	
	Trails	to the proposed Hill Family City Park Bridge		
	Davis Creek Park	Construct path and trail network within the		
11		Davis Creek Park and Campground; provide	Mary Drain at	
11	and Campground	a connection to the John Day River multi-use	New Project	
	Paths and Trails	path and Valley View Drive		
Projec	ets Constructing or Im	proving Bridges the Plan area		
12 Dregon Pine Bridge		Improvements to the existing bridge to serve	Novy Project	
		pedestrian and bicycle river crossings	New Project	
Hill Family City	Construct a bridge to serve pedestrian and	Nora Drois et		
13 Park Bridge		bicycle river crossings	New Project	
Projects Outside of the Plan area				
Ironwood Estates		Construct local streets west of Valley View	Name Durain at	
А	Phase II Streets	Drive and Government Entry Road	New Project	
	Valley View Drive			
D	/ Bridge Street	Improve the intersection of Valley View	Nora Drois et	
В	Intersection	Drive, Bridge Street, Charolais Heights and	New Project	
Improvement		Boulder Lane		
C	7th Street East	Extend 7 <sup>th</sup> Street to the Charolais Heights	Moru Proiost	
C Extension		extension	New Project	
C	Extension	extension		
D	Extension Charolais	Extend Charolais Heights to 3 <sup>rd</sup> Avenue;	Local Street Network Plan	



JOHN DAY INNOVATION GATEWAY



the east side and sharrows

both sides

Street Park

bicycle facilities

Ο Charolais Heights Extension: Extend Charolais Heights to 3rd Ave.; Heights extension include a new bridge over the John Day River.

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200 400

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Projects Outside the Plan Area

and bicycle river crossings

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LEGEND

PROJECTS

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То:	Aaron Lieual John Day	-	Topic:	FINAL Technical Memo #8: Implementation Section: Comprehensive Plan and Development Code Amendment Options	Date:	09/12/2019
From:	Matt Hastie,	APG	Project:	John Day Innovation Gateway	Project #:	P3465
Distrib	ution:	Technica	al Mer	no #8: Comprehensiv	ve Plan Po	olicy and
Basec	amp	Develop	ment	Code Section		
		amendments	s to the (	erves as the portion of Tech Me City of John Day's Comprehensi p implement the John Day Inno	ive Plan and D	evelopment Code
		SECTION <sup>·</sup>	1			
		Introductio				
		area in close members. T planned proj	e coordin he conce jects. The	s developed a proposed concep nation with City staff, advisory co ept also reflects recent developr e concept is guided by the over	ommittees and nents in city la arching vision	other community nd acquisition and statement and
		guiding prind	ciples that	at emerged from community out	reach during th	nis planning effort.
		<ul> <li>Integra</li> </ul>	ated syst	res 1 and 2, key elements of the tem of parks, trails and natural a ıral areas along both sides of th	areas, includin	g:
				gration with new or existing deve		cilities on both
		• New	/ facilities ○ Forn	s Creek Park, Hill Family Park, s and amenities for community r ner mill structures will be renova sses, events as well as commu	members and vated to host ev	ents such as
		John Day.				
		treatment pla	ant for a	of water sourced from the new new water garden and hydropo ential future office development,	nic greenhous	es.
			ds and ti	e employees, and public works f rails providing access and circu itv		e area and to othe
		•		id relocation of the City's waste	water treatmer	it plant
		properties w and Develop	ithin the ment Co	is Plan will require new develop planning area. It is important th ode allow for and support these ses the Comprehensive Plan ar	at the City's Co activities. Tow	omprehensive Pla ards that end, this

Page 2 of 11

consistency with the Innovation Gateway Plan and recommends potential needed amendments to those documents. Because cities relatively infrequently update their Comprehensive Plan policies and Development Code provisions, this also is an important opportunity to update those documents to address other recent planning processes and issues, including the Grant County Economic Opportunities Analysis project.

# **SECTION 2**

## **Comprehensive Plan Policy Amendments**

Local Comprehensive Plans provide the policy foundation for current and future decisions related to land use, housing, economic development, parks and recreation and other activities within a city. As such, it is important that policies in John Day's Comprehensive Plan be consistent with and supportive of the goals of the Innovation Gateway Plan. Following is a list of existing relevant Comprehensive Plan policies:

General Land Use Policies

- 1. All new developments should recognize and respect the particular character of established areas in which they locate.
- 2. Innovations in concepts and flexibility in design should be encouraged in new developments in those more undeveloped sections of the planning area where no particular urban character has been sufficiently established.

These policies generally support the Innovation Gateway Plan and only the following modest revisions to them are recommended.

5. To continue participation in the District OEDP Program <u>and other state and regional economic</u> <u>partnerships</u> to insure maximum opportunity for economic development assistance and financing.

#### **Economic Element Policies**

Many of the existing Comprehensive Plan policies generally apply to and are consistent with and supportive of the Innovation Gateway Plan. The following additional policies are recommended to further support the Gateway Plan and to incorporate guidance from the Grant County Economic Opportunities Analysis project.

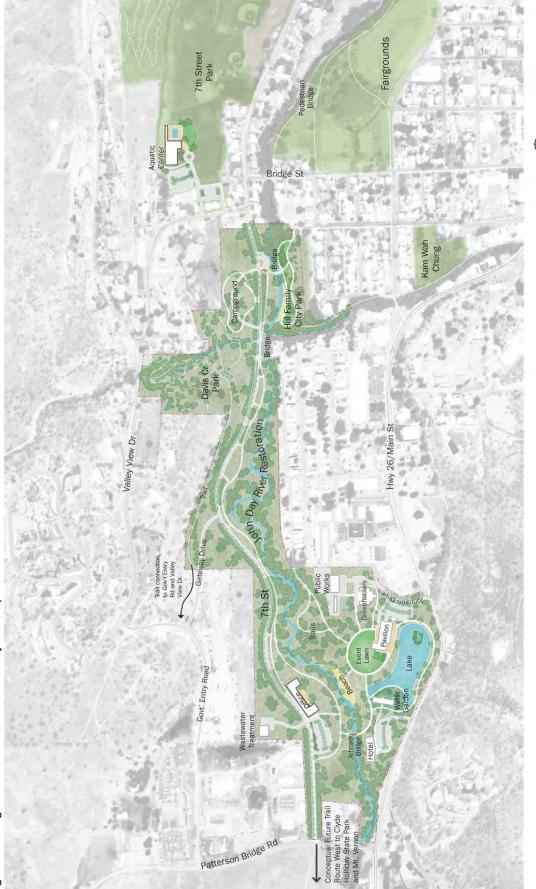
- XX. To support continued development of the Innovation Gateway Area as a community destination and attraction that focuses on rural innovation, agri-tourism, and value creation.
- XX. To support investments in public and private infrastructure that will complement city-wide goals, as well as those for specific areas such as the Downtown and Innovation Gateway Area.
- XX. To adopt and regularly update a short list of clear, achievable economic development actions as a Community Investment Strategy that will further the economic goals of the community.
- XX. To support and participate in achieving the goals of the Grant County Digital Network Coalition.
- XX. To support facilities and programs for new home construction and urban renewal as the John Day Urban Renewal Agency.

#### **Public Facilities Element Policies**

The following policy is recommended to be added to this section of the Comp Plan:

11. Conservation, reclamation and re-use of water shall be a goal of the City and shall be incorporated in future planning for public and private development.

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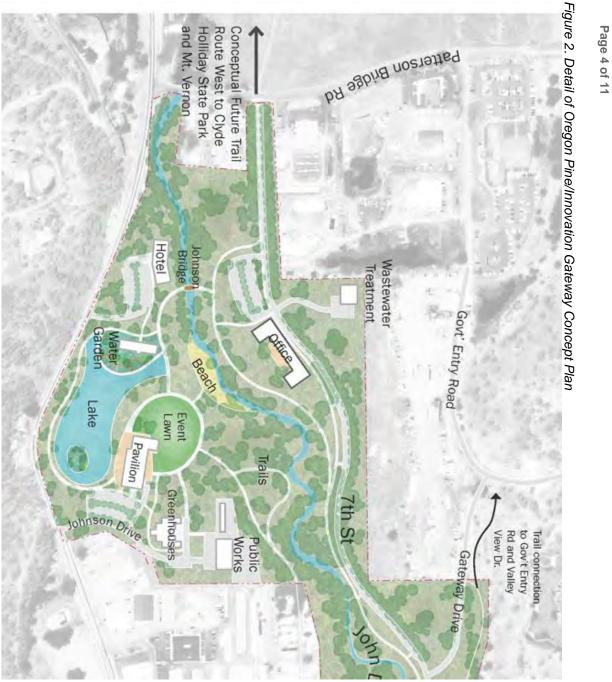
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# Figure 1. Oregon Pine/Innovation Gateway Concept Plan





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# SECTION 3 Potential Development Code Amendments

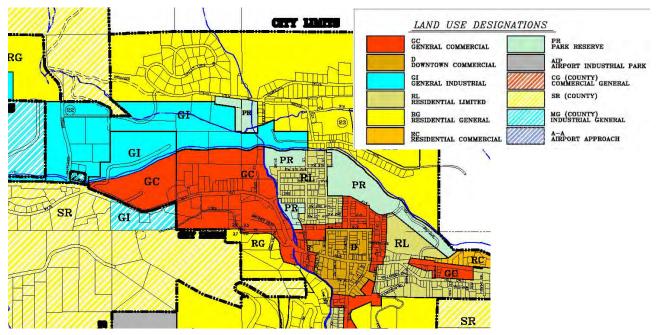
The study area is entirely within the city limits of John Day. A significant portion of the study area (primarily north of the John Day River) is zoned for industrial use. The majority of the project area south of the river, including a portion of the area recently annexed into the City, is zoned for general commercial use. The area southeast of the confluence of the John Day River and Canyon Creek (planned for future river recreation access) was recently rezoned to the City's Park Reserve zone to ensure consistency with current use and future plans for the property. Existing zoning designations are shown in Figure 3 and allowed uses (permitted outright and conditionally) in the applicable industrial and commercial zones are summarized in Table 1.

In addition to establishing allowed uses in each zoning designation, the city and county development codes outline other design and development standards. In both the city and county industrial zones, development standards are generally limited to building setbacks and heights, and requirements associated with parking, landscaping fences, signs and stream setbacks. The city's commercial zones include a variety of additional standards related to building orientation, block and lot layout, architectural design and pedestrian amenities.

Table 1:	
Land Use Designation	Allowed Uses
General Industrial	Residential: Existing as of 11/24/2005 <u>Commercial</u> : Drive-Up/Drive-In/Drive-Through, Offices, Parking lot (CU), Vehicle servicing and repair, Retail sales under 10,000 sf, Self-service storage <u>Industrial</u> : Industrial services, Manufacturing, Warehouse and freight movement, Waste-related (CU), Wholesale sales <u>Institutional</u> : Utilities, Public safety facilities, Parks and open space (CU) <u>Other</u> : Accessory structures, Agriculture (existing as of 11/24/2005 and as an accessory to a permitted industrial use), Mining (CU)
General Commercial	<ul> <li><u>Residential</u>: Existing as of 11/24/2005 or not occupying a ground floor space on Main Street <u>Commercial</u>: Drive-Up/Drive-In/Drive-Through (CU), Bed and breakfast inn, Educational services, not a school, Entertainment – major event (CU), Offices, Outdoor recreation (CU), Parking lot (CU), Vehicle servicing and repair – fully enclosed (CU), Retail sales and commercial services</li> <li><u>Industrial</u>: Industrial services – fully enclosed, Manufacturing – fully enclosed, Wholesale sales – fully enclosed and under 20,000 sf (CU)</li> <li><u>Institutional</u>: Utilities, Community service, Family day care, Adult day care, Day care subject to state licencing (CU), Parks and open space (P/CU), Religious institutions – new (CU), Schools – new (CU)</li> <li><u>Other</u>: Accessory structures, Radio frequency transmission facilities (CU), Utility corridors (CU), Temporary uses (P/CU), Transportation facilities</li> </ul>
Park Reserve Overlay	<u>Residential</u> : Single family dwelling, master planned development (CU) <u>Commercial</u> : Commercial campground or recreation facility (CU) <u>Industrial</u> : None <u>Institutional</u> : Utilities, Public safety facilities, Parks and open space (CU) <u>Other</u> : Farm use, public reserve areas of natural, historical or geological significance, public wildlife reserve, utility facility (CU), golf course (CU), public marina, recreation camp or resort (CU), public picnic grounds, parks, playgrounds, campgrounds or nature trails (CU), farm structures and permanent facilities (CU)

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Figure 3. Project Area Existing Zoning



The primary remaining land use actions for the study area will be to review and adjust specific development code requirements that apply to the area. This may include changes to allowed uses, as well as standards for building height, setbacks, landscaping requirements, or architectural design standards. This can be done by amending the base zoning designations or by establishing an overlay zone or other mechanism that allows for variations in the standards applied within the study area. Following is a summary of potential changes to these elements of the code.

# Allowed Land Uses

The proposed Innovation Gateway Plan calls for the following uses in the zones within the area:

#### General Industrial

- Wastewater treatment plant
- Park
- Open space, natural areas, trails
- Campground

#### General Commercial

- Office
- Hotel
- Parks, natural areas and trails
- Public works facility
- Greenhouses

#### Park Reserve

Park and trails

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For the most part, these uses are allowed in these areas by the City's Development Code, with the following exceptions:

- Campgrounds do not appear to be allowed within the General Industrial zone.
- Parks are allowed as a conditional use in all of the zones. While this may be generally appropriate in these zones, it will require much of the development identified in the Innovation Gateway Plan to go through a conditional use approval process.
- Public works facilities are not identified explicitly as an allowed use in the General Commercial (GC) zone. They do not appear to meet the definition of a "utility" but may fall under one of the other uses allowed in the zone.
- Hotels do not appear to be allowed in the GC zone.

City staff also note that the bulk of the area owned by the city and directly adjacent to the north side of the river is within a proposed riverfront restoration corridor. It includes the old and planned new wastewater treatment plant site, as well as proposed new road and network of trails and natural areas. Although they are not explicitly identified as a proposed use or activity on the north side of the river in the project area, it is possibly that the community would want to see some type of small cafes or restaurants allowed and/or developed in that portion of the study area in the future. This area currently is zoned GI along with the surrounding area. Given the overarching vision and proposed set of land uses and activities in this area, the GI designation is likely not the most appropriate designation. For example, the GI zone permits a wide variety of industrial uses but does not allow for retail or commercial uses over 10,000 sf in size; it allows for retail and commercial uses smaller than 10,000 sf as conditional uses. It allows for parks and open spaces as conditional uses. Options to address these issues could include:

- Rezone the area as Park Reserve (PR), similar to the future city campground at the east end of the Gateway Innovation area. However, commercial and retail uses are not allowed in the Park Reserve zone, creating a potential conflict with that zoning designation which would need to be resolved through other code amendments.
- Create and apply a new zoning designation that better matches the Innovation Gateway area concept and allows for a mix or park, open space, restoration, and small-scale commercial uses, as well as institutional uses such as the new wastewater treatment facility.
- Create and apply an overlay zone which allows for or limits uses differently than the base zones and/or incorporates other specific development or design standards.

**Recommendation**: Create and apply an overlay zone which allows for or limits uses differently than the base zones and/or incorporates other specific development or design standards.

#### Site Development Standards

These standards typically include minimum or maximum requirements for lot size, lot coverage, building height, setbacks, landscaping percentage and/or other site planning elements. Following is a summary of these standards for the zones in the planning area, as well as in the Light Industrial zone.

Standard	General Commercial	Park Reserve	General Industrial	Light Industrial
Minimum lot size	2,000 sf	None	None	None
Maximum lot coverage	90%	90%	90%	80%
Minimum landscaping	10%	10%	10%	10%

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Minimum setbacks Front Side Rear	0' 0' 0'	20-60' 5-10' 10'	20' 20' 20'	10' 10' 10'
Maximum setback (build-to line)	60'	NA	NA	NA
Maximum building height	45'	None	35'	35'

In general, most of these requirements appear to be reasonable and do not appear to represent a barrier to the types of uses and development proposed within the planning area, with just a couple of exceptions. The 60' maximum setback line could be problematic, given that the commercial area south of the river consists of a single-large site entirely owned by the City. As a result, several of the buildings proposed for the interior of the site would not meet this requirement. As currently formulated, the proposed concept includes several buildings which are located more than 60 feet away from the edge of the property. The possible exceptions are the greenhouses and public works facility.

In addition, the Parks Reserve zone does not include any maximum building height standard. This is not necessarily a problem or barrier for development in the Innovation Gateway area, but it could be a problem in the future for development in other areas with this designation.

#### Commercial District Standards – Block Layout, Orientation, and Architectural Design Standards

Sections 5-2.3.060 through 5-2.3.080 include a number of additional standards for development in commercial zones. The standards in these sections are oriented towards traditional commercial areas with a typical downtown or commercial area block layout and orientation of buildings and businesses to adjacent streets and sidewalks. The layout of these areas is expected to be based on a typical grid street layout similar to existing residential and commercial areas south of the river in John Day. However, the Innovation Gateway area will consist of a number of individual buildings or uses within a park-like setting. The area will contain roads and pathways but they do not conform to a typical layout commonly seen in commercial and residential areas in other parts of John Day. As a result, Block Layout standards (5-2.3.060.D.1 and 5-2.3.060.D.2) are not consistent with the Innovation Gateway Area concept.

Architectural design standards found in section 5-2.3.080 also are generally oriented to a traditional commercial area, including standards related to building entrances, windows and other features. It may be challenging for some of the specific buildings or uses proposed for the Innovation Gateway to meet these standards (e.g., the Public Works facility, Greenhouses, Hotel, etc.), given either the nature of those uses and/or their orientation to streets adjacent to or within the site. This could be the case for consistency with specific standards under the "Pedestrian Orientation," "Compatibility," and "Human Scale" provisions. However, the code language noting that "the approval body may approve a different design upon finding that the design contains an equally good or superior way of achieving the above standard" may be sufficient to address these potential challenges. Given the system of pathways, open areas and other pedestrian-oriented features envisioned for the Innovation Gateway area, standards found in section 5-2.3.090 (Pedestrian Amenities - Human Scale) are not likely to pose any significant constraints for future implementation of the concept.

Options to address these issues could include:

- Revise the potentially conflicting standards in the base zones, either as they apply to all development in the base zones or to apply specifically to the Innovation Gateway area
- Create an overlay zone which applies different standards to development in the Innovation Gateway area, in comparison to other areas in the City.

**Recommendation**: Create and apply an overlay zone which applies different standards to development in the Innovation Gateway area, in comparison to other areas in the City.

9/12/2019

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# SECTION 4 Other Potential Zoning and Development Code Issues

## Industrial Area Zoning, Northwest Corner of Project Area

City staff note that the City plans to acquire the approximately 14-acre property in the northwest corner of the project area from Iron Triangle. This area currently is zoned General Industrial (GI), similar to the rest of the surrounding area. Given its proximity to the riverfront and the residential areas, the City wants to ensure that future use or development of this area does not detract from the greenway along the riverfront and is not incompatible with future housing in the new development. Outdoor equipment storage or similar uses or activities should be avoided. Options to address this issue could include:

- Apply another zoning designation to this area. For example, city staff indicate that Light Industrial (LI) may be a more appropriate designation for this area if it is rezoned. The City has this designation in its development code, but it is not currently applied to any other property in the city limits.
- Limit specific uses or activities in the area through use of an overlay zone or other strategy.

# Residential Uses in Commercial Zones

Currently, residential uses are allowed in the City's Downtown and General Commercial zones only if the housing existed as of November 4, 2005; or if the housing unit(s) does not occupy the ground floor facing Main Street. The City has previously interpreted this language to mean that most forms of housing cannot be developed in its commercial zones. However, housing has been developed in these areas (typically apartments) and it has been difficult to track whether -property owners have reverted from Commercial to Residential use after November 4, 2005. Several property owners within the GC zone have requested to convert their businesses back to residences due to market demand for rental properties and single-family housing but are unable to do so due to this code restriction.

City staff note that it may be appropriate to allow housing in commercial zones in the future as long as it does not occupy ground floor spaces on Main Street. Depending on the nature of changes to this aspect of the Development Code and the amount of community conversation desired on this topic, this issue potentially could be addressed as part of the Innovation Gateway project or it might better be addressed as part of a separate future code amendment process. This potential change to the code would have implications for the Gateway Innovation area because it could result in housing being allowed in the commercially zoned portion of the area. Options to address the issue could include the following:

- Allow all types of housing in the city's commercial zones as long as the housing does not occupy ground floor space on Main Street
- Allow only certain higher density forms of housing in commercial areas (e.g., townhouses and apartments or housing developed above a certain minimum residential density threshold), also with the Main Street ground floor restriction

#### Fairgrounds Site Planning and Zoning

The City has been approached about taking the lead on a planning effort for the County Fairgrounds and adjacent areas within John Day, east of downtown near the 3<sup>rd</sup> Street Extension. The area is currently zoned as Park Reserve (PR) and Residential Limited (RL). As part of that planning effort, the City will need to determine the most appropriate zoning designation for the area, depending on the mix of land uses envisioned there.

#### Recommendations

• Within the industrial area in the northwest corner of the project area, limit specific uses or activities in the area through use of an overlay zone,

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- The following types of housing should be allowed in the City's commercial zones:
  - New attached housing types (single-family attached housing with four or more units and multi-family housing)
  - Housing within buildings that were built or developed as residential structures prior to November 4, 2005, regardless of when the building was converted from a residential use to a non-residential use or was reconverted to a residential use
- The following types of housing should be prohibited in the City's commercial zones:
  - o Housing that occupies ground floor space on Main Street
  - o New single-family detached housing, duplexes and triplexes
- Exceptions to the above standards could be allowed within the Gateway Innovation area if desired.
- Zoning and development requirements in the Fairgrounds/3<sup>rd</sup> Street Extension area will be addressed as part of a separate planning process.

# SECTION 5 Road and Trail Designations and Standards

Proposed major new roads in the project area ultimately will need to be identified in the City's Local Street Network Plan and/or Transportation System Plan. They will need to have designations (e.g., arterial, collector or local roads) that match their intended levels of traffic, design and use and be consistent with other related planning and/or funding efforts. For example, the City will benefit from classifying 7<sup>th</sup> Street as a Minor Arterial to ensure it can be competitive in pursuing a federal BUILD program grant. Proposed classifications for the proposed streets are shown in Table X. Similarly, to the extent that specific trails identified in the Innovation Gateway Area Plan are intended to be developed to specific standards, they also should receive the appropriate designations, with corresponding cross-sectional or construction standards identified in the City's TSP, Public Works Design Standards or other appropriate document.

Project Number	Project Title	Reference Sheet(s)	Description
1	7 <sup>th</sup> Street West Extension	2-5	4,950-foot minor arterial that extends from Patterson Bridge Road at the west to Bridge Street on the east.
2	Government Entry Road	6	1,453-foot collector street that connecting the 7th Street extension via Gateway Drive
3	Gateway Drive	7	1,040-foot collector street between Government Entry Road and the 7th Street west extension
4	Johnson Drive	8	633-foot local street to provide public access to the Gateway via Hwy 26
5	Wastewater Treatment Plant Facility Access Road	5	351-foot local street will provide public access to the City's new wastewater treatment plant
6	Charolais Heights Intersection Improvements	18	Reconstructs intersection to improve traffic flows to future residential areas for Ironwood Estates and the future Prospector Way development

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Project Number	Project Title	Reference Sheet(s)	Description
7	Integrated Park System Transportation Infrastructure	17	Improves transportation infrastructure at the eastern edge of the Innovation Gateway to connect city parks via access parking and a new multi-modal bridge
8	Oregon Pine Bridge Improvements	Engineering Plans	Oregon Pine bridge rehabilitation restores a multi- modal bike/ped bridge for public access
9	7th Street East Extension	14	A 450-foot minor arterial extension to the east end of 7th Street will connect to the new Charolais Heights collector extension
10	3rd Avenue Bridge and Charolais Heights Extension	12-14	Builds new 130-foot span 3 <sup>rd</sup> Avenue bridge crossing and extends collector street north to complete Charolais Heights
11	Ironwood Estates Phase II Street Improvements	15-16	2,370-feet of new pavement in three local street extensions ending in stub-outs, two accessed from the existing Valley View Drive and one from the Government Entry Road built in Phase One.
12	John Day Future Industrial Development	4-7 & MOU Map	City will acquire 14 acres of industrial land shown in part on sheets 4-7 and in the enclosed MOU map for future redevelopment
13	OTEC Power Upgrades	OTEC Addendum & MOU	OTEC has provided a system map and project description for proposed power upgrades
14	OTC Fiber Optic Network Infrastructure	OTC MOU	OTC Fiber will be installed on all Phase 1 and Phase 2 streets in conduit supplied by City – conduit is included in costs for each project
N/A	Prospector Way	9-11	Prospector Way is not included in the scope of the grant application nor is it included in the costs or benefits section of the benefit-cost analysis. It is shown to reference a future street development which the Charolais Heights intersection and extension projects will be designed to accommodate.

#### TIMELINE

The following timeline is recommended for preparation, review and adoption of proposed Comprehensive Plan and Development Code amendments.

- August 15 Prepare draft Plan and Code amendments for review by City staff
- August 20 Provide draft Plan and Code amendments to PAC, TAC, Planning Commission and City Council for review
- August 27 Conduct PAC/TAC meeting #4 and joint Planning Commission/City Council work session to review draft Comp Plan and Code amendments
- September 2 Provide 35-Day Notice of proposed Plan and Code amendments to Department of Land Conservation and Development
- October 8 Conduct joint Planning Commission/City Council hearing to adopt proposed Plan and Code amendments (first reading)



ECONOMICS · FINANCE · PLANNING

DATE: August 22, 2019 TO: Nick Green, City of John Day FROM: Bob Parker, Matthew Craigie SUBJECT: JOHN DAY TECH MEMO #8 INNOVATION GATEWAY PLAN – FUNDING AND FINANCE-REVISED DRAFT

This memorandum describes a proposed funding and financing plan for future transportation improvements to support new land use development, as identified in City of John Day's Innovation Gateway Plan. This document meets partial requirements for Technical Memorandum #8 under the Oregon Department of Transportation (ODOT) Transportation Growth Management (TGM) project with the City of John Day. The other requirements are met through two companion memoranda that are being produced by Angelo Planning Group and DKS.

# **Purpose and Background**

The City of John Day is striving to overcome multiple challenges that have resulted from decades of population loss and economic stagnation. The City and community are committed to reorienting towards a growth and economic diversification mindset. Simply put, the City and its partners have outlined concrete actions to attract more visitors and residents to incrementally stabilize and start to regrow the local population and economy. To accomplish these high-level goals — population growth and economic stability — the City adopted the Strategy for Growth in 2017. This is a visionary document that outlines the City's goals and how they intend to achieve them. The City of John Day understands that leveraging their investments to achieve multiple goals is the best way to achieve the City's desired outcomes. The Strategy for Growth has led to several City ambitious economic development initiatives, including:

- The John Day Community Investment Strategy (CIS)
- Investments in housing supportive infrastructure, broadband internet, and greenhouses
- The Innovation Gateway Plan

The purpose of this document is to summarize the proposed funding plan for the Innovation Gateway Plan (the Plan). This memorandum, along with other technical memoranda, will be used to document the details of the Plan.

# John Day's Innovation Gateway Plan

John Day's Innovation Gateway Plan seeks to coordinate redevelopment of a 90 acre+ riverfront property to function as a gateway and focal point for the community. As shown in Exhibit 1, the Innovation Gateway Plan study area is anchored by the former Oregon Pine mill site to the west, and then follows the John Day River to the east (upstream) and into the central part of John Day.

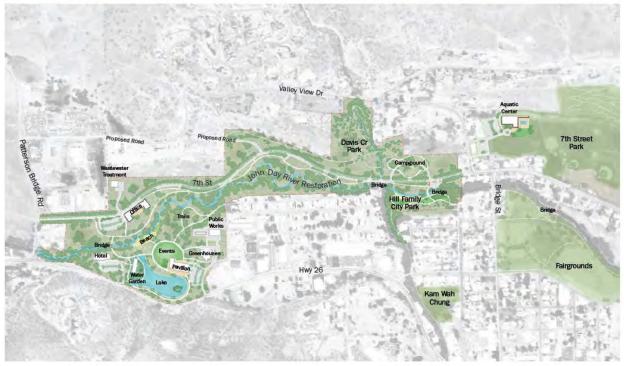


Exhibit 1. Oregon Pine/Innovation Gateway Concept Plan

Source: Walker Macy

Specifically, the Plan focuses on the redevelopment of the Oregon Pine mill property and new development on adjacent City-owned land. The Plan identifies new investments in local transportation systems and open space to support future redevelopment efforts. Major Plan elements include:

- The development of a new wastewater treatment plant an innovative \$14 million facility that will reuse and repurpose 100 percent of the city's solid and liquid waste. Reclaimed water will be tied into other new investments such as greenhouses focused on local food production and new city parks.
- An extension of 7<sup>th</sup> Avenue that runs along the north side of the John Day River and includes connections to key investment areas.
- A redevelopment concept for the **Oregon Pine** site that incorporates park spaces, access to the river, greenhouses that will grow food for local consumption, a planer shed that will be redeveloped into a community space, and identified areas for future office, hotel, and employment uses.
- A restoration of the John Day River and creation of a linear riparian area park.
- Connections to other areas of investment, including; the Hill Family City Park, Davis Circle Park, the 7<sup>th</sup> Street Park, the Kam Wah Chung national site, and the Grant County Fairgrounds.

# **Innovation Gateway Plan – Phasing**

The timing of capital projects and new development is reliant upon funding. Currently, the City has identified three phases of development for the Innovation Gateway Area and associated investment areas<sup>1</sup>.

- Phase 1. The first phase is almost complete. The first phase featured property
  acquisitions and land assembly activities along the John Day River, environmental site
  assessments, initial fundraising of \$1.5 million from multiple sources. Key sources
  included securing ODOT Transportation Growth Management (TGM) and Economic
  Development Administration grants that funded the Innovation Gateway Area Plan and
  the Community Investment Strategy. Phase 1 culminates in October of 2019 with the
  City Council adoption of the Innovation Gateway Plan.
- Phase 2. The second phase will feature development of the new wastewater treatment plant (scheduled completion in 2021) and transportation infrastructure in the western portion of the area, including an extension of 7<sup>th</sup> Street, connections and/or enhancements to connector roads. In addition, this phase includes new trails (concrete and gravel), parking areas, interpretive overlooks, a pedestrian bridge, and enhancements to the current bridge located at the Oregon Pine Mill site.
- Phase 3. The third phase will feature an extension of 7<sup>th</sup> Street in the eastern portion of the city, the creation of a 3<sup>rd</sup> Avenue bridge, and utility and infrastructure improvements to the industrial area within the Innovation Gateway. If funding is available, Phase 3 will also feature river restoration activities along the John Day River.

<sup>&</sup>lt;sup>1</sup> As advised in the scope of work for this project, this memorandum is directly focused on funding for the Innovation Gateway Plan transportation improvements. There are investment areas in John Day that are inter-related and tie into the Innovation Gateway. Most prominent among these areas is the Riverfront Recreation Area—a series of properties along the John Day River to the east and south of the Oregon Pine site. While we do not detail the investments in those areas in this document, their connections to, and relationship with, the Innovation Gateway is crucial to the success of investments in both areas.

# **Innovation Gateway Plan – Funding and Finance**

## **Funding and Implementation Approach**

The City of John Day has taken the lead to promote the local area and organize economic development activities. However, the City has severely limited financial resources. To overcome this constraint, the City has initiated a project funding and implementation approach that relies on strong partnerships with sponsors and partners at the local, regional, and national level.

The City has been successful in acquiring funds for new investment. In fact, the City has already secured funds and invested in multiple Phase 1 projects; over \$1.5 million in public funding has been spent on activities aimed at reinvigorating the local economy. These recent investments include the following:

- The Oregon Pine property purchase and greenhouse development were financed with \$850,000 in Special Public Works (SPW) loan through Business Oregon. Local contractors were hired to complete the greenhouse facility at a total cost of just under \$150,000 provided by the City of John Day.
- Business Oregon Brownfield Redevelopment Funds were used to complete a Phase 1 and Phase 2 environmental site assessment and remediation of the Oregon Pine property.
- The riverfront trails connecting the Oregon Pine were funded with \$191,300 from the Oregon Parks and Recreation Department (OPRD) Recreational Trails Program (RTP) grant with \$48,900 in matching funds provided by the City.
- The area development plan for the Innovation Gateway was created with \$192,000 in grant funding from the ODOT and the Department of Land Conservation and Development (DLCD) through the Transportation Growth Management (TGM) program with \$27,000 in matching funds provided by John Day.
- A community investment strategy was developed with a \$50,000 local economic opportunity fund through Business Oregon and a \$70,000 grant from the Economic Development Administration to evaluate future uses of the site and other economic investment opportunities.
- A joint investment by Business Oregon and the Oregon Water Resources Department provided \$70,000 to create the conceptual design for the new reclaimed water facility that will act as the engine to provide over 80-million gallons of reclaimed water to the community on an annual basis.
- DLCD contributed through an updated Economic Opportunities Assessment as part of the Eastern Oregon Economic Development Planning Project.

## **Roles in Economic Development**

The City sees itself in the following project funding and implementation roles:

- Lead coordinator of economic development activities;
- Steward and careful manager of local infrastructure; and,
- Supporter of business development and private investment.

The City recognizes that the successful implementation of the Plan will require a community effort and ongoing collaboration with partners. Strong partnerships already exist, and the City is working to create new partnerships and recruit project sponsors. However, not all partners will play the same role. Some partners will be expected to contribute funding, while others are primarily partners in implementation, and others may play multiple roles. Exhibit 2 provides an overview of the core partners for all of John Day's current economic development projects.

Implementing Partners: Organizations that will take a necessary and active role in project implementation	Funding Partners: Public, private, and nonprofit organizations that will support strategic investments	Dual Role Partners: Implementation and Funding
<ul> <li>City departments (public works)</li> <li>John Day / Grant County Chamber of Commerce</li> <li>John Day Canyon City Parks and Recreation district</li> <li>Grant County municipalities: Canyon City, Prairie City, Dayville, etc.</li> <li>Greater Easter Oregon Economic Development District (GEOEDD)</li> <li>North Fork John Day Ranger District (USFS)</li> <li>School District</li> <li>Regional WIB</li> <li>Eastern Oregon Regional Solutions Team</li> <li>Community organizations</li> <li>Universities</li> <li>Consultants</li> </ul>	<ul> <li>USDA Rural (broad array of funding programs)</li> <li>Business Oregon</li> <li>Oregon Department of Transportation</li> <li>U.S. Department of Commerce, Economic Development Administration (EDA)</li> <li>Oregon Department of Land Conservation and Development</li> <li>Oregon Parks and Recreation Department</li> <li>Oregon Housing and Community Services</li> <li>U.S. Housing and Urban Development</li> </ul>	<ul> <li>Grant County (economic development, roads)</li> <li>Travel Oregon</li> <li>Private businesses</li> </ul>

#### Exhibit 2. Overview of Partner Roles

Source: City of John Day, ECONorthwest

The Innovation Gateway Plan is a large-scale project that will require substantial funding. To fully fund and implement the Innovation Gateway, the City is looking to outside partners. As described in the following section, the City has identified several partner opportunities to fund the projects identified in the Innovation Gateway Plan.

## **Funding Sources Identification Process**

To identify and coordinate funding sources for Innovation Gateway Plan investments, the City of John Day worked with a consultant team and sought feedback from a Project Advisory Committee and the John Day City Council. The City and partner agencies will leverage funds from a variety of local, regional, and federal sources to fund the implementation of the Innovation Gateway Plan.

The City of John Day public and nonprofit partners have already leveraged over \$13 million dollars to fund projects across the community. These funds are being used to improve local streets, expand a runway at the airport, build a shelter for vulnerable community members, and to develop plans for everything from restoring the river to fortifying the local economy and bringing broadband to the John Day area. Of this total, about \$2.8 million dollars has gone to projects that are directly linked to the Innovation Gateway Plan. These dollars include funds from the Oregon Department of Transportation for street improvement projects, safe routes to schools planning, and the Transportation Growth Management (TGM) project that funded the Plan. Additional funds that have supported Innovation Gateway Plan efforts to date include those from the Oregon Parks and Recreation Department, Business Oregon, and from the Federal Department of Housing and Urban Development.

The City is currently seeking additional funds to restore the portion of the John Day River that passes through the community, to expand the local street network to allow for new development of homes and businesses, and to enhance their initial investments in greenhouses to enable an export scale of local food production.

The City is actively pursuing funds for Innovation Gateway projects, including the first phase of the Innovation Gateway Plan, the construction of a new wastewater treatment plant, connecting the community to broadband internet, and developing a new community aquatic center. But the City needs more funds to fully support their Strategy for Growth.

In June 2019, the City of John Day hosted a one-day Regional Economic Development Summit (REDS). The purpose of the REDS was to work with potential project funders and sponsors to identify sources of capital and partnerships to propel the Strategy for Growth projects forward. The discussions during the summit were positive, with many sponsors offering guidance on how to secure federal funding. Specifically, federal funders encouraged the City to pursue a BUILD grant. BUILD, or Better Utilizing Investments to Leverage Development, is a U.S. Department of Transportation grant that is awarded annually through a merit-based process. Formerly called TIGER, BUILD grants are tailored for investments that can leverage private investment, save on project costs, and be delivered efficiently.

In July 2019, the City of John Day submitted its BUILD grant application for the Innovation Gateway Plan. If awarded, the BUILD dollars would provide funds for a substantial portion of Plan projects. In fact, if the City receives the grant, the grant dollars would fund all of the transportation capital projects listed in Technical Memorandum #6: Revised Innovation Gateway Area Design Concepts (Exhibit 3).

#### **Additional Funding Incentives to Consider**

As described in the previous section, the City has worked with its partners to identify several potential sources of funding for the Innovation Gateway Plan's transportation infrastructure investments, parks, employment areas, recreational amenities, and housing supportive infrastructure. The City may want to consider additional funding sources, especially those that provide incentives for private developers to include project priorities as part of private investment. The following funding tools and programs could fund key Plan priorities.

- Local Improvement District
- Reimbursement District
- Seasonal Fuel Tax

For more details on some of these tools, see Appendix A.

## **Preliminary Innovation Gateway Financing Plan**

The City is seeking funds to start transportation infrastructure improvements, and to move forward with more detailed plans for parks, employments areas, recreational facilities, and a development of a new wastewater treatment plant.

Exhibit 3 below provides an overview of how the City intends to implement the transportation infrastructure projects listed in Technical Memorandum #6: Revised Innovation Gateway Area Design Concepts. All of these projects have been prioritized to take place during the first phase of the Innovation Gateway Plan. As is shown in the Exhibit, the projects on the list are identified to receive funding from the Federal BUILD grant. Additional dollars from Non-Federal source (e.g. the State of Oregon) will also be used in the both phases of development<sup>2</sup>.

<sup>&</sup>lt;sup>2</sup> The second phase for the Innovation Gateway Plan will rely on the BUILD Grant and Non-Federal fund sources.

Project	Project Cost Range Identified Funding Source		S CONTRACTOR	Phased Actions/Timeframe
Study Area Roads				
7 <sup>th</sup> Street Extension (West)	\$4,826,779	BUILD Grant	Funding acquired	Phase 2 - Implementation /near-term
7 <sup>th</sup> Street Extension (East)	\$324,182	BUILD Grant	Funding acquired	Phase 3 – Implementation /mid-term
Government Entry Road Extension	\$652,363	BUILD Grant	Funding acquired	Phase 2 – Implementation /near-term
Gateway Drive	\$684,097	BUILD Grant	Funding acquired	Phase 2 – Implementation /near-term
Johnson Drive	\$131,080	BUILD Grant	Funding acquired	Phase 2 – Implementation /near-term
Wastewater Treatment Plant Facility Access	\$165,968	BUILD Grant	Water treatment development initiated	Phase 2 – Implementation /near-term
Campground Road	\$57,000	BUILD Grant	Funding acquired and campground development initiated	Phase 2 – Implementation /near-term
Study Area Roads Subtotal	\$6,841,469			
Other new City Roads 3 <sup>rd</sup> Ave Bridge and Charolais Heights Street Ext.	\$6,257,636	BUILD Grant and Non-Federal Sources	Funding acquired and development initiated	Phase 3 - Implementation /mid-term
Charolais Heights Intersection Improvements	\$244,143	BUILD Grant and Non-Federal Sources	Funding acquired and development initiated	Phase 3 – Implementation /mid-term
Other City Roads Subtotal	\$6,501,779			
Trails and Recreations Oregon Pine-area trails	\$136,000	BUILD Grant and Non-Federal Sources	Funding acquired	Phase 2 - Implementation /near-term
Oregon Pine Bridge	\$90,000	BUILD Grant and Non-Federal Sources	Funding acquired	Phase 2 – Implementation /near-term

#### Exhibit 3. Funding for Innovation Gateway Plan Refined and Prioritized Projects<sup>3</sup>

<sup>&</sup>lt;sup>3</sup> Rounded values include contingency, contractor overhead and profit, and soft costs (at 30%)

<b>Grand Total</b> Source: Walker Macy, ECONorth	\$15,122,232			
Trails and Recreation Subtotal	\$1,778,984			
Gravel Parking Lot (Planer shed)	\$153,000	BUILD Grant and Non-Federal Sources	Funding acquired and planer shed renovation initiated	Phase 2 – Implementation /near-term
Gravel Parking lot/Trailhead	\$23,970	BUILD Grant and Non-Federal Sources	Funding acquired	Phase 2 – Implementation /near-term
Interpretive Overlooks on River	\$204,000	BUILD Grant and Non-Federal Sources	Funding acquired	Phase 2 – Implementation /near-term
Integrated Park Transportation Infrastructure	\$555,864	BUILD Grant and Non-Federal Sources	Funding acquired	Phase 2 – Implementation /near-term
Campground Trails	\$34,150	BUILD Grant and Non-Federal Sources	Funding acquired and campground development initiated	Phase 2 – Implementation /near-term
Multi-use Trail along John Day River	\$460,000	BUILD Grant and Non-Federal Sources	Funding acquired	Phase 2 – Implementation /near-term
Oregon Pine-area concrete paths	\$122,000	BUILD Grant and Non-Federal Sources	Funding acquired	Phase 2 – Implementation /near-term

# **Appendix A: Potential Additional Funding Sources - Details**

Encouraging new development and redevelopment of properties in the Innovation Gateway Area and improving infrastructure to meet the needs of new and existing users will require financial tools to fill feasibility gaps, and capital funding programs to construct infrastructure projects. Filling these gaps and financing infrastructure could take several forms and come from several sources. We suggest that the City of John Day use the following criteria when evaluating these tools:

- 1. **Economic feasibility.** This category covers everything related to creating and maintaining net revenues. We break efficiency into four subcategories: (1) revenue-generating capacity, (2) administrative costs, (3) revenue stability, and (4) revenue flexibility:
  - a. **Revenue-generating capacity** considers how much money the source can generate.
  - b. **Administrative cost** considers the portion of gross revenues that will be spent on administration. The easier it is to administer the tax or fee, the more of the gross revenue collected that will be available as net revenue for transportation projects and programs in the corridor.
  - c. **Revenue stability and predictability** considers whether the source is likely to avoid large fluctuations each year and whether the source is likely to be close to the forecasts analysts might make.
  - d. **Revenue flexibility** considers limitations on the types of projects that can be funded with a given source. A funding source may be a little less useful to jurisdictions if its use is limited to certain types of projects.
- Political acceptability. Will stakeholders accept or support the tool? Political acceptability considers whether elected officials and the public at large are likely to support the funding source. This depends to a large extent on the efficiency components described above: if a revenue source is legal, efficient, and fair, then it should get political support from the public, advisory groups, and decision makers. For this analysis, we evaluate whether a source is politically acceptable using two approaches: (1) is the source widely used elsewhere in Oregon? And (2) does the source collect revenue mostly from non-locals (as opposed to local residents)?
- 3. **Fairness.** In the context of infrastructure funding, the key question related to fairness is "who pays?" A standard definition of fairness in public finance, especially relating to transportation infrastructure, is that the charges that fund the infrastructure system are tied to the users who receive benefits from (or impose costs on) the system. Fairness may also be referred to as equity.
- 4. **Legality.** All the benefits of a funding source are moot if the source is not legal or cannot become legal within the desired timeframe. If the source is currently prohibited by State statute, then there is a very big administrative hurdle to be surmounted up front.

Using the above criteria, ECONorthwest narrowed the range of potential funding tools that the City of John Day might want to consider to a list summarized in the tables below.

## Local Improvement District (LID)

Summary	Enables a group of property owners to share the cost of a project or infrastructural improvement.					
How It Works	A special assessment district where property owners are assessed a fee to pay for capital improvements, such as streetscape enhancements, underground utilities, or shared open space. For residential property, the estimated assessment cannot exceed the pre-improvement value of the property based on assessor records.					
	An ordinance must be passed through a public hearing process which must be supported by a majority of affected property owners. Part of this process includes an estimation of the improvement costs and the portion of those costs in which property owners will be responsible to pay for. The public hearing process allows for LIDs to be challenged by property owners.					
	The City collects the funds and regardless of whether the actual cost is greater than the estimated cost (on which the assessment was based), the City may make a deficit assessment for the additional cost, which would be prorated among all benefitted properties. Another public hearing would be held, in the event that an additional assessment were placed with property owners (due to underestimation).					
Fund Sources	LID bonds are backed by revenue committed by property owners (which can be public as well as private). Property owners can obtain low-interest financing through the City.					
Benefits	Politically acceptable: Organizes property owners around a common goal.					
	Economic feasibility:					
	<ul> <li>Allows property owners to make payments over time to bring about improvements quickly that benefit them individually.</li> </ul>					
	<ul> <li>Improvements within smaller areas can enhance catalytic and redevelopment value of th area.</li> </ul>					
	• LIDs can be bundled with other resources such as TIF.					
Drawbacks	Equity Challenges:					
	<ul> <li>Setting up fair LID payments for various property owners, who are located different distances from the improvement, is challenging.</li> </ul>					
	<ul> <li>Small geographic areas may not have sufficient LID revenues to support bonds for the desired improvement.</li> </ul>					
	Administrative Challenges:					
	<ul> <li>Some lenders insist that LIDs be paid off when properties are transferred.</li> </ul>					
	<ul> <li>LIDs require significant coordination, attention to detail, and administrative time to manage.</li> </ul>					
	Limited Scope:					
	<ul> <li>LIDs only address the financing of improvements to existing infrastructure rather than construction of new infrastructure.</li> </ul>					

## **Reimbursement District**

Summary	Provides reimbursement from one party that benefitted from an infrastructural improvement made and
	paid for by another party.

How It Works	A Reimbursement District is a cost sharing mechanism, typically initiated by a developer. The purpose is to provide a reimbursement method to the developer of an infrastructure improvement, through fees paid by property owners at the time the property benefits from the improvement. A developer applies to create a Reimbursement District by demonstrating benefit to properties beyond their own. In addition, the size of the improvement must be measurably greater than would otherwise be ordinarily required for the improvement				
	Eligible Reimbursement District projects typically include (but are not limited to) construction of or connections to a sewer, water, storm water or street improvements. Applications typically include: a fee sufficient to cover the cost of administrative review, a description of the project, properties that would be impacted, and a detailed methodology and calculation of how the estimated costs would be reimbursed by payments from benefitted properties over a specified timeframe. A report from the City Engineer is generated in review of the submitted application. After a public hearing process, the council will approve, reject or modify the proposal. The approval of a Reimbursement District results in a resolution and distribution of notice among benefitted properties before construction can begin.				
	Benefitted properties must pay the Reimbursement Fee when they make a physical connection to the improvement (or in the case of a sewer project, when the benefitted property creates an impervious surface that drains into the public sewer) within the Reimbursement District Area. Reimbursement fees are collected by the City and are distributed to the developer for the duration of the Reimbursement District, which are typically 10-15 years.				
Fund Sources	Paid by benefitted properties at the time the property benefits from the improvement, typically at connection to the sewer, water or storm drain system.				
Benefits	Economically feasible:				
	<ul> <li>Encourages development by ensuring compensation to installer.</li> </ul>				
	<ul> <li>Developer/applicant/installer fronts the cost and is paid back over time by any/all propert owners who initiate their benefit to the improvement, regardless of their acquisition of the property</li> </ul>				
	o It mitigates the cost of financing public improvements that the City would typically incur.				
	Politically Feasible:				
	<ul> <li>They generally work best when a developer or property owner would be highly motivated to construct a particular segment of infrastructure, for example, when one segment of infrastructure serves a large development parcel or parcels, and that infrastructure is necessary to allow development to occur.</li> </ul>				
	<ul> <li>Benefitted property owners arguably have an option to connect to the infrastructure improvement and incur the Reimbursement Fee.</li> </ul>				
Drawbacks	Administratively Challenging:				
	• Reimbursement Districts require City staff involvement to review the application, conduct an engineering report, make a decision through a public hearing process, and collect Reimbursement Fees for the duration of the Reimbursement District agreement term.				

## Seasonal Gas Tax

Summary	Generates revenue that can be used to fund public provisions, services, or projects by taxing gas consumption.					
How It Works	Tax is imposed on the sale per gallon of gasoline sold in jurisdiction over the course of a pre- determined length of time. The geography must be large enough to capture an adequate amount of fees. Typically, gas taxes are enacted at the county level. Seasonal gas taxes, that is those that are enacted for only part of the year, usually during peak visitor times, enables higher collection rates while reducing the tax burden of local residents.					
Fund Sources	Sales tax paid by gasoline consumer.					
Benefits         • Economic Flexibility:           o         Revenues can be directed to specific maintenance and new transportation projection						

Drawbacks	<ul> <li>Politically Challenging:</li> <li>Requires public vote.</li> <li>Economically Challenging:</li> </ul>
	<ul> <li>Takes time and money to run a campaign for public vote.</li> </ul>



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P18194-000

# MEMORANDUM

DATE:	September 16, 2019
TO:	John Day Innovation Gateway Area Plan Team
FROM:	Kevin Chewuk, DKS Associates
SUBJECT:	<b>Technical Memo #8:</b> Implementation and Transportation Funding

This memorandum documents required amendments to the John Day Transportation System Plan (TSP) to implement the Innovation Gateway Area Plan (Concept Plan) and identifies potential funding sources for proposed transportation improvements in the Concept Plan study area.

# **Proposed Concept Plan**

The proposed Concept Plan is shown in Figure 1. Most of the study area north of the John Day River is zoned for industrial use, while most of the area south of the river is zoned for general commercial use. The proposed Concept Plan includes offices, a wastewater treatment plant, parks and open space and a campground north of the river, and a hotel, parks and open space, public works facility and greenhouses south of the river. These uses are generally allowed under the City's Development Code with the current zoning, except for the campground in the General Industrial zone and the hotel in the General Commercial zone. An overlay zone is proposed for the concept plan area to allow these expanded uses.





## Figure I: Proposed Concept Plan

## Land Use and Motor Vehicle Trip Generation Assumptions

Land use is a key factor in developing a functional transportation system. The amount of land that is planned to be developed, the type of land uses, and how the land uses are mixed together have a direct relationship to the expected demands on the transportation system. Understanding the amount and type of land use is critical to maintaining or enhancing transportation system operations.

Estimates of future vehicle trips generated by the Concept Plan area were determined by applying trip generation rates to land use types. Vehicle trips for the Concept Plan area were estimated for two scenarios:

- Existing Zoning this assumes land use permitted under existing zoning without the proposed overlay. This scenario includes the office space, public works facility, and parks and open spaces permitted outright with the existing zoning. In addition, a permitted use is assumed to replace the proposed non-permitted campground in the General Industrial zone (assumed to be replaced with a light industrial facility) and the non-permitted hotel in the General Commercial zone (assumed to be replaced with a shopping center).
- Existing Zoning with Proposed Overlay this scenario assumes the existing zoning with the proposed zoning overlay. The proposed overlay for the Concept Plan area will allow the proposed campground in the General Industrial zone and the proposed hotel in the General

# DKS

Commercial zone. This scenario includes the office space, public works facility, and parks and open spaces assumed in the Existing Zoning scenario, in addition to the proposed campground and hotel.

Overall, the Concept Plan area with the proposed zoning overlay is expected to generate about 158 motor vehicle trips during the weekday p.m. peak hour, or roughly 8 more than what was assumed with existing zoning without the proposed overlay (see Table 1). With the recommended improvements in place (as documented in Technical Memorandum #7), development of the Concept Plan area with the proposed zoning overlay would not significantly impact the transportation system.

Table I: Vehicle Trip Estimates for the John Day Innovation Gateway Area Plan							
			Forecasted Weekday PM Peak Vehicle Trip Generation*				
Land Use	ITE Land Use	Size					
			In	Out	Total		
Existing Zoning							
Office	Code 710 (General Office Building)	42,000 square feet	7	41	48		
Public Works Facility	Code 712 (Small Office Building)	7 employees	3	5	8		
Parks and Open Spaces (i.e., pavilion, lawn, open space, beach, trails, water garden, greenhouses)	Code 411 (Public 90 acres** Park)		15	13	28		
Light Industrial	Code 110 (General Light Industrial)			28	32		
Shopping Center	Code 820 (Shopping Center)	15,000 square feet	27	30	57		
	Total for E	xisting Zoning	53	97	150		
Proposed Zoning Overlay							
Office	Code 710 (General Office Building)	42,000 square feet	7	41	48		
Public Works Facility	Code 712 (Small Office Building)	7 employees	3	5	8		
Parks and Open Spaces (i.e., pavilion, lawn, open space, beach, trails, water garden, greenhouses)	Code 411 (Public Park)	90 acres**	15	13	28		
Campground	Code 416 (Campground/	50 campsites	9	5	14		

## Implementation and Transportation Funding | Page 3



Table I: Vehicle Trip Estimates for the John Day Innovation Gateway Area Plan							
Recreation Vehicle							
	Park)						
Hotel	Code 310 (Hotel)	100 rooms	31	29	60		
	Total for Proposed Z	oning Overlay	65	93	158		
Change (Proposed Zoning Overlay – Existing Zoning) +12 -4 +8							
*Based on ITE Trip Generation Manual, 10th Edition							

\*\*90 acres of park space assumed as a conservative estimate

# **Street Functional Classification**

The proposed street system modifies some of the classifications of the John Day Transportation System Plan (TSP). Given the City's standards, the estimation of traffic volumes on area streets and overall circulation needs, recommended classification/reclassification is as follows:

- W. Main Street will continue to serve as an Arterial Street
- Patterson Bridge Road, Bridge Street and the proposed Government Entry Road will continue to be Collector Streets
- 7<sup>th</sup> Street is recommended to be a Minor Arterial Street, modified from a Collector Street in the TSP
- Gateway Drive and Johnson Drive are newly identified streets that were not in the TSP and are recommended as a Collector Street and Local Street, respectively

# Street Design

The John Day TSP includes the typical street cross sections for the City. However, the Concept Plan recommends different street standards within the plan area to better fit the vision. Several future streets need additional design treatments or are envisioned to be different from the typical cross sections. They are as follows:

## W Main Street between Johnson Drive and Patterson Bridge Road

W Main Street between the proposed Johnson Drive and Patterson Bridge Road is constrained by upward slopes on the south side of the highway. The south side of the highway along this segment also has no development potential and is recommended to include a sidewalk on the north side only (see Technical Memorandum #7 for the proposed street cross section). The sidewalk on the north side is recommended to be wider (8 feet versus 6 feet in the HDM standard)



and include a four-foot buffer. A left turn lane is only recommended at the proposed hotel driveway, Screech Alley and Patterson Bridge Road intersections with W Main Street. Otherwise, the center turn lane can be narrowed at mid-block locations as feasible.

#### 7<sup>th</sup> Street

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for an Arterial would require 7<sup>th</sup> Street to include at least 62-feet of right-of-way. This includes two 14-foot travel lanes and a 12-foot center turn lane, two 5-foot bike lanes and a 6-foot sidewalk on each side.

Narrower travel lanes are recommended along 7th Street (11 feet versus 14 feet) to encourage slower vehicular travel speeds. Given the relatively slow vehicular speeds expected, it is recommended to include sharrows instead of bike lanes. In addition, no center turn lane is recommended given the lack of driveways and slow travel speeds. A sidewalk is recommended on only one side of 7th Street given the proposed multi-use path that will parallel the roadway (see Technical Memorandum #7 for the proposed street cross section).

#### Gateway Drive

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for a Collector would require Gateway Drive to include at least 40-feet of right-of-way. This includes two 11-foot travel lanes, two 5-foot bike lanes and a 6-foot sidewalk on one side.

Wider landscape strips are recommended to enhance the pedestrian experience and allow for occasional on-street parking. No bike facilities are recommended since facilities are recommended on adjacent routes (i.e., 7<sup>th</sup> Street, Patterson Bridge Road and Government Entry Road) (see Technical Memorandum #7 for the proposed street cross section).

#### Government Entry Road

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for a Collector would require Government Entry Road to include at least 40-feet of right-of-way. This includes two 11-foot travel lanes, two 5-foot bike lanes and a 6-foot sidewalk on one side.

Since Government Entry Road is expected to have low traffic volumes, slow vehicular speeds and given rural nature of the surrounding uses, it is recommended to include shoulders in place of bike lanes and a sidewalk for pedestrian and bike travel (see Technical Memorandum #7 for the proposed street cross section).



## Johnson Drive

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for a Local Street would require Johnson Drive to include at least 40-feet of right-of-way. This includes two 10-foot travel lanes, an 8-foot parking lane and a 6-foot sidewalk on one side.

Parking is proposed to be provided in lots surrounding Johnson Drive, so no on-street parking is recommended. In addition, a network of walkways and trails is recommended just to the west of Johnson Drive that would connect to recommended sidewalks on W Main Street. Therefore, no sidewalks are recommended along the alignment (see Technical Memorandum #7 for the proposed street cross section).

## Patterson Bridge Road

The current street design standards in the John Day TSP (amended by the 2009 John Day Local Street Network Plan) for a Collector would require Patterson Bridge Road to include at least 40-feet of right-of-way. This includes two 11-foot travel lanes, two 5-foot bike lanes and a 6-foot sidewalk on one side.

Given the relatively slow vehicular speeds expected along Patterson Bridge Road, it is recommended to include sharrows instead of bike lanes and a slightly narrower sidewalk on one side (5 feet versus 6 feet) (see Technical Memorandum #7 for the proposed street cross section).

# **Pedestrian Facilities**

Pedestrians approaching and traveling within the project site will be able to safely and efficiently walk between destinations using a proposed system of sidewalks, multi-use paths and trails. As a primary pedestrian thoroughfare, an improved W Main Street section will include a continuous sidewalk on the north side from downtown John Day to Patterson Bridge Road, and from downtown John Day to the proposed Johnson Drive on the south side. No sidewalk is recommended on the south side west of the proposed Johnson Drive due to the lack of adjacent development and topography constraints. A network of walkways and trails is recommended just to the west of Johnson Drive that would connect to recommended sidewalks on W Main Street at the Johnson Drive intersection. In addition, curb ramps are recommended at each intersection crossing along W Main Street to bring them into Americans with Disabilities Act (ADA) compliance.

The proposed 7<sup>th</sup> Street extension will serve as the primary pedestrian route north of the John Day River. It will provide a continuous pedestrian connection between Patterson Bridge Road and Bridge Street. This street is proposed to include a 5-foot sidewalk and a multi-use path will parallel the roadway on the south side.



A 5-foot sidewalk is recommended on the east side of Patterson Bridge Road between W Main Street and the proposed Government Entry Road. The proposed Gateway Drive will also provide a local walkway linking the proposed 7<sup>th</sup> Street with the proposed Government Road extension. This street is proposed to include a 5-foot sidewalk on the north side. Government Entry Road is also proposed to include a 5-foot shoulder on each side for pedestrian travel from Patterson Bridge Road to Valley View Drive.

The proposed multi-use path on the south side of the proposed 7<sup>th</sup> Street will provide a primary walkway along the John Day River. This multi-use path will provide for convenient and comfortable travel and recreation between Patterson Bridge Road, the Oregon Pine and Innovation Gateway areas and the proposed Aquatic Center and existing multi-use pathway network within 7<sup>th</sup> Street Park, east of Bridge Street. In addition, a proposed network of multi-use paths and trails will link the proposed multi-use path along the John Day River with Hill Family City Park, Davis Creek Park and Campground, Oregon Pine and Innovation Gateway areas and Valley View Drive.

An improved Oregon Pine Bridge and a bridge adjacent to Hill Family City Park will provide new pedestrian crossings of the John Day River, in addition to the existing crossings at Patterson Bridge Road and Bridge Street. This network of river crossings will provide shorter block lengths for the pedestrian system, will increase pedestrian access to destinations, and will also provide a recreational loop trail.

Safe and comfortable pedestrian crossings will be provided where facilities cross streets. This will include curb extensions and marked cross-walks where appropriate. A pedestrian wayfinding system for the site and the entire downtown area should also be developed.

# **Bicycle Facilities**

The proposed 7<sup>th</sup> Street extension will serve as a local bikeway, serving those traveling from downtown John Day and the neighborhoods to the north and east. Given the relatively slow vehicular speeds along the proposed street, bicyclists will share travel lanes with vehicular traffic. It is recommended to include sharrows to alert drivers to share the street and be designed to allow bicyclists to travel outside of the door zone of parked vehicles.

In addition, cyclists can travel along the proposed multi-use path along the John Day River between the Oregon Pine and Innovation Gateway areas and Bridge Street. A potential link to Hill Family City Park and 7<sup>th</sup> Street Park will also connect the site with the multi-use path networks in these parks. This multi-use path will provide for convenient and comfortable bicycle travel between the Oregon Pine and Innovation Gateway areas and Bridge Street.



W Main Street will serve as the primary bikeway south of the John Day River. It is recommended to include 6-foot bike lanes between downtown John Day and Patterson Bridge Road. The recommended bike lanes will also enhance the Old West Oregon Scenic Bikeway through the project area.

The proposed Johnson Drive will connect cyclists from the bike facilities on W Main Street to the Oregon Pine and Innovation Gateway areas. Since it will be a local street and expected to have low traffic volumes and slow vehicular speeds, cyclists will share the travel lanes with vehicular traffic.

Government Entry Road is proposed to include a 5-foot shoulder on each side for bike travel from Patterson Bridge Road to Valley View Drive. Sharrows are also recommended on Patterson Bridge Road between W Main Street and the proposed Government Entry Road.

In addition, it is recommended that bike racks and bike storage zones be incorporated in strategic locations along streetscapes and within future development to encourage bicycle use. A bicycle wayfinding system for the site and the entire downtown area should also be developed. This system could also consider routing to mountain biking opportunities, to enhance the city's reputation for the sport.

# **Transit Facilities**

The Concept Plan sets the stage for future transit, recognizing that the type and extent of service improvements will play out over time. Specifics of transit service will depend on the actual rate and type of development, City and County resources and policies, and consideration of local options. Bus pull-outs are recommended in strategic locations along the proposed 7<sup>th</sup> Street alignment to serve future bus service. The on-street sidewalk and multi-use pathway network will connect transit users from these facilities to other key destinations. Within reasonable proximity to the project site, pedestrians and cyclists can also access the existing bus stops on W Main Street.

The project site will be served by high quality pedestrian/bicycle connections. A network of river crossings will provide shorter block lengths and is oriented towards pedestrian and bicycle users, with active and inviting pathways and public walkways on both sides of the river. It is the intended that the project site will also include a supportive mix of uses and amenities for encouraging transit ridership. Future bus stop locations should also include necessary infrastructure (e.g., shelter, bench, signage) to encourage transit ridership.

# **TSP** Amendments

The following provides a summary of the recommended amendments to the John Day TSP resulting from the Innovation Gateway Area Plan.



- The City should adopt the modified or new transportation system improvements, shown in Figure 2 at the end of this document and in Table 1 of Technical Memorandum #7, to replace the projects in the TSP for the Concept Plan area.
- These recommended street functional classifications for the Innovation Gateway Area should update the classifications in the TSP:
  - o Maintain classification of W. Main Street as an Arterial Street
  - o Maintain classification of Patterson Bridge Road as a Collector Street
  - o Maintain classification of Bridge Street as a Collector Street
  - o Maintain classification of the proposed Government Entry Road as a Collector Street
  - o Reclassify 7th Street as a Minor Arterial Street
  - o Classify Gateway Drive as a Collector Street
  - o Classify Johnson Drive as a Local Street
- The City should modify typical street cross sections for the streets in the Innovation Gateway Area Plan area, including for W Main Street, 7th Street, Gateway Drive, Government Entry Road, Johnson Drive and Patterson Bridge Road. This would require an amendment to the City's street design standards included in the TSP.

# **Potential Transportation Funding Sources**

New transportation funding options include local taxes, assessments and charges, and state and federal appropriations, grants, and loans. Factors that constrain these resources, include the willingness of local leadership and the electorate to burden citizens and businesses with taxes and fees; the portion of available local funds dedicated or diverted to transportation issues from other competing City programs; and the availability of state and federal funds. The City should consider all opportunities for providing or enhancing funding for the transportation improvements included in the Concept Plan.

Counties and Cities have used the following sources to fund the capital and maintenance aspects of their transportation programs. As described below and summarized in Table 2, they may help to address existing or new needs identified in the Innovation Gateway Area Plan.



Table 2: Potential Transportation Funding Options						
Funding Option	Allowed Use of Funds	Existing or New Funding Source	Action Required to Implement	Example Charge	Potential Additional Annual Revenue	
System Development Charge	Capital improvements	New	City Council action	\$500 per peak hour trip for new development	\$10,000	
Transportation Utility Fee	Capital improvements or maintenance	New	City Council action	\$1 per month for residential units and \$.01 per month per square foot for non-residential uses	\$100,000	
Local Fuel Tax	Capital improvements or maintenance	New	Voter Approval	One cent per gallon	\$12,000	
County Vehicle Registration Fee	Capital improvements or maintenance	New	Voter Approval (County- wide)	\$18 for passenger cars, and \$8 for motorcycles per year	\$11,000	
Property Tax Levy	Capital improvements or maintenance	New	Voter Approval	\$0.20 per \$1,000 in assessed value (per year, for 5 years)	\$20,000 (per year, for 5 years)	
Transient Room Tax	Capital improvements or maintenance	New	City Council action	n/a	n/a	
Local Improvement Districts	Capital improvements	New	Affected Property Owners	n/a	n/a	
Debt Financing	Capital improvements	New	Varies	n/a	n/a	

## **Transportation System Development Charge**

System development charges (SDC) are fees collected from new development and used as a funding source for all capacity adding projects for the transportation system. The fee is based on the proposed land use and size and is proportional to each land use's potential PM peak hour vehicle trip generation.

The City may wish to collect an SDC for transportation facilities based on the transportation needs established. As an example, an SDC rate of \$500 per peak hour trip (and assuming similar growth as the previous years) would provide the City with \$10,000 annually. If an SDC is desired, a rate study



would be required to determine appropriate fees based on capacity projects costs, growth potential, and local preferences.

## **Transportation Utility Fee**

A transportation utility fee is a recurring monthly charge that could be paid by all residences and businesses within the City. The City can base the fee on the estimated number of trips a particular land use generates or as a flat fee per residence or business. This fee is typically collected through regular utility billing; however, it could be collected as a separate stand-alone bill. Existing law places no express restrictions on the use of transportation utility fee funds, other than the restrictions that normally apply to the use of government funds<sup>1</sup>. Some local agencies utilize the revenue for any transportation related project, including construction, improvements and repairs; however, many choose self-imposed restrictions or parameters on the use of the funds.

For every \$1.00 per month in charged rates for residential units and \$0.01 per month per 1,000 square feet of non-residential uses in the City, the City could expect to collect about \$100,000 annually. La Grande, for example, charges a flat fee of \$8 per month for all users.

## Local Fuel Tax

Twenty-five cities and two counties in Oregon have adopted local fuel taxes ranging from one to ten cents per gallon. The fuel distributers pay collected taxes to the jurisdictions monthly. The process for presenting such a tax to voters will need to be consistent with Oregon State law as well as the laws of the City. Nearby locations with a fuel tax include Sisters (three cents per gallon), Oakridge (three cents per gallon), and The Dalles (three cents per gallon).

To estimate the potential revenue generated from a local fuel tax in John Day, the monthly gallons of fuel utilized per resident was assessed in Oregon, and each of the jurisdictions where ODOT administers the local fuel taxes<sup>2</sup>. Based on this analysis, Oregon residents utilized on average around 35.89 gallons per month. Assuming the Oregon rate (35.89 gallons per resident, per month), John Day residents were estimated to utilize around 62,000 gallons of fuel per month. A local fuel tax of one cent per gallon could generate an additional \$1,000 monthly or \$12,000 annually.

<sup>&</sup>lt;sup>1</sup> Implementing Transportation Utility Fees, League of Oregon Cities.

<sup>&</sup>lt;sup>2</sup> Based on 2015 population reports compiled by the Population Research Center, Portland State University, and Taxable Fuel Distribution Reports published by ODOT, June 2016.



# **County Vehicle Registration Fee**

The State of Oregon currently requires vehicle owners to register their vehicles and then review their registration on a biennial basis. The State's biennial registration fee is \$112. In addition to the State fee, Multnomah and Washington Counties also have a vehicle registration fee. Multnomah County has a \$38 biennial vehicle registration fee and Washington County a \$60 biennial (or \$30 annual) vehicle registration fee. Vehicle registration fees for Counties in Oregon can be enacted by ordinance, but if a County has a population less than 350,000 residents (like Grant County), then the ordinance requires voter approval. Under State law, 40 percent of the collected fee must go to the Cities within a County, unless they agree to a different percentage.

Grant County has 8,304 registered passenger cars, and 280 registered motorcycles<sup>3</sup>. As an example, with a biennial registration fee of \$18 for passenger cars, and \$8 for motorcycles, the County could expect to collect over \$75,000 annually, with \$45,000 going to the County, and \$30,000 distributed to Cities, including around \$11,000 to John Day.

## **Property Tax Levy**

Property tax levies are another funding option available to Cities. Voter approval is required to enact a local option tax, and the tax may be imposed for up to five years at a time, at which time a City will need voter approval if it desires to renew the levy. The only exception is that a levy for a specific capital project may be imposed for the expected useful life of the capital project up to a maximum of 10 years. Assuming a rate of \$0.20 per \$1,000 in assessed value as a five-year levy for the City, the City could expect to collect around \$100,000 million over five years<sup>4</sup>.

## **Transient Room Tax**

Transient room taxes are taxes for temporary lodging at hotels, motels, campgrounds, and similar facilities. The City may choose to dedicate some portion of the tax to transportation projects. This tax could place more of the cost burden for the transportation improvements in the City on non-residents. Nearby locations with a transient room tax include Burns (9%), Prineville (8.5%), and Heppner (5%).

<sup>&</sup>lt;sup>3</sup> Oregon Motor Vehicle Registrations by County, as of December 31, 2018.

<sup>&</sup>lt;sup>4</sup> Based on total assessed value of property in John Day for FY 2017-18 (\$97,422,307); John Day Budget for Fiscal Year 2017-2018.



## **Local Improvement Districts**

Local Improvement Districts (LIDs) can fund capital transportation projects that benefit a specific group of property owners. LIDs require owner/voter approval and a specific project definition. Assessments against benefiting properties pay for improvements. LIDs can supply match for other funds where a project has system wide benefit beyond benefiting the adjacent properties. LIDs are often used for sidewalks and pedestrian amenities that provide local benefit to residents along the subject street. Property owners pay fees through property tax bills over a specified number of years.

## **Debt Financing**

While not a direct funding source, debt financing is another funding method. Through debt financing, available funds can be leveraged and the cost can be spread over the projects useful life. Though interest costs are incurred, the use of debt financing can serve not only as a practical means of funding major improvements, but it is also viewed as an equitable funding source for larger projects because it spreads the burden of repayment over existing and future customers who will benefit from the projects. One caution in relying on debt service is that a funding source must still be identified to fulfill annual repayment obligations. Three methods of debt financing are listed below:

- General Obligation (GO) Bonds Subject to voter approval, a City can issue GO bonds to debt finance capital improvement projects. GO bonds are backed by the increased taxing authority of the City, and the annual principal and interest repayment is funded through a new, voter-approved assessment on property throughout the City (i.e., a property tax increase). Depending on the critical nature of projects identified and the willingness of the electorate to accept increased taxation for transportation improvements, voter-approved GO bonds may be a feasible funding option for specific projects. Proceeds may not be used for ongoing maintenance.
- Limited Tax General Obligation (LTGO) Bonds Limited Tax General Obligation (LTGO) Bonds are similar to General Obligation (GO) bonds; however, they do not have to be voted on by constituents. A City pledges its general revenues to bondholders along with the utility revenues. The advantages to this option are that it does not require reserves or coverage (such as Revenue bonds) and does not require a vote.
- Revenue Bonds Revenue bonds are debt instruments secured by rate revenue. For a City to issue revenue bonds for transportation projects, it would need to identify a stable source of ongoing rate funding. Interest costs for revenue bonds are slightly higher than for general obligation bonds due to the perceived stability offered by the "full faith and credit" of a jurisdiction.



## Better Utilizing Investments to Leverage Development (BUILD) Grant

The BUILD Transportation Grant program is awarded on a competitive basis for projects that will have a significant local or regional impact. Funding provided under National Infrastructure Investments have supported capital projects which repair bridges or improve infrastructure to a state of good repair; projects that implement safety improvements to reduce fatalities and serious injuries, including improving grade crossings or providing shorter or more direct access to critical health services; projects that connect communities and people to jobs, services, and education; and, projects that anchor economic revitalization and job growth in communities.

## Statewide Transportation Improvement Program (STIP) Enhance Funding

ODOT has modified the process for selecting projects that receive STIP funding to allow local agencies to receive funding for projects off the state system. Projects that enhance system connectivity and improve multi-modal travel options are the focus. The Concept Plan prepares the City to apply for STIP funding.

## All Roads Transportation Safety (ARTS) Funding

The ARTS Program is designed to address safety needs on all public roads in Oregon. The program is data driven to achieve the greatest benefits in crash reduction and blind to jurisdiction. The available money is separated into two categories — systemic and hot spots. Systemic projects are proven, low-cost measures that have successfully reduced the occurrence of fatal and serious injury crashes and that can be widely implemented, like rumble strips on the shoulder of the road. Hot spots are identified by a higher than normal crash occurrence. These are often higher cost projects and are targeted to a specific segment of roadway or intersection. Local agencies and ODOT regions submit applications to the region ARTS representative.

## **Connect Oregon**

Connect Oregon is a competitive grant program that invests in air, rail, marine, and bicycle/pedestrian infrastructure to ensure Oregon's transportation system is strong, diverse, and efficient. Connect Oregon projects are eligible for grants that cover up to 70 percent of project costs. A minimum 30 percent cash match is required from the recipient for all grant funded projects. Projects eligible for funding from state fuel tax revenues are not eligible for Connect Oregon funding.





То:	Nick Green, City of John Day Aaron Lieuallen, City of John Day		Торіс:	DRAFT Technical Memo #9: Revised Policy Framework and Code Amendments	Date: 09/21/2019	
	Cheryl Jarvis-Smith, ODOT					
From:	Matt Hasti	e, APG	Project:	John Day Innovation Gateway	Project #: P3465	
Distribu Baseca				o #9: Revised Policy Fr ents – Final Draft	amework and	
		This memorandum builds on work completed in Technical Memorandum #8 which identified a preliminary set of amendments to the John Day Comprehensive Plan along with a set of potential options and recommendations for implementing the Innovation Gateway Plan through amendments to the City's Development Code. This memo repeats the proposed amendments to the Comprehensive Plan identified in Tech Memo #8 and provides a detailed set of proposed amendments to the Development Code in the form of a proposed John Day Innovation Gateway (IG) Overlay Zone.				
		SECTI Compr	-	In Policy Amendments		
		support	the John Day I	isting relevant Comprehensive Pla nnovation Gateway (JDIG) Plan or further support the Plan.		
		<u>General</u>	Land Use Polic	<u>cies</u>		
		The foll	owing policies a	generally support the JDIG Plan.		
		1.		pments should recognize and resp areas in which they locate.	ect the particular character	
		2.	developments	concepts and flexibility in design s in those more undeveloped section icular urban character has been su	ns of the planning area	
		These fo	ollowing revisio	on to Policy #5 is recommended to	further support the Plan.	
		5. T	regional econo	ticipation in the District OEDP Prog omic partnerships to insure maxim assistance and financing.		

John Day Innovation Gateway Technical Memo #9 – Revised Policy Framework and Code Amendments Page 2 of 13

#### **Economic Element Policies**

Many of the existing Comprehensive Plan policies generally apply to and are consistent with and supportive of the Innovation Gateway Plan. The following additional policies are recommended to further support the Gateway Plan and to incorporate guidance from the Grant County Economic Opportunities Analysis project.

- *XX.* To support continued development of the Innovation Gateway Area as a community destination and attraction that focuses on rural innovation, agri-tourism, and value creation.
- *XX.* To support investments in public and private infrastructure that will complement city-wide goals, as well as those for specific areas such as the Downtown and Innovation Gateway Area.
- *XX.* To adopt and regularly update a short list of clear, achievable economic development actions as a Community Investment Strategy that will further the economic goals of the community.
- *XX.* To support and participate in achieving the goals of the Grant County Digital Network Coalition.
- *XX.* To support facilities and programs for new home construction and urban renewal as the John Day Urban Renewal Agency.

#### Public Facilities Element Policies

The following new policy is recommended to be added to this section of the Comp Plan:

11. Conservation, reclamation and re-use of water shall be a goal of the City and shall be incorporated in future planning for public and private development.

## SECTION 2 Proposed Comprehensive Plan Text Amendments

In addition to amending the Comprehensive Plan to update or add selected policy statements, the Comprehensive Plan also should be revised to refer to the JDIG Plan as an ancillary document that provides additional information relevant to future planning for the area. The following text is recommended to be added to the narrative section of the *Urbanization* chapter of the Plan:

#### JOHN DAY INNOVATION GATEWAY AREA

In 2018-2019, the City worked with a team of consultants to prepare the John Day Innovation Gateway (JDIG) Plan. This Plan articulates a vision for the area of the City north and south of the John Day River, between approximately Patterson Bridge Road and the Downtown area. Key objectives of the JDIG Plan include:

- Integrated system of parks, trails and natural areas
- New facilities and amenities for community members and visitors, including redevelopment of the former mill site and re-use of selected structures there

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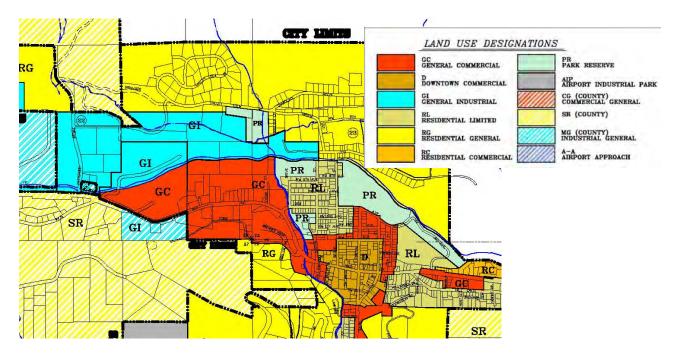
- <u>Re-use of water from a proposed new state-of-the-art wastewater treatment plant for a new</u> water garden and hydroponic greenhouses
- Roads and trails providing access and circulation within the area and to other parts of the community
- Redesign and relocation of the City's wastewater treatment plant

The JDIG Plan has been adopted as a supporting element of this Comprehensive Plan and provides further information about future plans for this area.

## SECTION 3

## **Proposed Development Code Amendments**

The study area is entirely within the city limits of John Day. A significant portion of the study area (primarily north of the John Day River) is zoned for industrial use. The majority of the project area south of the river, including a portion of the area recently annexed into the City, is zoned for general commercial use. The area southeast of the confluence of the John Day River and Canyon Creek (planned for future river recreation access) was recently rezoned to the City's Park Reserve zone to ensure consistency with current use and future plans for the property. Existing zoning designations are shown in Figure 1.



## Figure 1. Project Area Zoning

Key elements of the JDIG Plan include:

- Integrated system of parks, trails and natural areas, including:
  - Natural areas along both sides of the river for the length of the project area

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- Integration with new or existing developed park facilities on both sides, including Davis Creek Park, Hill Family Park, and others
- New facilities and amenities for community members and visitors, including:
  - Former mill structures will be renovated to host events such as farmers markets, classes, events as well as communicate the history and identity of John Day.
  - Use of water sourced from the new state-of-the-art wastewater treatment plant for a new water garden and hydroponic greenhouses.
  - Potential future office development, vendors in the Planer Shed Pavilion, greenhouse employees, and public works facilities
- Roads and trails providing access and circulation within the area and to other parts of the community
- Redesign and relocation of the City's wastewater treatment plant

Most of the City's existing Development Code provisions support implementation of these objectives. However, a number of needed code provisions are either lacking or potentially in conflict with these goals, including following, which are described in more detail in Technical Memo #8.

- Land uses. Some specific uses allowed and/or prohibited in the base zones are not consistent with the uses envisioned in the JDIG area.
- Site development standards. Most existing base zone standards appear to be reasonable and do not appear to represent a barrier to the types of uses and development proposed within the planning area. However, several specific standards are recommended to be applied differently in the project area, including building size, lot coverage, and landscaping coverage.
- Block layout, orientation and architectural design standards. Some of the standards applied in the City's commercial base zones are not appropriate for the JDIG area. Existing standards are oriented towards traditional commercial areas with a typical downtown or commercial area block layout and orientation of buildings and businesses to adjacent streets and sidewalks. The layout of these areas is expected to be based on a typical grid street layout similar to existing residential and commercial areas south of the river in John Day. However, the Innovation Gateway area will consist of a number of individual buildings or uses within a park-like setting. Similarly, existing architectural design standards also are generally oriented to a traditional commercial area, including standards related to building entrances, windows and other features. It may be challenging for some of the specific buildings or uses proposed for the Innovation Gateway to meet these standards.
- Other standards. Some additional standards are desired to address specific objectives in the JDIG area, including those related to lighting and landscaping.

Tech Memo #8 identified several options to address these goals along with a recommended or preferred option related to each topic. In each case, use of an overlay zone to establish standards specific to the JDIG area was recommended. The remainder of this memo includes the proposed text of a proposed Innovation Gateway (IG) overlay zone.

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#### Section 5-2.11. John Day Innovation Gateway (IG) Overlay Zone

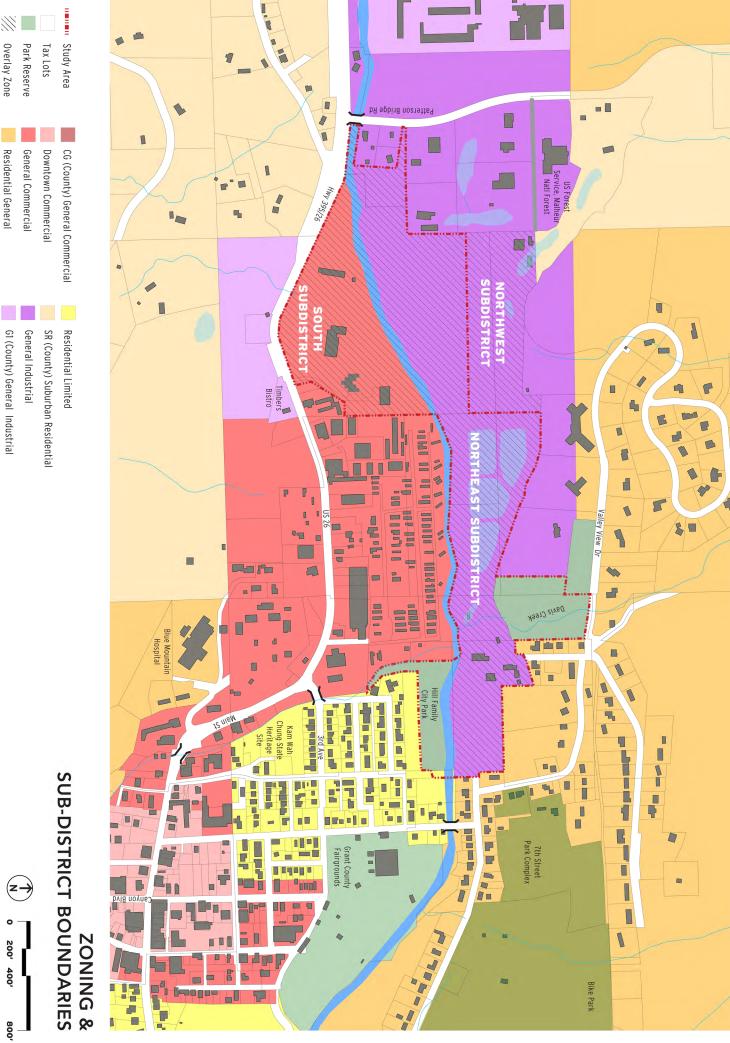
- 5-2.11. Innovation Gateway (IG) Overlay Sections:
- 5-2.11.010 Scope and Purpose of Regulations
- 5-2.11.020 Subdistricts
- 5-2.11.030 Allowed Uses
- 5-2.11.040 Limitations on Uses
- 5-2.11.050 Development Standards
- 5-2.11.060 Site Layout and Design
- 5-2.11.070 Architectural Design Standards
- 5-2.11.080 Pedestrian Amenities
- 5-2.11.090 Special Use Standards

#### 5-2.11.010 Purpose

The Innovation Gateway (IG) Overlay applies to the area planned and evaluated as part of the John Day Innovation Gateway Plan and as designated on the Zoning Map. In any zone which is a GW overlay zone, the requirements and standards of this Chapter shall apply in addition to those of the underlying zone; provided, that if a conflict in regulations or standards occurs, the provisions of this Chapter shall govern. The purpose of the IG overlay zone is to implement the recommendations of the Innovation Gateway Plan, including to create welcoming, attractive, active area on both sides of the John Day River; support establishment of a system of integrated parks and trails within and adjacent to the area; allow for an appropriate mix of commercial, industrial and institutional uses; support the City's goals associated with water reclamation and re-use; and promote use of a variety of modes of transportation to travel to, from and within the area.

#### 5-2.11.020 Subdistricts

The IG zone includes three sub-districts, the Northeast, Northwest and South sub-districts as depicted in Figure 5-2.11-1. Use permitted outright or as conditional uses vary within the sub-districts as described in Sections 5-2.11.030 and 5-2.11.040 of this chapter.



800'

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#### 5-2.11.030 Allowed Land Uses

Permitted uses are those allowed outright in the underlying base zones, with the exceptions summarized in Table 5-2.11.030.

Use	South Sub-	Northwest Sub-	Northeast Sub-
	District	District	District
Residential uses			
- New dwelling built in conjunction with	Р	N	Ν
a permitted commercial or industrial			
use located above the ground floor			
- All other residential uses			
Commercial Categories			
Drive-up/Drive-in/Drive-through	Ν	N	N
Hotels and Motels	Р	N	N
Quick Vehicle Servicing or Vehicle Repair	N	S	S
Retail Sales and Commercial Uses <sup>1</sup>	Y	S	S
Self-Service Storage <sup>2</sup>		S	N
Industrial Categories			•
Industrial Service			
- Fully enclosed	Ν		
- Not enclosed	Ν	N	Ν
Manufacturing and Production			
- Fully enclosed <sup>3</sup>	S		
- Not enclosed	Ν		
Warehouse and Freight Movement	Ν		
Waste Related		N	N
Wholesale Sales			
- fully enclosed, less than 20,000 square	Ν		
feet of floor area			
<ul> <li>fully enclosed, equal to or greater</li> </ul>	Ν	N	Ν
than 20,000 square feet of floor area			
- not enclosed	Ν	Ν	Ν
Institutional Categories			
Basic Utilities			
Community Service, except as otherwise			
allowed by Public Facilities Overlay zone			
Daycare			
Parks and Open Space			
- Consistent with the John Day	Р	Р	Р
Innovation Gateway Area Plan			
<ul> <li>Other parks and open space uses</li> </ul>	CU	CU	CU
Religious institutions/churches			
Schools			
Other Categories			
Accessory Structures (with a permitted use)			

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Agriculture – Greenhouses and similar horticulture	Р	Р	Р
Buildings and Structures Exceeding the Height Limits in Table 5-2.3.030			
Radio Frequency Transmission Facilities, including building-mounted facilities, that project above roofline			
Utility Corridors			
Temporary Uses (limited to "P" and "CU" uses), per Section 5-4.9.010.			
Transportation Facilities (operation, maintenance, preservation, and construction in accordance with the City's Transportation System Plan)			

#### Notes:

*P* = Permitted; CU = Permitted as a conditional use; S = Permitted with standards; N = No permitted; -- = Refer to base zone for use status

- 1. Uses subject to size limitations in the northeast and northwest sub-districts subject to section 5-2.11.050 of this code.
- 2. Self-storage uses must be within a fully enclosed building; access to individual storage units shall be provided from the interior of the building only.
- 3. Manufacturing and production allowed in the Southern Subdistrict only if the use includes a retail component.

#### 5-2.11.040 Limitations on Uses

[Placeholder for possible additional standards.]

#### 5-2.11.050 Development Standards

Development standards are summarized in Table 5-2.11.050.

Use	South Sub- District	Northwest Sub- District	Northeast Sub- District
Minimum Lot Area			
<ul> <li>New dwelling built in conjunction with a permitted commercial or industrial use located above the ground floor</li> </ul>	None		
- All other residential uses	None		
Minimum Lot Width (feet)	20		
Minimum Lot Depth (feet)	None		
<i>Maximum Building/Structure Height (feet)</i> Ornamental and symbolic features not exceeding 200 square feet in gross floor area	35		

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or 15% of the total building featurint			
or 15% of the total building footprint,			
whichever is less, and including towers, spires,			
cupolas, belfries, and domes, where such			
features are not used for human occupancy,			
may exceed the height limit by no more than			
10 feet.			
Maximum Building Size, Retail Commercial		5,000	5,000
Uses (square feet)			
Lot Coverage , maximum building coverage	85%	85%	85%
(Foundation plane as % of site area)			
Landscape Area (% site area). Landscape area	15%	15%	15%
may include a combination of plant beds,			
planters, and plaza space			
Minimum Setbacks (feet):			
<ul> <li>Front, Street, Side, and Rear property</li> </ul>			
lines, except garage or carport			
<ul> <li>Garage/Carport Entry, setback from</li> </ul>		NA	NA
street or vehicle accessway		NA	NA
- Alley			
Build-to-Line (feet) <sup>1</sup>	20		

Notes:

-- = Same as indicated in base zone.

1. Build-to line is measured from the edge of any adjacent public or private path or roadway providing primary access to the front of the proposed use.

#### 5-2.11.060 Site Layout and Design, Building Orientation and Commercial Block Layout

A. Purpose. The purpose of these standards is to promote pedestrian-oriented development where walking is encouraged, and to avoid conflicts between pedestrians and vehicles. Providing direct pedestrian connections between buildings and adjacent streets and pathways also encourages crime prevention, natural surveillance or security, and safety by having more "eyes-on-the-street."

B. Applicability. This section applies to projects that are subject to Site Design Review.

C. Site layout and design standards for the Northwest and Northeast Subdistricts are as described for the General Industrial base zone in section 5-2.4.060 of this code.

D. Building Orientation and Commercial Block Layout standards described in section 5-2.3.060 do not apply to the South Sub-District. Instead, the following standards are applicable to projects that are subject to Site Design Review in the South Sub-District.

1. Building Orientation Standards

a. Compliance with the setback and build-to line standards in Section 5-2.11.040, where applicable. The build-to line may be setback to provide pedestrian amenities between a building and its adjoining street or vehicular accessway.

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b. All buildings shall have at least one prominent entrance oriented to the nearest pedestrian walkway or pedestrian plaza that is intended to be used for pedestrian access to the building. The primary entrance shall not be more than 40 feet from the nearest pedestrian walkway, except to provide pedestrian amenities; a walkway shall connect the primary entrance to the sidewalk in this case.

c. Parking areas shall not be located between a proposed building and a trail adjacent to the John Day River.

2. Block Layout Standards

a. Block layout patterns shall be consistent with development concepts described in the John Day Innovation Gateway Plan.

b. Where block lengths exceed 600 feet, mid-block pedestrian pathway connections shall be provided to connect adjacent roadways. In such instances, mid-block pedestrian pathway connections shall be provided at least every 400 feet.

c. Walkways shall connect the street right-of-way to building entrances and the interior parking courts located between or adjacent to buildings, as necessary to ensure reasonably safe, direct, and convenient access to building entrances and off-street parking.

#### 5-2.11.070 Architectural Design Standards

A. Purpose and Applicability. Section 5-2.11.070 is intended to provide detailed, human-scale design that is characteristic of the City of John Day while affording flexibility to use a variety of architectural building styles. The standards also are intended to address the unique nature of the John Day Innovation Gateway Area and the objectives of the John Day Innovation Gateway Area Plan. All new buildings and major remodels within the South Sub-District shall meet the standards of subsections 5-2.11.070.B-C., which are applied through Site Design Review. The applicant demonstrates that the standards are met by complying with the criteria under each standard.

B. Pedestrian-Orientation. The design of all buildings on a site shall support a safe and attractive pedestrian environment. This standard is met when the approval body finds that all of the criteria in 1-3, below, are met. Alternatively, the approval body may approve a different design upon finding that the design contains an equally good or superior way of achieving the above standard.

1. The building orientation standards under Section 5-2.11.060 are met; and

2. Primary building entrances shall open directly to the outside and, if not abutting a street, shall have walkways connecting them to a street sidewalk or other pedestrian walkway; every building shall have at least one primary entrance that does not require passage through a parking lot or garage to gain access; and

3. Primary building entrance(s) are designed with weather protection, such as awnings, canopies, overhangs, or similar features.

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C. Human Scale. The design of all buildings shall be to a human-scale. This standard is met when the approval body finds that all of the criteria in 1-8, below, are met. Alternatively, the approval body may approve a different design upon finding that the design contains an equally good or superior way of achieving the above standard. Figure 5-2.3.080D contrasts examples of building elevations that are consistent/inconsistent with human scale criteria.

1. Regularly spaced and similarly shaped windows are provided on all building stories;

2. Ground floor retail spaces have tall ceilings (i.e., 12-16 feet) with display windows on the ground-floor;

3. Display windows are trimmed, recessed, or otherwise defined by wainscoting, sills, water tables, or similar architectural features;

4. Ground floor windows for commercial or institutional uses must allow views into ground floor working areas or lobbies, pedestrian entrances, or display areas.

5. On multi-story buildings, ground floors are defined and separated from upper stories by appropriate architectural features (e.g., cornices, trim, awnings, canopies, arbors, trellises, overhangs, or other features) that visually identifies the transition from ground floor to upper story; such features should be compatible with the surrounding architecture;

6. The tops of flat roofs are treated with appropriate detailing (i.e., cornice, pediment, flashing, trim, or other detailing) that is compatible with the surrounding architecture;

7. Pitched roofs have eaves, brackets, gables with decorative vents, or other detailing that is consistent with the surrounding architecture;

8. Where buildings with greater than 20,000 square feet of enclosed ground-floor space are proposed, they shall provide articulated facades on all elevations facing a street or primary vehicle or pedestrian accessway. This criterion is met when an elevation contains at least one of the following features for every 40 feet of building (horizontal length): windows; primary entrances; weather protection (awnings, canopies, arbors, trellises), building offsets; projections; changes in elevation or horizontal direction; sheltering roofs; terraces; a distinct pattern of divisions in surface materials; ornamentation; screening trees; small-scale lighting (e.g., wall-mounted lighting, or up-lighting); and/or similar features as generally shown in Figure 5-2.3.080.D. Note: Figure 5-2.3.080.D should not be interpreted as a required architectural style.

[Insert figures from 5-2.3.080.D or just include references to that section as needed.]

#### 5-2.11.080 Pedestrian Amenities

A. Applicability. Standards for Pedestrian Amenities found in Section 2-2.3.090 are applicable to new developments and major remodels in all sub-districts in the John Day Innovation Gateway Area.

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### 5-2.11.090 Special Use Standards or Other Requirements

A. Landscaping. All landscaping standards found in Section 5-3.2.030 of this Code are applicable to development within the overlay zone. The following additional landscaping standards also apply.

1. Landscaping within the portion of the overlay zone that lies within the Floodplain Overlay zone will be species of plants that are flood-resistant. For the purposes of this requirement, "flood-resistant" means trees and shrubs that have a high or medium flood tolerance rating as determined by the Oregon State University Extension Service.

2. Landscaping and open areas shall emphasize the use of native trees, shrubs, or other plants adapted for survival or growth in this area. Shrubs and/or living groundcover shall be planted to assure fifty percent (50%) coverage within one (1) year and ninety percent (90%) coverage within five (5) years.

B. Exterior Lighting. Exterior lighting facilities throughout the overlay zone should improve night-time public safety and security, promote energy efficiency, and avoid detrimental impacts to the environment or to public use and enjoyment of public and private property. The following standards apply to all areas:

1. Light fixtures shall be full-cutoff. When installed, a full-cutoff fixture gives no emission of light above a horizontal plane.

2. Pole-mounted lighting shall not exceed a height of 20 feet.

3. Façade lighting shall be limited to illumination from building-mounted fixtures. Up-lighting is not permitted. When installed, up-lighting emits light above a horizontal plane.

4. Pedestrian scale lighting is required for the public walkways, plazas, and courtyards within areas that are open to the public after dark.

Time: 1.30-2.30pm	Topic: PMT Meeting #1 (Task 1.1)	Meeting Date: 11/13/2018
Location: Conference Call	Project: TGM 5A-17 John Day Innovation Gateway	Project #: P3465.01

Attendees:

Ken Pirie, Walker Macy

Mike Zilis, Walker Macy

Aaron Maples, Walker Macy

Cheryl Jarvis-Smith, ODOT

Nick Green, City of John Day

Aaron Lieuallen, City of John Day

Kristen Kibler, JLA

Carl Springer, DKS

Matt Craigie, ECONW

Matt Hastie, APG

This call was the first, kickoff meeting of the core Project Management Team, or PMT, which guides the project's progress. The purpose of this meeting was to introduce the project team, hear City goals for the project and review the project scope and schedule

- Team Introductions.
- Nick and Aaron listed the many projects ongoing at the City and noted that state and federal agencies are providing substantial support. City received a significant grant from the state for trail construction and their application was ranked first in Oregon
- There is a parallel economic development project starting that we'll coordinate with, culminating in an economic development summit in early May 2019, which should include an open house for the Innovation Gateway project.
- Goal is a series of quick wins to continue a positive trajectory and galvanize the community and City Council around the future of this evolving 'frontier town' and stop population decline, while promoting the John Day brand and lengthening visitor stays.
- Negotiations with OPRD on land swap of existing City pool (close it in 2020) for an expanded Kam-Wah-Chung facility. Opsis Architects have been hired to provide aquatic/recreational facility design concepts. Walker Macy will be involved in discussions of a new aquatic/rec facility siting and design.
- Hospital bond expires in 2020, freeing up bond capacity and allowing aquatic center to be presented as a bond that does not require additional taxes for residents.
- Additional negotiations ongoing over a land exchange to provide downtown parking and services.
- Cheryl Jarvis-Smith noted an interest in amending the City's TSP to facilitate implementation and refine the City's zoning code.
- Ken Pirie outlined project scope highlights, the overall schedule and reviewed the first steps for team members, leading up to the first visit to John Day on January 8<sup>th</sup>, 2019. These steps include the first 3 Technical Memos, project branding and base mapping.

Time: 2pm-3.30pm	Topic: PMT I	Meeting #2 (Task 3.2)	Meeting Date: 1/8/2019	
Location: John Day	Project: TGM	5A-17 John Day Innovation Gateway	Project #: P3465.01	
Attendees:	The second Project Man	agement Team meeting took the fo	rm of a driving and walking	

Ken Pirie, Walker Macy

Mike Zilis, Walker Macy

Aaron Maples, Walker Macy

Tiffany Swift, Walker Macy

Cheryl Jarvis-Smith, ODOT

Nick Green, City of John Day

Aaron Lieuallen, City of John Day

Kristen Kibler, JLA

Kevin Chewuk, DKS The second Project Management Team meeting took the form of a driving and walking tour of the Study Area, beginning at the Oregon Pine mill site, touring the Greenhouse site and Public Works shop building, then the Planer Shed and Sawmill, then continuing on the north side of the John Day River to view the site of the proposed wastewater treatment plant and potential areas for river restoration on the site of the existing plant. The Site tour then visited 7<sup>th</sup> Street Park to view potential Aquatic Center sites then included a walk through Davis Creek Park to view a potential overlook location. Finally, the group viewed the future Hill Family City Park, Kam Wah Chung and Canyon Creek.

Time: 5pm-7pm	Торіс:	Combined PAC/TAC Meeting #1 (Tasks 3.3&3.4)	Meeting Date: 1/8/2019
Location: Grant Co. Airp	oort Project:	TGM 5A-17 John Day Innovation Gateway	Project #: P3465.01
Attendees:			
Ken Pirie, Walker Macy Mike Zilis, Walker	Committee (PAC/TAC introduce the consul	eting of the combined Project Advisory Comm C) which guides the project's progress. The pu ltant team, review the project schedule and go oject. (Note: The consultant team presentation	rpose of this meeting was to pals and confirm a Vision
Macy		nediately following the PAC/TAC meeting, as a	
Aaron Maples, Walker Macy		PAC/TAC Introductions.	
Tiffany Swift, Walker Macy	<ul> <li>Walker Ma</li> </ul>	acy presented an overview of the project a of the project process and schedule.	and the site, including a
Cheryl Jarvis-Smith, ODOT		ion Statement was shared and refined ve members broke into groups to discuss the	-
Nick Green, City of John Day		acy then presented their observations on including the recreational context	the John Day landscape and
Aaron Lieuallen, City of John Day	What aspe	members were asked: ects of John Day's character are most imp	portant to recognize as we
Kristen Kibler, JLA		<i>iture economic development?</i> acy then presented existing site condition	s and apportunitios including
Kevin Chewuk, DKS	a discussi	on of possibilities for John Day River restorements and the second test for John Day River restorements 1-4	
PAC/TAC members	<ul> <li>PAC/TAC r</li> </ul>	members were asked: ur vision for John Day in 2035?	
		survey was shared with the group.	
	<ul> <li>After the F</li> <li>WM<sup>+</sup></li> </ul>	PAC/TAC meeting, the following list of follo to send a sketch of Public Works facility t /discussion.	-
	•WM	and JLA to continue to coordinate with Ec may help formulate site concepts.	oNW to understand how their
		o coordinate branding/messaging with Bewth" for future community conversations.	ell+Funk, including defining
		fluve to provide graphic diagrams broadly eral options/intensity levels for river resto	
	and	fluve to provide estimate of what they can what additional budget would be needed river restoration design.	
	•WM ·	to develop initial site concepts, with team	
		to prepare Draft Tech Memo #2 and dist to send Draft Tech Memo #3 to PMT (by V	

zoning map. •WM to make updates to Draft Tech Memo #4 and send to PMT.



### January 2019 Community Survey Summary for Innovation Gateway Area Plan Process

Updated: March 6, 2019

### **Summary Overview**

An online survey was available in January 2019 to introduce the Innovation Gateway planning process to more members of the community, to solicit input on the planning objectives, and to allow community members to share thoughts about the future use of the publicly owned properties. The survey received good participation, with 62 total respondents, particularly from the 25–44 age group, who made up 66% of responses. Almost all survey respondents live and work in John Day.

### Input on Draft Objectives

Participants weighed in on objectives for the project, grouped into categories.

- Economic Opportunities: These objectives resonated with almost all of the 60 respondents to this question:
   "Grow a thriving community for our businesses and residents; and "Design a place for sustainable commerce and jobs."
- Transportation Connections: Transportation connections were indicated as a high priority, with the majority
  of the 58 respondents indicating all three objectives were important. (Note: This feedback is contrasted by
  the response to the City Functions question.)
- Vibrant "Place" for Community: All objectives were indicated as important, with "Creating a beautiful environment to enjoy throughout the year" receiving the highest response.
- **City Functions:** Generally, respondents favored a new recreation center and siting a new wastewater treatment plant over improving transportation circulation around the city.
- River and Recreation: Most people thought all river and recreation criteria were important. Building a tourism destination was indicated as the highest priority, followed closely by restoring public access to both sides of the river.
- Environment: Respondents generally saw all of the environmental criteria as important.

Additional input: Generally, comments were supportive of the effort and thought improvements in the Innovation Gateway Project Area would be a positive addition to John Day. Those with reservations wanted to make sure efforts are coordinated with other revitalization activities in John Day, including downtown. The most common theme was the efforts should not detract or compete with efforts happening downtown. The project should consider and try to address existing community needs. A more complete summary and full comments included below.

### John Day Now and in the Future

When asked to describe John Day *today* in no more than 5 words, it was clear that different people in the community have vastly different views, some very positive and others very negative. The most common theme that emerged was about **stagnancy versus change**. There were noticeable discrepancies between negative and positive comments, particularly that some felt a stagnancy/lack of progress/economic depression, whereas others saw change happening in the community and a time of revitalization.

When asked to describe in short descriptions what they would like to see included in redevelopment plans for the John Day Innovation Gateway Area, the most common words were: recreation, jobs, community, natural, and access. As with the previous question, some dichotomies appeared. **Recreation / economic development**: Recreation (including natural areas/trails/facilities) and economic development (including jobs/industry) were by far the most frequently mentioned themes. **The pros and cons of tourism**: Several comments saw tourism as a positive motivator and boon for community development; whereas some comments thought redevelopment should focus more on existing community and economy than developing tourism. A number of comments expressed the desire to maintain the community feel and culture.

Finally, participants were asked what community concerns the project should be aware of. As with the previous two questions, many respondents **self-identified with one of two groups**: those supporting the innovation gateway concept (often characterizing themselves as supportive of change and development) and those who felt the proposed changes were not the right priority or would not bring the desired effects.

### **Recreation and Transportation Habits**

In response to three questions, participants indicated:

- Most people visit the John Day River on a regular basis.
- The majority of people walk, hike or run for recreation on a regular basis.
- They do not use the People Mover transit services to move around John Day.

### Input on Draft Objectives

The first six questions focused on draft objectives for the Innovation Gateway Area Development Plan and asked participants to indicate which of the objectives they felt were important for them or the community (participants were allowed to select as many options as desired). Following is the feedback by theme.

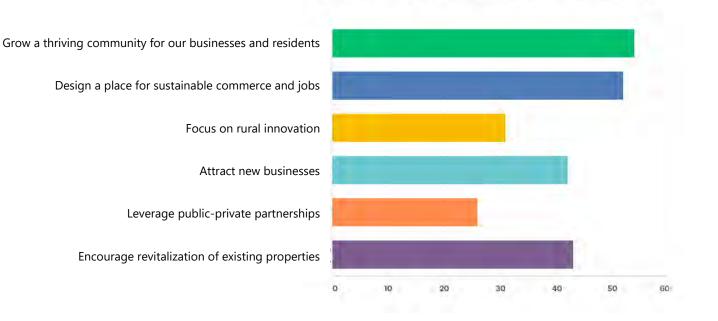
### Q1. Economic Opportunities

These objectives resonated with almost all of the 60 respondents to this question:

- Grow a thriving community for our businesses and residents (54)
- Design a place for sustainable commerce and jobs (52)

These were the least selected objectives, with around half of respondents indicating they were important:

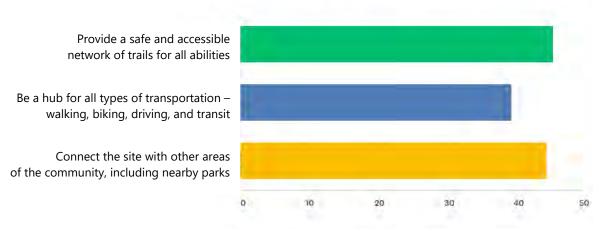
- Focus on rural innovation (31)
- Leverage public-private partnerships (26)



### Q1 Economic opportunities for our community

### Q2. Transportation Connections

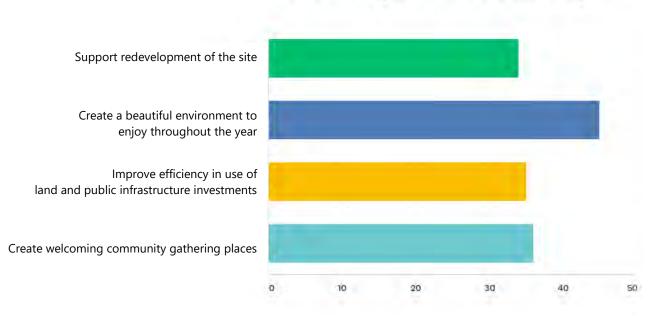
Transportation connections were indicated as a high priority, with the majority of the 58 respondents indicating all three objectives were important (45, 39, 44 respectively).



### Q2 Transportation connections for our community

### Q3. Vibrant "place" for community

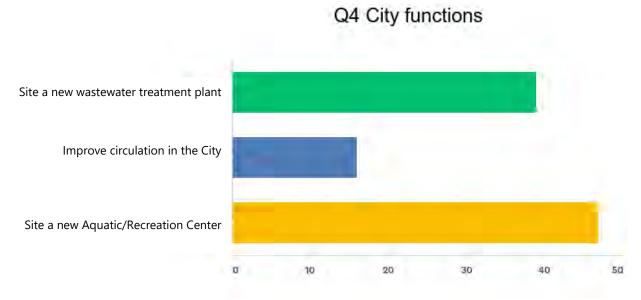
These objectives were generally seen as important. "Creating a beautiful environment to enjoy throughout the year" was indicated as the most important objective (selected by 45 out of 58 respondents), which the three other options also being selected by the majority of respondents.



### Q3 Vibrant "place" for our community

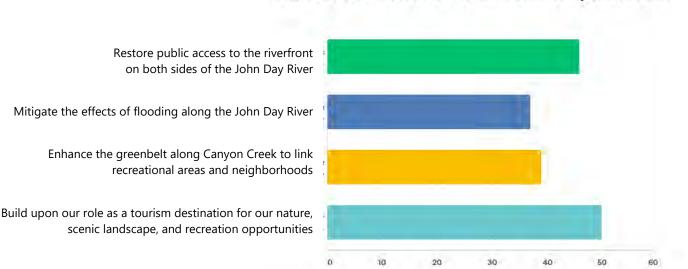
### Q4. City Functions

Generally, the 59 people who responded saw a new recreation center (47 responses) and siting a new wastewater treatment plant (39 responses) as being more important than improving transportation circulation around the city (16 responses).



### Q5. River and Recreation

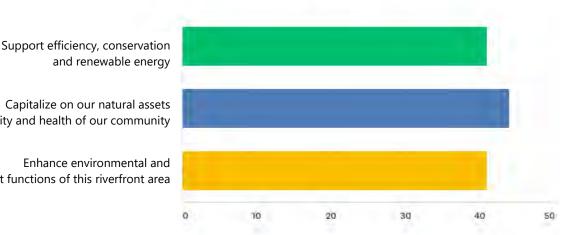
Of 59 responses, the majority of people thought all these criteria were important. Building a tourism destination was indicated as the highest priority, with 50 responses, followed closely by restoring public access to both sides of the river, with 46 responses.



Q5 River and recreation for our community and visitors

### Q6. Environment

Respondents generally saw all of the environmental crtieria as important. Of 57 responses, 41, 44 and 41 people (respectively) indicated it was important to support sustainable initiatives; capitalize on natural assets; and enhance natural and habitat areas.



### Q6 Environment

Capitalize on our natural assets for the livability and health of our community

Enhance environmental and fish habitat functions of this riverfront area

### Q7. Additional input

When asked if they anything to add to the draft objectives, input included the following:

- Generally, comments were supportive of the effort and thought it would be a positive addition to John Day.
- Those with reservations wanted to make sure efforts are coordinated with other revitalization activities in John Day, including downtown. It should not detract or compete with these efforts.
- Some comments indicated that the project should consider and try to address existing community needs • for example, recreation options and child care.
- Connections between key community centers, as well as consideration of adjacent facilities, should be considerations.
- There was a question about what modes will be allowed on trails including horses and motorized options.

Key words in responses included:



Full comments are included in the appendix.

### John Day in the Future

### Q8. Describing John Day now

Participants were asked to describe John Day *today* in no more than 5 words. The question was answered by 43 people. The most common words (shown the word cloud) were: stagnant (5); rural (4); community (4); change (3); and potential (3).

### community change Stagnant Potential rural

The most common comments were about **stagnancy versus change**. There were noticeable discrepancies between negative and positive comments, particularly that some felt a stagnancy / lack of progress / depression, whereas others saw change happening in the community and a time of revitalization.

Other comment themes included:

- Ageing community / a lot of older community members
- Rural community with home town feel and good quality of life
- Need to work together / need for help
- Rural and outdoor are key features

### Q9. Vision for redevelopment plans

Participants were asked to describe in short, one- to three-word descriptions what they would like to see included in redevelopment plans for the John Day Innovation Gateway Area. The question was answered by 45 people. The most common words (shown the word cloud) were: recreation (21), jobs (13), community (7), natural (6), access (5).

### natural community access jobsrecreation

As with the previous question, some dichotomies appeared.

- Recreation / economic development: Recreation (including natural areas/trails/facilities) and economic development (including jobs/industry) were by far the most frequently mentioned themes. Many comments listed both of these as priorities and/or implied the two could be related. At least one comment seemed portray this as an "either/or" proposition and propose that economic development should be the priority.
- The pros and cons of tourism: Several comments saw tourism as a positive motivator and boon for community development; whereas some comments thought redevelopment should focus more on existing community and economy than developing tourism. A number of comments expressed the desire to maintain the community feel and culture.

Other comment themes included:

- Desire to create connections within the community (e.g. trails)
- Desire for a variety of recreational opportunities that are appealing and accessible to all community members – including youth and families
- Desire for visual appeal for locals and tourists
- Most frequently mentioned locations/facilities: natural area, trails, recreation center, playground, park/greenspace, pool, river access
- Most frequently mentioned activities: biking/mountain biking, walking, swimming, wildlife viewing

Full comments are included in the appendix.

### Q10. Community Concerns

Participants were asked what community concerns the project should be aware of. The question was answered by 28 people.

As with previous questions, many respondents self-identified with one of two groups: those supporting the innovation gateway concept (often characterizing themselves as supportive of change and development) and those who felt the proposed changes were not the right priority or would not bring the desired effects.

Other key themes included:

- Concerns about funding for planned improvements, and what the cost would mean for John Day residents.
- Desire to scale plans to an appropriate scale for John Day.
- Desire to support current community. Want plans to enhance current John Day community and culture, not threaten it.

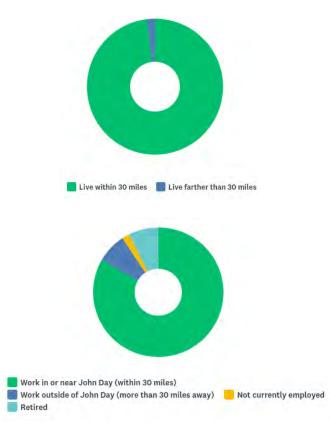
 Existing community issues need to be addressed. Some listed include: unemployment, drug use, lack of recreational facilities

A full list of comments is included in the appendix.

### **Participant Demographics**

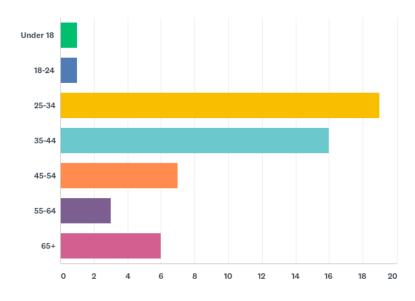
### Q11/12. Do you live and work near John Day?

The large majority of survey respondents live and work in John Day. All but one of 54 respondents to this question live within 30 miles of John Day. Of 54 respondents, 45 work within 30 miles of John Day, while 4 work farther away. 4 people indicated they're retired and one person was not currently employed.



### Q13. How old are you?

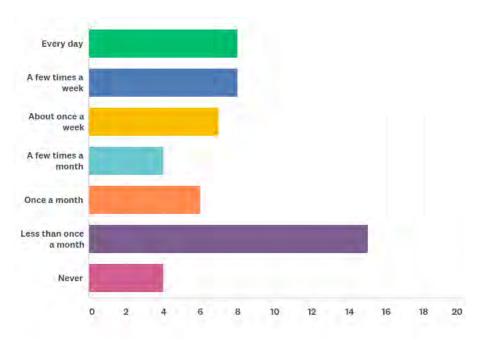
Most survey respondents were between the ages of 25 and 44 (35 out of 53 responses). There was a smaller representation from ages older than 44, and two people were 24 or younger.



### Recreational and transportation habits

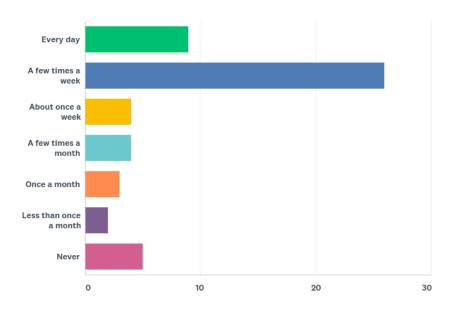
### Q14. How often do you visit the John Day River?

Overall, participants indicated they do visit the John Day River on a regular basis. Out of 52 responses, 33 people indicated they visit at least once per month, with 23 of those visiting at least once a week. 15 people indicated they visit less than once per month. Only 4 people indicated they never visit the river.



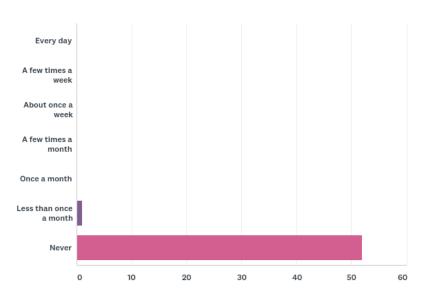
### Q15. How often do you walk, hike, or run for recreation?

Overall, survey responses were active recreationalists. Out of 53 responses, 35 walk, hike or run for recreation at least a few times a week, with 9 of those doing those activities every day. Only 5 survey responses said they never do those activities.



### Q16. How often do you use People Mover to get around John Day?

Only one person indicated they use People Mover, at a rate of less than once per month. All the rest of the 53 respondents never use the service.



### **Appendix: Full comments**

### Question 7. Further comments on draft objectives (11)

- I am concerned that this innovation gateway will pull businesses and activities away from the downtown area

   for which renovation efforts have been going on for years and about the effects on the activities and
   opportunities of the fairgrounds. Coordination is a must and should be part of the overall planning.
- Are the trails going to be open to motorized vehicles or just non-motorized vehicles. Are horses going to be allowed on the trails?
- The Farmers market needs to stay down town to help the community businesses that support the farmers market. We already have a community meeting place in the fire hall.
- Linking key community locations through trails is very important for livability!
- Almost all of these are important, some more than others. I'll support the city in all of these efforts. We should be trying to entice new employers to the area, and focus on non-extractive industries—for example, focus on knowledge economy-based businesses.
- What will best attract more tourists and residents to come to this eastern Oregon destination? More people
  equals more revenue for the city and more business for the businesses in town. Trails and recreation are an
  obvious answer for me. The John Day River is a gem, and has no city access...
- The value of the aquatic center cannot be minimalized it is irresponsible and unforgivable for us as adults to foster a generation of children brought up to love the outdoors but not provided a safe place to learn to swim in a safe protected environment, or be taught basic water safety.
- Building running trails in the basin.
- I applaud the effort! One of the first things a visitor sees when entering JD from the west is an abandoned mill site. Let's make it inviting with a greenhouse, RV park, river access, green space, etc. Very much support this!
- Waste water green house before waste water treatment plant? Hope instead of produce? Grid stress for electrical demands from projects?
- I would love to see the new aquatic / recreation building possibly incorporate some other aspects like daycare and after hours daycare, indoor/outdoor pool (so people can use it as a rehabilitation pool) maybe combine with a library so that kids in the daycare can spend time in the library, parents can go to the gym

while kids are in daycare still at the same facility. It would be amazing if it could be next to 7th street so that kids can easily go from daycare to practices etc and kids at daycare could spend time outside at the park. Have arts and craft classes etc. I think it would be great to add jobs to the community and provide healthy activities and safe care for children. If there was a daycare it would be nice to have early drop off and late pick up hours and many of us work 10 hour shifts and need to drop kids off before 7 am. Our community needs to attract working young adults. Continue to improve internet access because there are a lot of online jobs that people can do and live in our community. I think our community needs to be resourceful like the greenhouse etc.

 Oregon Pine has already built industrial power grid, use that to lure server farms like Prineville did with Facebook.

### Question 8. Describing John Day Now (43)

Full comments, grouped by tone:

Negative	Positive	Neutral / Other
dying a slow painful death	Community minded wonderful place to live	A rural retirement community.
stagnant, rear-view focused	Twilight of being a depressed community	Weaning itself off natural resource extraction
worn out. Not working together. City hall needs purchase local.	A Great Quality of Life	Rural mountain community; needs help.
Run down. Lacking inviting culture.	Rural, bike friendly, beautiful	Small town, rural
Held back, change averse	I love my home town!	Has unrealized and untapped potential
losing its hometown feel	Beautiful small outdoorsy remote quirky	Deadbut coming alive again!
Uncared for and dead	Progressing	Traditional values attempting to grow.
Out of touch	Transitioning and ripe for revitalization.	Stagnant but slowly moving forward

Oregon Meth and Welfare Hub	Potential	Dust covered blank canvas
Stagnant as much as citizenry		Still focusing on trying to log and
		farm where today it's becoming
		more about tourists
Island. Conservative. Resistant to		Potential but being dragged down.
change.		
Stagnant		Slowly changing
Old, remote, minimal services		Livable but near hospice eligible
old & crotchety		My home
Stuck in the past		
Stagnant, boring. Behind times		
Dying.		
Refusing to grow.		
Living in yesterday		
Barely hanging on		

### Question 9. Full comments (166)

Verbatim comments, grouped by respondents:

Response	Comment 1	Comment 2	Comment 3	Comment 4	Comment 5
1	Improved Health Care	Operate within its means.	More inovations from Grant County	Does not lose its culture.	ls not over populated.
2	new businesses	river access			
3	career-level jobs	non-timber industry	self-sustaining natural areas		
	Natural areas to				
4	explore	Tourism.			

Build sidewalks from 395 S to High School	renovate river bed for fish habitat	create walking paths linking to down town	new lighting west of downtown.	Have public restrooms open all year like in Prairie City.
hub for outdoor recreation and adventure tourism	hub for forest products industry	leader in agricultural innovation	community livability	small town feel
Sustainable use of natural resources	Appealing to eco-tourism	Welcoming different people	Encouraging change	
Rural Innovative Showcase	Aesthetic Natural Area	Sensitive Redevelopment	Economic Opportunities	Sustainable
Jobs	Recreation center			
jobs	schools	swimming pool	community involvement	hometown pride in schools
Educational opportunities				
Recreation	Job Creation	Tourism	Forward Thinking	
Natural areas	Recreation center	Curbside appeal		
Natural areas	Welcoming			
Non motorized recreation	Area attracting playground	Year round swimming pool/ recreation center	We	
green space	community connection	provide ecosystem services	family friendly	recreation opportunities
Great playgrounds	Low unemployment	Recreation opportunities	Welcoming	Bike friendly
Historical education Jobs	Nature walks			
	from 395 S to High School hub for outdoor recreation and adventure tourism Sustainable use of natural resources Rural Innovative Showcase Jobs Educational opportunities Recreation Natural areas Natural areas Natural areas Non motorized recreation Great playgrounds Historical education	from 395 S to High Schoolbed for fish habitathub for outdoor recreation and adventure tourismhub for forest products industrySustainable use of natural resourcesAppealing to eco-tourismRural Innovative ShowcaseAesthetic Natural AreaJobsRecreation centerjobsschoolsEducational opportunitiesJob Creation centerNatural areasWelcomingNon motorized recreationArea attracting playgroundsGreat playgroundsLow unemploymentHistorical educationLow unemployment	from 395 S to High Schoolbed for fish habitatpaths linking to down townhub for outdoor recreation and adventure tourismhub for forest products industryleader in agricultural innovationSustainable use of natural resourcesAppealing to eco-tourismWelcoming different peopleRural Innovative ShowcaseAesthetic Natural AreaSensitive RedevelopmentJobsSchoolsswimming poolEducational opportunitiesJob Creation centerTourismNatural areasWelcomingVelcoming eco-tourismNon motorized recreationArea attracting playgroundYear round swimming pool/ recreation centerNon motorized recreationArea attracting playgroundYear round swimming pool/ recreation centerGreat playgroundsLow unemploymentRecreation opportunitiesGreat playgroundsLow unemploymentRecreation opportunities	from 395 S to High Schoolbed for fish habitatpaths linking to down townnew lighting west of downtown.hub for outdoor recreation and adventure tourismhub for forest products industryleader in agricultural innovationcommunity livabilitySustainable use of natural resourcesAppealing to eco-tourismWelcoming different peopleEncouraging changeRural Innovative JobsAesthetic Natural AreaSensitive RedevelopmentEconomic OpportunitiesJobsRecreation centercommunity 

20	Community recreation center	Playground	Outdoor recreation destination	Sustainably developed	New library
21	Mtn biking hub	Forest recreation	Riverfront park and trails	Tourist support the economy	
22	Playground indoor or covered play area	Pool	Trail access to odfw USFS odf from town	Jobs	Library
23	Rec center	Trails	Innovative commerce	Tourism promotion	
24	Accessible walkways/trails/ri ver access for all abilities	Walkable community	Playgrounds	Youth activity areas	Reuse/redevelop ment
25	River access	Recreational trails	Wildlife viewing	Picnic areas	
26	Bike paths	Natural areas	Garden	Community center	
27	Recreation	Jobs	Tourism		
28	Natural areas to explore	Jobs	Community		
29	Widespread outdoor recreation	Approachable public spaces	Environmentally conscious operations	Pride in community	Healthy, active, and informed residents
30	mountain bike mecca	young active families	solid tax base	sustainable industry	demographic diversity
31	beautification	appealing	livability		
32	Solar farm	Non profit green house	Teen center	Manufacturing	

	Clean up the town. Make more	Get our internet		Keen feelier en	
	appealing for	speed up. Then people that can	Create a place	Keep focusing on the bike paths	
	tourists. And	work from home	for kids of all	and things that	
33	younger people moving here.	can move their families here	ages to enjoy year round.	bring tourists to our area.	
	moving here.	Sustainable	year round.		
34	Jobs	economy	Forward thinking!		
					Improve
05	Recreation	David	lah a	Ostanovana	community
35	aquatic center	Daycare	Jobs	Solar energy	businesses
				Activities for	
				families ie	
36	Jobs	Aquatic center	Proud of ownership	trampoline park bowling alley	Shopping
	3005	Aqualic certier	ownersnip	bowning aney	Shopping
	Recreation			Sustainable,	
	center with new		River access and	organic, local	Family friendly
37	pool	Trail systems	recreation	food sources	green spaces
	Duration				
38	Business development				
00					
	Outdoor		Sidewalk		
	Recreation	Downtown	connectivity		Recreation
39	Opportunities	Renovation	between JD/CC	Young People	Center
40	Jobs	Private industry			
	Recreation	Outdoor activity			
41	center	hub	Job growth		
42	High speed internet.	Alternative industries	Look beyond	Family sized	Industry over
42		muustnes	timber.	apartments	recreation.
				Healthy	
43	Art	Culture	Science	opportunities	
		<b>F</b> eeners's	Year round		
44	Jobs	Economic Growth	Recreation for ALL ages		
				1	Page 17 of 20

### Question 10. Full comments (28)

- The residents and 1100 plus property owners cannot resolve all the issues in Grant County nor fund all the things that are desirable but unnecessary for quality of life. Let's not be pricing out those whom have sacrificed to live in a rural atmosphere.
- People who are rear-view focused and don't see the benefit of being truly self-sufficient, while including
  interactions with the rest of the world for commerce and ways of improving.
- The city is spending more money than the population can support.
- Maintaining a clean environment in public spaces, free of debris
- Encouraging/advocating for the value of public lands, good environmental habitat for species of concern, etc. to the public.
- I hope this community always keeps that small town feel, where people know each other and any new neighborhoods will be designed to promote people to get to know each other.
- being closed-minded and closed off to change/growth
- Change is a challenge for people, so important to still maintain the character and integrity of a small town while promoting a thriving community.
- That the new and big dreams are going to break grant county we need to focus on the jobs here now and why they are closing and not try to bite off more than we can chew.
- Not losing what we do have and connecting to downtown
- We need closer 5-10 mile long trails for running and biking.
- We need a year-round recreation facility and a swimming pool! (Known concern, but would like it reiterated due to importance.)
- Daycare
- Drugs are out of control and punishment for those caught is a joke!

- It's too hard to get to the elementary school by foot or bike unless you already live on humbly street (which is nearly impossible). Walkable access should be improved.
- Noise and parking
- A loud minority that will resist any form of progress for unknown reasons
- A few squeaky wheels. Just know they exist.
- Litter, vandalism, inappropriate use, stagnation.
- Meth fueled trailer court in the center of the redevelopment area
- We're slowing becoming a place where druggie/ low income people move to. We need to clean our community up and focus on getting younger families to move here. More and more jobs can be done at home on your computer so these families can choose to live anywhere. We need to make John day that place. A cute small town that's a safe place to raise your kids. Need better internet service and more newer homes.
- Crotchety voices will be loud, but eventually they'll fade away as time marches forward
- Our community attracts a lot of unemployed state assisted residents that specifically seek out our community because of the assistance they can get and our community doesn't have enough resources to support and get these residents engaged and contributing to our community by becoming employed. It is putting more stress on our education system and using up medical resources and time. These are all things that drive younger/middle aged working families away that could contribute to our community in a more impactful way. We need to support out agricultural community because that is where are roots are from and there is a lot of importance in teaching our kids the key component of agriculture in our survival. More buildable bigger acreage lots like 5 to 20 acres need to become available for families that are interested in our community and want a little space and privacy but be closer to town. There are some elders in our community that are very opinionated and find every way to waste our time and resources complaining about the same things over and over again. While they may have some valid points, they don't speak for all of us and they tend to have all the time in the world to constantly complain and drive people out of our community because of their negativity. We need good internet and better cell coverage in outer areas.
- Monopoly no variety for people to choose from only one grocery store everything way too expensive!!!
- Local Government over committing to efforts typically accomplished by the private sector
- You gotta get some things done in order to get community buy in. Maybe think about hiring a community liaison or public relations staff member. You could use it.

- Winter street maintenance, SE Dayton street parking it's a narrow street that is overcrowded by vehicles being worked on by the automotive business. Incentive for main street business to improve the overall look of their buildings.
- More families moving away faster than what are moving in.
- Existing business owners are aging and is there a plan for new owners? We cannot afford more vacant buildings. We need more options for grocery shopping. I am concerned with this survey being available for those with computers but also realize other survey means my still not get full citizen participation. I do appreciate the opportunity to give input and fell the city manager and council are working to improve JD.

Time: 2pm-3.30pm	Topic: PMT Meeting #3 (Task 3.8)	Meeting Date: 3/5/2019
Location: Conference Call	Project: TGM 5A-17 John Day Innovation Gateway	Project #: P3465.01

Attendees:

Ken Pirie, Walker Macy

Mike Zilis, Walker Macy

Aaron Maples, Walker Macy

Cheryl Jarvis-Smith, ODOT

Nick Green, City of John Day

Aaron Lieuallen, City of John Day

Kristen Kibler, JLA

Matt Craigie, ECONorthwest This call was the third meeting of the core Project Management Team, or PMT, which guides the project's progress. The purpose of this meeting was to review draft project concepts presented by Walker Macy and Interfluve in preparation for the March 12<sup>th</sup> workshop in John Day. (Note: PMT #2 was a tour of the project study area.)

- Team Introductions.
- Walker Macy and Interfluve presented draft concepts
- Nick Green asked for the team to articulate the value of such a dramatic restoration project. Initial benefits discussed include:
  - o Increased floodwater storage and potential reduction in floodplain
  - o Future flooding should not affect new meanders created along the river.
  - Improved recreation and access to the river for residents and visitors
  - o Improved habitat for fish and other species
  - o Job creation from restoration activities
  - o Increased land values adjacent to the river
  - o An improved identity for the City
  - There should not be any adverse impacts on irrigation withdrawals from the river or on surrounding property owners.
- Interfluve was asked to provide a wide range of costs for what the restoration work entails and will provide that before the March 12<sup>th</sup> presentation as well as any major implementation logistics.
- Cheryl Jarvis-Smith asked if there was any risk to the project if the City does not move forward with dramatic river restoration—does it jeopardize other improvements? Team responded that it wouldn't really affect other proposed improvements but the overall river context wouldn't be as attractive and wouldn't leverage assets as much.
- Nick described a major challenge as coordination with DEQ (Oregon Dept of Environmental Quality) in terms of using the land currently occupied by wastewater ponds for river restoration and getting permits. But DEQ could provide some seed funding for the work and it's good that they're "at the table."
- Team discussed circulation options, including a proposed new 7<sup>th</sup> Street, which is needed to provide better access to Ironwood Estates, whose developer wants to build 50 more houses but cannot without secondary access. There will be some efficiency in building all of the new roads shown at once and will show a public benefit early, with access to the north bank of the river.

- City team advised consultants on the preferred route for 7<sup>th</sup> Street, avoiding two
  wastewater ponds and avoiding conflicts with the Iron Triangle properties. The
  extension of Government Entry Road gives Ironwood residents another access
  option and completes the grid of streets for the north and west sides of the city.
- Team presented the proposed 7<sup>th</sup> Street cross-section, with on-street parking in pockets (versus a separate parking lot). Cheryl Jarvis-Smith asked if the City would consider a 10' travel lane, instead of 11' but Nick noted that the City Council is nervous about 10', with truck and 5<sup>th</sup>-wheel traffic. However, Cheryl noted that studies have shown the narrower section to work fine with larger vehicles. The team discussed the potential to add parking pockets on the north side, potentially offsetting them so that there's only parking on one side at any given time.
- Trail concepts were presented and the City team asked for trail diagrams to include on-street routes to connect sidewalks that aren't continuous and connect people staying downtown with the river and Innovation Gateway/Oregon Pine site. The 2009 TSP Local Street Plan (Fig 4-2) shows the full network of city trails and should be consulted.

Time: 5pm-7pm	Topic: PAC/TAC Meeting #2 (Task 4.2)	Meeting Date: 3/12/2019
Location: Conference Call	Project: TGM 5A-17 John Day Innovation Gateway	Project #: P3465.01

Attendees:

Ken Pirie, Walker Macy

Mike Zilis, Walker Macy

Aaron Maples, Walker Macy

Cheryl Jarvis-Smith, ODOT

Nick Green, City of John Day

Aaron Lieuallen, City of John Day

Kristen Kibler, JLA

Matt Craigie, ECONorthwest This call was the second meeting of the combined Project Advisory Committee/Technical Advisory Committee (PAC/TAC) which guides the project's progress. The purpose of this meeting was to review project concepts presented by Walker Macy and Interfluve. (Note: The consultant team presented concepts via telephone. This meeting was combined with a presentation on economic development work by ECONorthwest, who were physically present in John Day. The same presentation was delivered to the John Day City Council immediately following the PAC/TAC meeting.)

- Team Introductions.
- Walker Macy reviewed the project schedule and goals
- Interfluve presented their analysis of existing conditions related to John Day River hydrology, including the history of gold dredging activity, recent floodplain mapping and the degraded nature of much of the river through the city.
- Interfluve then presented two river restoration concepts, differentiated by the extent to which they reshape the river with new meanders, riverbanks and trails. Precedents of other successful river restoration were presented.
- Benefits of this river restoration project could include:
  - 1. Overall increased flood conveyance is likely to reduce flood impacts to nearby infrastructure during large flood events
  - 2. New inset floodplain surfaces would have more frequent inundation to better mimic natural river conditions, and can also be utilized for multiple recreational uses
  - Potential to create constructed wetlands to reduce need for City stormwater treatment
  - 4. More recreational trails and access to natural area
  - 5. Improved fish habitat
  - 6. Additional access for fishing
  - 7. Enhanced land values for neighboring properties
  - 8. Improved visual identity for City
  - 9. No impacts to current irrigation diversion
- There were several considerations or challenges to be addressed in further study of the concept, including:
  - 1. Current wastewater ponds need analysis and permitting to be integrated with river restoration
  - 2. Past gold dredging left unstable subsurface conditions, need more geotechnical study
  - 3. Needs additional hydrologic engineering feasibility study
  - 4. City could still build the proposed improvements without river restoration, but there would be less visual and recreational benefit
- PA Committee discussion included concerns about irrigation diversions and water quality challenges from the project. Members asked about the likely duration of the restoration and Interfluve suggested this is highly variable but

that further design could clarify the timeline. Permitting will be complex but could potentially be integrated with the proposed wastewater treatment plant.

- Walker Macy summarized the existing conditions study they reviewed with the PAC/TAC in January and presented new diagrams showing the study area's interconnected parks and trail system
- Team presented a proposed new 7<sup>th</sup> Street, which is needed to provide better access to Ironwood Estates, whose developer wants to build 50 more houses but cannot without secondary access. There will be some efficiency in building all of the new roads shown at once and will show a public benefit early, with access to the north bank of the river.
- Team presented the preferred route for 7<sup>th</sup> Street, a gracefully curving parkwaytype street along the north bank of the restored John Day River, avoiding two wastewater ponds and potential conflicts with the Iron Triangle properties. Examples images were provided. Team also presented the proposed 7<sup>th</sup> Street cross-section.
- Two overall Innovation Gateway concepts were presented, showing consistent improvements at Davis Creek Park, Hill Family Park and John Day River restoration. The two concepts were primarily differentiated by the location of a proposed Aquatic Center. Detailed ideas for the former Oregon Pine site were displayed, showing options for reuse of the buildings, greenhouse access as well as riverbank access and public gathering spaces.

### CITY OF JOHN DAY CITY COUNCIL MINUTES JOHN DAY, OREGON

March 12, 2019

### **COUCILORS PRESENT:**

Ron Lundbom, Mayor Gregg Haberly, Councilor Steve Schuette, Councilor David Holland, Councilor Paul Smith, Councilor Shannon Adair, Councilor

### **COUNCILORS ABSENT:**

Brandon Smith, Council President

### STAFF PRESENT:

Nicholas Green, City Manager Monte Legg, Public Works Director Chantal DesJardin, Secretary Aaron Lieuallen, Senior Project Manager Matt Manitsas Fred Ostberg Oren Wyss

### **GUESTS PRESENT:**

Rick Hanners, Blue Mountain Eagle Logan Bagett, Elkhorn Media Group Bob Pereira, John Day Hannah Hinman, 134 NW Charolais Heights Dr. Elliot Sky, 134 NW Charolais Heights Drive Mike Springer, Benchmark Surveying Kenley Boss, John Day Kim Randleas Jim Spell, John Day Judy Schuette, John Day

Steve Fischer, Canyon City Jim Johnston, Canyon City Brett Moore, Anderson Perry Mike Lees, Anderson Perry Lindsy Ciepiela, 204 NW Bridge Street Ken Pirie, Walker Macy (via phone) Michael Zilis, Walker Macy (via phone) Gardner Johnston, Interfluve (via phone) Matt Craigie, ECONorthwest

### Agenda Item No. 1—Open and Note Attendance

Mayor Lundbom called the meeting to order at 7:00 pm, noting everyone was present except Councilor Brandon Smith who was absent and excused.

### Agenda Item No. 2—Approval of City Council Minutes of February 26th, 2019

Councilor Paul Smith moved to approve the February 26<sup>th</sup>, 2019 minutes. The motion was seconded by Councilor Schuette and passed unanimously.

### Agenda Item No. 3—Appearance of Interested Citizens

Mayor Lundbom welcomed the audience and noted there would be a time to ask questions after all the presentations.

### Agenda Item No. 4—Consent Agenda – None

### Agenda Item No. 5—Introductory Remarks

City Manager Green reviewed the information included in the agenda memo. He discussed how important the meeting was, as a culmination of nearly two years' worth of work—fundraising, assembling a world class team, and implementing their Five Bold Steps addressing community priorities. He noted that the public hearing will take place in the summer, but they wanted to give residents an opportunity to coordinate with city councilors early in the process.

### Agenda Item No. 6—Resolution 19-804-05, A Resolution Adopting the 2019 Waste Water Facilities Plan Update

Brett Moore from Anderson Perry reviewed their work over the last twelve years coordinating with the city council to address the deficiencies of the wastewater treatment plant. He reviewed the background on the wastewater treatment regarding its degradation and changes in environmental regulations. They noted it has been a struggle to find a cost-effective approach to remedy the situation and when City Manager Green was hired, they began looking into value added effluent to help reduce cost. Mr. Moore reviewed the technical components of the new facility and the expected reclaimed effluent uses as a resource. He also touched on some of the hurdles they encountered including problems with state law limiting scale based on population projections and issues permitting the disposal under new federal regulation. City Manager Green reiterated that DEQ approved the final facilities plan that day to allow the City to operate the new facility under administrative extension until a permitting pathway can be finalized. This allows the city to move forward with design and construction of the new facility, they have a clear path for the next eight months of work. There was discussion with the council about permitting issues for the facility. Mr. Moore noted that John Day has additional hurdles due to its geography and lack of disposal options but is compounded by statewide environmental litigation issues. City Manager Green noted that they found common ground on the understanding that the aging condition of the facility warrants its replacement and the quality of water entering the percolation ponds from the proposed new facility would be of higher quality than that of today. Mr. Moore noted that it could take a long time to identify that permitting pathway, especially if the federal government is involved.

### Councilor Holland moved to adopt Resolution 19-804-05, A Resolution Adopting the 2019 Waste Water Facilities Plan Update. The motion was seconded Councilor Schuette and passed unanimously.

### Agenda Item No. 7— Innovation Gateway Initial Concepts

City Manager Green presented the video noted in the agenda memo then Ken Pirie from Walker Macy, via phone, gave an introduction of their team and the slideshow included in the agenda. Mr. Pirie noted that following the meeting they will go through a period of refining their concepts into an integrated presentation, based on feedback, at the Regional Economic Development Summit on June 11<sup>th</sup>. Gardner Johnston from Interfluve reviewed some background of the study area. He noted the impact of dredge mining on the river corridor, flood plain, and entire valley bottom—essentially turning the natural meandering river into a ditch. Mr. Johnston reviewed the current condition of the river and the environmental impacts of such a simple, unnatural channel. He then explained the various components of the various preliminary river restoration concepts. Mr. Johnston also elaborated on the benefits and considerations included in the slideshow. Ken Pirie reviewed how the river corridor impacts the planning

of the surround trails, transportation, and Innovation Gateway planning. He highlighted the surround parks and opportunities for integrating them all into one system with the Innovation Gateway as the central piece that links them all together. Mr. Pirie elaborated on the proposed improvements to address transportation challenges on the north west side of town. Mike Zilis then reviewed the initial concepts of an integrated system of trails and streets, the potential opportunities at the Oregon Pine site and new city parks, as shown in the slideshow.

### Agenda Item No. 8—Comprehensive Economic Development Strategy Presentation

City Manager Green introduced Matt Craigie from ECONorthwest, noting they are essentially taking the improved land concepts Walker Macy is working on and looking at ways to reinvigorate the economy. Matt Craigie presented their work so far on the comprehensive economic development strategy. Mr. Craigie touched on the project schedule and what the CEDS is; he elaborated on the goals they have been tasked to look including: the greenhouse industry, along with housing and community development. The third component—marketing, branding, and tourism— being led by Bell + Funk out of Eugene, will help communicate what is authentically John Day for those looking to settle down or even just visit and spend money here. He noted that by setting forth goals, the CEDS can then lay out a strategy that identifies objectives to reach the goals and actions to achieve those objectives. Then you can line those things up with funding and sequence them in a viable way that makes sense. The CEDS provides a playbook for community leaders that helps communicate to potential funders what and why they are doing, along with showing progress over time. He provided examples of other communities that took steps toward stabilizing and incrementally expanding their economies. Mr. Craigie provided a brief overview of their first technical analysis. He noted a secondary phase of greenhouses has the advantage of utilizing the available land and reclaimed water, but the industry is complex with a high failure rate. He briefly touched on ways to mitigate risk through managing the math, people, and location. Regarding housing and development, he noted they are still working on an initial assessment—getting a handle on the current housing stock and available residential land—with plans for more depth presentation in May. Mr. Craigie noted that Jen Bell spearheaded a focus group earlier in the day to review the many marketing materials to promote the community. They discussed which ones resonated and tell the authentic story of John Day. She will continue that work towards visual identity development via messaging, graphics, and other collateral to tell the story to an outside audience in a way that connects with the local community. As they work with the city and advisory committees, they will present at a May meeting the more technical work of the CEDS. Mr. Craigie also touched on the REDS and how the effort builds up to that Regional Economic Development Summit. It is a synthesis of all the work and an outwardly facing summit to bring in folks from outside of the community to show all the momentum built in John Day, the local efforts at the Innovation Gateway area, economic development, and housing.

### Agenda Item No. 9—Concluding Remarks & Audience Q&A

Mayor Lundbom opened up the meeting for questions or comments from the audience. Jim Spell noted that construction had begun on the greenhouse but wanted to know how the facility tied into the new wastewater facility, which is not even close to being available. Mayor Lundbom noted the pilot greenhouses will be utilizing freshwater initially. City Manager Green noted they will phase the reclaimed water into the facility once available, but council decided to operate the greenhouse for the first couple of years using freshwater to get a better sense of the annual operations and maintenance and what the revenue model looks like. They were frustrated by the delays caused by the state but happy to be under construction now.

### Agenda Item No. 10-Other Business & Upcoming Meetings

Mayor Lundbom reviewed the other business and upcoming meetings. City Manager Green talked about the scheduled One Stop, the initial found of fundraising for the treatment plant. He noted they will not have the answer that day but significantly closer to putting together their financing portfolio. City Manager Green also presented a plaque and letter sent to the city by Northwest Real Estate Corporation, who completed the Meadowbrook II renovation in January 2018, thanking the city for their planning assistance.

### <u>Adjourn</u>

There being no further business before the council, Councilor Schuette moved to adjourn the meeting. The motion was seconded by Councilor Adair and passed unanimously.

Respectfully Submitted:

Nicholas Green City Manager

ACCEPTED BY THE CITY COUNCIL ON MARCH 26, 2019

Ron Lundbom, Mayor

Time: 12.30pm-1.30pm	Topic: PMT Meeting #4 (Task 4.4)	Meeting Date: 5/21/2019
Location: Conference Call	Project: TGM 5A-17 John Day Innovation Gateway	Project #: P3465.01

Attendees:

Ken Pirie, Walker Macy

Mike Zilis, Walker Macy

Aaron Maples, Walker Macy

Cheryl Jarvis-Smith, ODOT

Nick Green, City of John Day

Aaron Lieuallen, City of John Day

Kristen Kibler, JLA

Matt Craigie, ECONorthwest This call was the fourth meeting of the core Project Management Team, or PMT, which guides the project's progress. The purpose of this meeting was to review refined project concepts presented by Walker Macy in preparation for the June 10th PAC/TAC workshop and Open House #3 in John Day. (Note: PMT #2 was a tour of the project study area.)

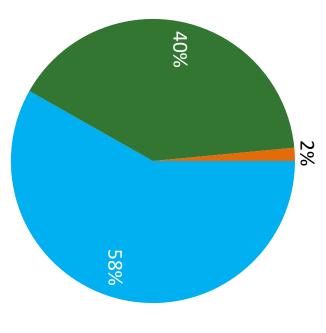
- Team Introductions.
- City team described updates to the greenhouse project and trail construction timeline.
- Nick Green described his goals for the Regional Economic Development Summit (REDS) on June 11<sup>th</sup>.
- PMT discussed logistics for Open House #3, to be held the evening of June 10<sup>th</sup>, after PAC/TAC meeting #3. Both meetings to be held at the Elks Lodge, with updates from both Walker Macy and ECO Northwest.
- Kristen Kibler suggested ways to publicize the event, via a radio spot and City newsletter.
- Walker Macy presented refined concepts and discussed Open House exhibits/stations, including:
  - o Aquatic/Rec Center
  - o Greenhouse/Oregon Pine site
  - o Wastewater Treatment Plant
  - o Parks and Trails
  - o Circulation
  - o River Restoration
  - o The "Go Big" graphic
- Matt Craigie with ECO Northwest described his ongoing work on the marketing/investment prospectus for John Day.
- Rest of the meeting discussion centered on logistics and agenda for the REDS workshop.
- City team asked for a pre-REDS check in call on June 5<sup>th</sup>.

## John Day Innovation Gateway

Preliminary Concept Survey Responses 80 RESPONSES as of 6/4/19

### Where do you live?

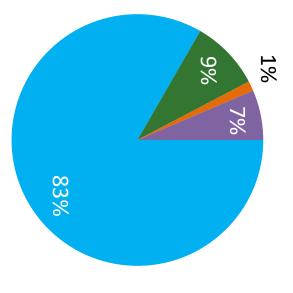
- In the City of John Day (39)
- Within 30 miles of the City of John Day (27)
- More than 30 miles from the City of John Day (1)



# What do you think about the 7<sup>th</sup> Street

### extension?

- I like this. (65)
- I don't have an opinion. (7)
- I have concerns and/or suggestions. (1)
- Other (5)

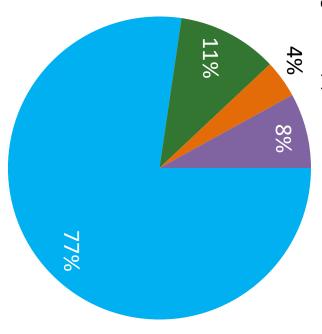


# 7<sup>th</sup> Street Extension: Comments

- Key themes include:
- Support for the extension
- Concern about property taxes
- Support for separated bike and walking paths
- Support for leaving the area undeveloped
- Concern about ongoing maintenance

## What do you think about the trail network, including types of trails and uses or the connections to places in town?

- I like this and I plan to use any new trails. (58)
- I wouldn't use the trails much, but think residents and visitors would use and benefit from these trails. (8)
- I don't have an opinion. (3)
- I have concerns and/or suggestions. (6)

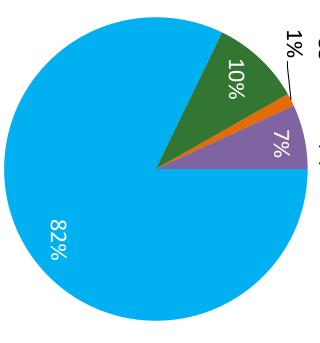


## Trail Network: Comments

- Key themes include:
- Support for the trail network
- Support for a variety of modal uses on trails
- Support for providing access to the river

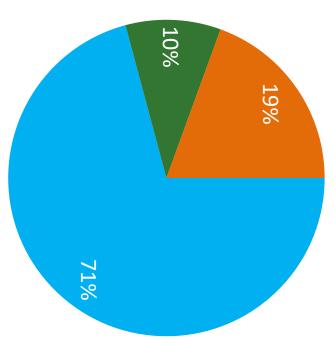
### improved river access for people at the site? What do you think about additional and

- I like this a lot and would access the river. (60)
- I wouldn't use the river access, but think people and visitors would use it a lot. (7)
- I don't have an opinion. (1)
- I have concerns and/or suggestions. (5)



### What do you think about the proposal to restore the river to include natural meanders?

- I like this a lot. (51)
- I don't have an opinion. (7)
- I have concerns/suggestions. (14)



# **River Access and Restoration: Comments**

- Key themes include:
- Support for improving the river access and restoring the river
- Concern about impact to wildlife and habitat i.e. salmon, headwaters, etc.
- This is the last undammed river
- Concern about flooding and high water seasons
- Concern about moving the waterway/support for keeping the current waterway or limiting changes
- Support for working with ODFW
- Concern about consistent future use
- Concern about cost

2	I don't have an opinion about this
16	Other types of buildings or development
7	Office space
21	Public spaces only
26	Hotel or other tourism destination
36	Recreational lake
41	Event space
44	Aquatic center
oughts on types of «e to see at the site? ould be appropriate.	Do you have any thoughts on types of development you'd like to see at the site? <i>Select all you think would be appropriate.</i>

# Types of Development Suggested

Sampling of individual comments

### Would like to see:

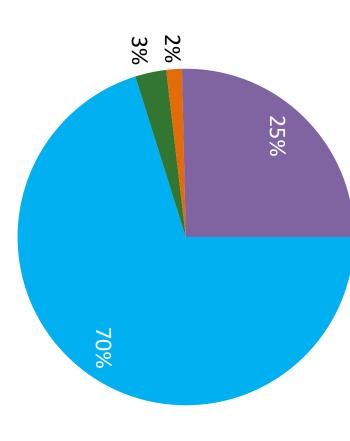
- Swimming pool
- Youth recreational facility
- Mixed commercial/recreational facilities
- Waterfront restaurant/brewery
- Bakery/coffee shop
- Apartments
- Camping
- Parking
- Outdoor/indoor shopping
- Roller skating
- Discount grocery store

### Don't want to see/concerns:

- "Big box" stores
- Asphalt parking lots
- Private development
- City operated space that
- competes with private industry
- Losing "green space" to development
- Lake

## Which of the following applies to you?

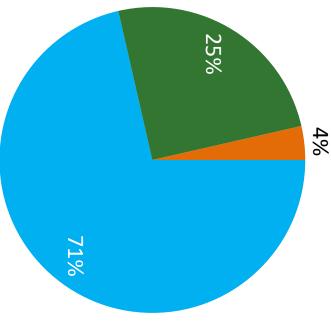
- I work in or near John Day. (47)
- I am currently unemployed. (1)
- I work outside of John Day. (2)
- I am retired. (17)



# For those that do not live in John Day

### How often do you visit John Day?

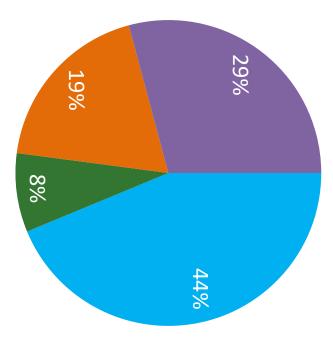




# For those that do not live in John Day

## Why do you usually visit John Day?

Work (21) Visit family (4) Recreation (9) Other (14)

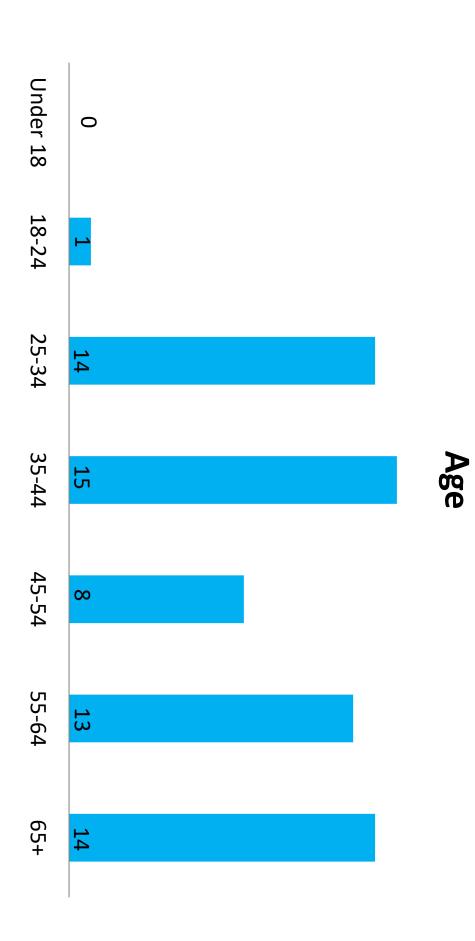


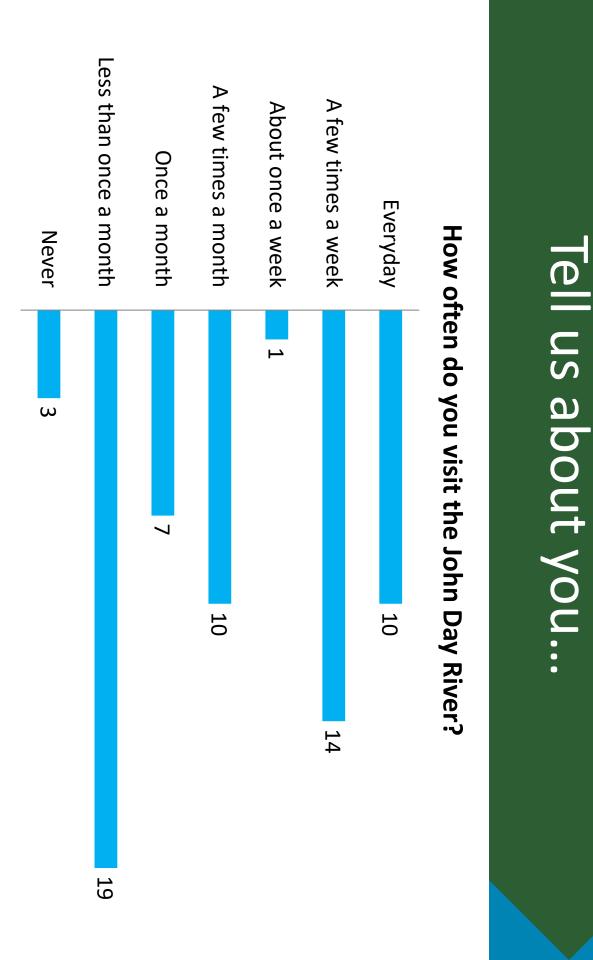
# For those that do not live in John Day

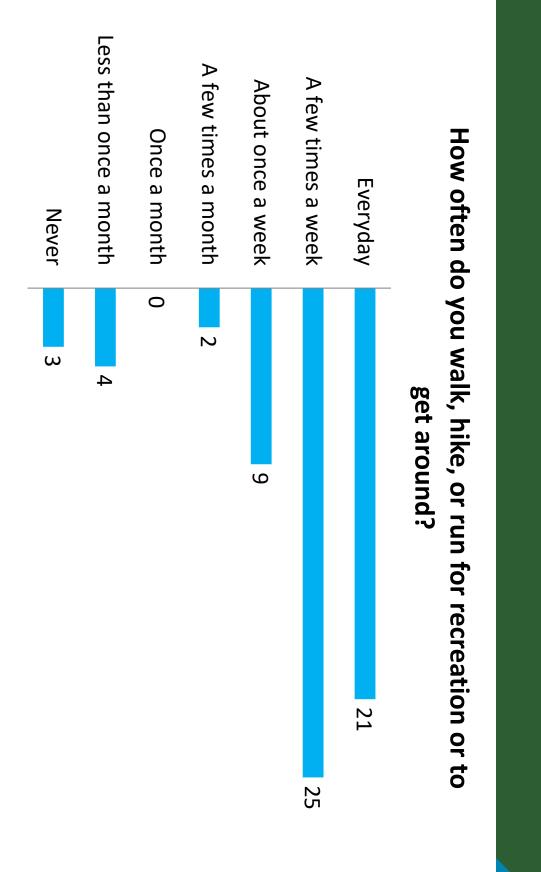
## Other reasons for visiting John Day:

- Shopping
- Groceries
- Medical services
- Restaurants
- School sports
- Childcare
- Friends
- Post office
- Library
- Community events

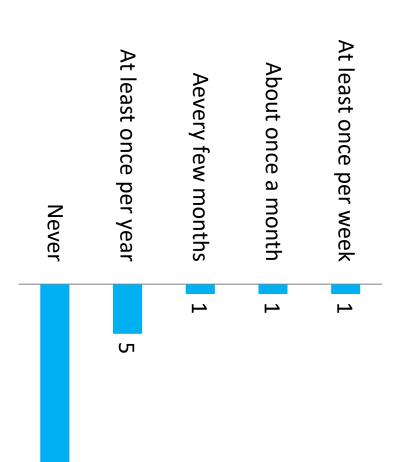




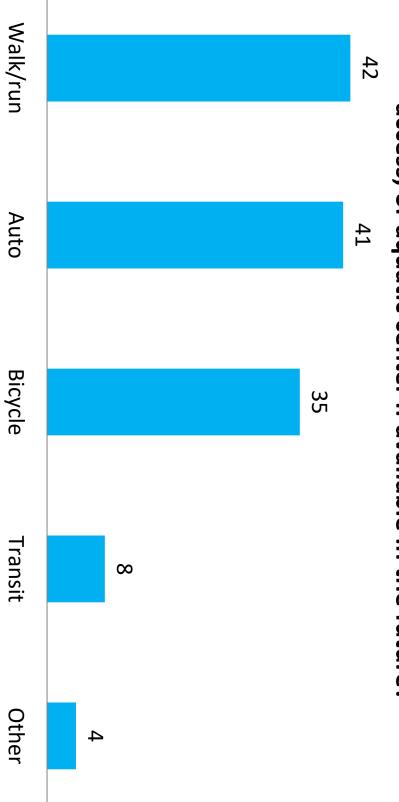




### How often do you use transit (People Mover) to get around?



How might you want to get to any new trails, parks, river access, or aquatic center if available in the future?



- Reasons that prevent you from using the previously mentioned travel choices:
- Access
- Time it takes on transit
- Parking
- Lack of separated bike/walking trails
- Maintenance of bike lanes
- Lack of safety for biking, walking, kids, etc.
- Lack of safe access to the river

### Thank you!



### FINAL PROGRESS REPORT

TO:ECONOMIC DEVELOPMENT ADMINISTRATIONFROM:NICHOLAS GREEN, CITY MANAGERSUBJECT:FINAL PROGRESS REPORT FOR JOHN DAY COMPREHENSIVE ECONOMIC<br/>DEVELOPMENT STRATEGY (CEDS)DATE:SEPTEMBER 30, 2019

### EXECUTIVE SUMMARY

The John Day CEDS consists of two primary activities: developing a comprehensive economic development strategy (CEDS) for the City and coordinating a regional economic diversification summit (REDS). The City of John Day contracted with EcoNorthwest (EcoNW) and the Center for Sustainable Infrastructure (CSI) to accomplish these tasks.

### FINAL PROGRESS REPORT – CEDS

EcoNW completed the following tasks and activities:

- John Day Investment Prospectus, a summary of significant financial undertakings by the City of John Day and its investment partners presented at the REDS.
- Funding Memo, illustrating how we've been working towards funding solutions for the John Day capital improvement projects using content from the CEDS, prospectus, housing analysis, and City's capital improvement plans.
- Community Investment Strategy, formal document identifying the City's economic strategy.
- Housing and Community Development Analysis, a technical memorandum on housing and community development opportunities in and around John Day.
- Hydroponics Market Assessment, a technical memorandum on the economics of the controlled environment agriculture/greenhouse industry and City's position relative to the market.
- Marketing and branding evaluation, a comprehensive look at the City's economic development efforts was performed to identify branding potential to attract outdoor recreation opportunities and interest from potential residents.

Copies of the deliverables are available upon request.

### FINAL PROGRESS REPORT - REDS

### Background

During a June 6, 2019 planning call with US EDA and CSI partners, John Day City Manager, Nick Green articulated the following success criteria for this event (paraphrased):

- Various agencies attend, participate, and identify programs that are a good fit for John Day's Innovation Gateway project.
- Agencies offer support and advice on completing their application processes to apply for funding and/or help to streamline permitting and related processes

An additional priority outcome specifically identified by John Day was that State officials and legislators will better understand challenges faced by rural communities with regard to restrictions on class A water, implementation of broadband, and other economic development and urban renewal related issues.

US EDA (Frances Sakaguchi) helped organize a separate phone call prior to the event to help facilitate additional agency connections.

### Outcomes

- 45 Participants from more than 20 federal, state and local agencies attended the REDS in John Day on June 11, 2019.
- Project-agency synergies were identified and recommendations for next steps developed.
- Those in attendance were introduced to the city and strategic investments John Day already has underway and learned about the City's Comprehensive Economic Development Strategy.
  - Walker Macy gave an overview of the area plan concept and presented design work.
  - ECONorthwest helped to explain the economic context and strategy for the development.
  - City of John Day led a tour of their commercial-scale greenhouse and future integrated park system.
- Governor Kate Brown participated via teleconference at the conclusion of the meeting and emphasized Oregon's ongoing support for the City's projects.

City Manager Nick Green has been invited to present at three conferences in 2019 based on the success of the REDS: the Water Environment Federation Technical Exhibit and Conference (WEFTEC) in Chicago, IL; the Oregon Infrastructure Summit in Salem, OR; and the Oregon Chapter of the American Planning Association annual conference in Portland, OR.

### Finance Opportunity Categories Explored

- Wastewater Treatment Plant / Reclaimed Water
- River Restoration
- Integrated Parks System
- o Greenhouse / Food Cluster
- o Street Improvements
- o Business Accelerator / Incubator

### Key Synergies Identified

(Includes those identified through REDS directly, or through advance conversations and research)

- There is a real possibility to tap into FEMA pre-disaster mitigation funds for part of the river restoration work, to be matched by Oregon Watershed Enhancement Board (OWEB) and other non-federal funds.
- EPA Brownfields is another major federal program that could serve as a major federal anchor funding source to support restoration.
- US EDA connected the City with EPA's Smart Growth program which may support additional project elements.
- National Telecommunication and Information Agency (NTIA) has many tools, data, and information available to point toward funding sources such as the FCC which can support broadband deployment.
- USDA Rural Development can potentially also support broadband infrastructure via Re-Connect Broadband funds.
- National Park Service's Recreation Trails and Conservation Act (RTCA) program may support outdoor recreation development components.
- > HUD funds may be applicable to support housing and urban development needs.

- US EDA introduced Mr. Green to representative for Northwest Interagency Partnership for Sustainable Communities which can provide additional support identifying potential funding sources.
- Federal Highways Administration (FHWA) may have funding sources that would fit well with needed roadway developments for Innovation Gateway – a BUILD grant may be particularly promising, and ODOT can serve as a point of contact (possibly teeing up state matching funds?)

### **Recommended Next Steps**

- Assess viability of all funding opportunities listed above Based on CSI's knowledge, it sounds like there has been or will be progress working with FEMA, EPA, FHWA BUILD grant program, and possibly USDA or NTIA in addition to several Oregon State agencies.
- Continue and expand working relationship with US Economic Development Admin. and Center for Sustainable Infrastructure who are both keenly interested in Innovation Gateway's success.
- Leverage interest generated by REDS event by continuing to seek speaking engagements at conferences and summits.
- Continue telling the John Day Innovation Gateway story via press outreach, working with federal and state partners where possible.

### SUMMARY & KEY TAKEAWAYS

The REDS and CEDS were highly successful events/programs for the City of John Day. Several significant accomplishments resulted from this effort, including:

- City chose to apply for \$15M in Federal Highway Administration funding through the 2019 BUILD grant to complete the city's street network based on recommendations from participants at the REDS. Decision in November 2019.
- City received League of Oregon Cities Award for Excellence (only recipient) a statewide recognition for the Phase 1 Innovation Gateway planning effort that included the CEDS/REDS funded through this grant
- City selected as one of ten winners nationwide of the Environment Protection Agency's Recreation Economies for Rural Communities (RERC) 2019 planning assistance grant

### MEETING NOTES

Time: 3pm-4pm	Topic: PMT Meeting #5 (Task 5.7)	Meeting Date: 7/18/2019
Location: Conference Call	Project: TGM 5A-17 John Day Innovation Gateway	Project #: P3465.01

Attendees:

Ken Pirie, Walker Macy

Mike Zilis, Walker Macy

Matt Hastie, Angelo Planning Group

Cheryl Jarvis-Smith, ODOT

Nick Green, City of John Day

Aaron Lieuallen, City of John Day

Matt Hastie, Angelo Planning Group This call was the fifth meeting of the core Project Management Team, or PMT, which guides the project's progress. The purpose of this meeting was to discuss the remaining tasks in the scope of work and the schedule for completing these tasks through October 2019.

- City team described the BUILD grant application timeline (decision by November 2019) and recently-completed efforts to coordinate MOUs with property owners. City team noted that the extensive work to prepare the grant application will be useful to the city, regardless of grant outcome.
- Nick Green requested that Walker Macy's TM#6 be revised to align with the cost estimates presented in the BUILD grant application, for consistency. Cheryl Jarvis-Smith also requested that the pedestrian bridge (to be named 'Johnson Bridge', be added to the cost estimate in TM#6). It was noted that parking lots, curb ramps and lighting and signage don't appear to be included in the costs, but since the lots will be privately-funded, it was decided to leave them out. Lighting and signage and curb ramps are assumed to be already included in the overall estimates for streets. Walker Macy will double-check and add a number for the costs of accessing parking lots from public streets.
- Cheryl Jarvis-Smith noted that there might be inconsistency with the County TSP regarding needed upgrades to two bridges, on Bridge Street Patterson Bridge Road. Nick Green noted that traffic counts used in previous plans were unrealistically low and he would like DKS to provide a quote to the City to update those counts. Ken Pirie will coordinate with DKS.
- Nick asked for the Evaluation Criteria in TM#6 to have a total evaluation 'count' and Cheryl asked team to include an arrow extending west from the study area suggesting future trail extensions towards Mt Vernon.
- Nick also asked that TM#6 include a short description of what was in the 2009 Local Street Plan and what is proposed for change, as outlined in the BUILD grant.
- Team discussed the schedule for completing TM#8, TM#9 and the Draft Plan. Matt Hastie noted that we should pick a Council Hearing date based on the required DLCD 35-day notice, allowing enough time for the team to prepare the Memos. It was suggested that the plan be scheduled for Planning Commission and City Council Hearing on October 8<sup>th</sup>. This would require the 35-day notice to be sent to DLCD by September 3<sup>rd</sup>. The notice can include draft content from TM#8 and TM#9. The week before this, on August 27<sup>th</sup>, will be the final PAC/TAC (#4) followed on the same day by a Planning Commission and City Council Work Session. A full draft schedule is attached to these meeting notes.

- Cheryl Jarvis-Smith asked if the City's Comp Plan includes policies on reusing wastewater and if there are any relevant statewide goals. Matt Hastie and Nick will check. Cheryl also asked if there are riparian setbacks that need to be updated, based on concepts presented in this plan. Nick noted that the Greenway Overlay chapter addresses this, with 20' setbacks, but they may need a new overlay or code chapter for the complexity envisioned. Hood River has a good precedent for a river recreational overlay zone.
- Cheryl suggested that a street cross-section be prepared for the proposed access road into the proposed campground.

end

### MEETING NOTES

Time: 1pm-2pm	Topic: PMT Meeting #6 (Task 6.2)	Meeting Date: 8/21/2019
Location: Conference Call	Project: TGM 5A-17 John Day Innovation Gateway	Project #: P3465.01

Attendees:

Ken Pirie, Walker Macy

Mike Zilis, Walker Macy

Matt Hastie, Angelo Planning Group

Cheryl Jarvis-Smith, ODOT

Nick Green, City of John Day

Aaron Lieuallen, City of John Day This call was the sixth meeting of the core Project Management Team, or PMT, which guides the project's progress. The purpose of this meeting was to preview the presentation for the joint PAC/TAC and final PMT meeting on August 27<sup>th</sup> and discuss the remaining tasks in the scope of work and the schedule for completing these tasks through October 2019.

- City team described progress on trail construction along the river and in Davis Creek Park. The City won a \$500,000 OPRD grant to add more segments of trail and integrate the trail system. Grant County will also be involved (Prospector Trail is managed by the County). Deputy Director of OPRD, MG Devereux, will be coming to town in October to discuss the grant and the city's integrated parks and trail system, including the Kam Wah Chung site's future.
- Edits to Tech Memo #7 were discussed, including:
  - Confirming that 7<sup>th</sup> Street should have a sidewalk only on its south side, to serve people using on-street parking. Memo text needs to add clarifying text about this and on-street parking.
  - The Government Entry Drive extension will have wider paved shoulders on the east side for uphill bike travel.
  - Gateway Drive doesn't need a sidewalk on north side, but maps should show a trail on north edge of Gateway, connecting up to Valley View Drive. South side of Gateway will have a sidewalk.
  - There should be a trail connection between US 26/Main Street into the park, so pedestrians don't need to walk on Johnson Drive.
  - Cheryl Jarvis-Smith asked if residential access roads, the campground road and the proposed wastewater treatment plant access road should be added as Local Streets to the Transportation Solutions map, since the city has a 2009 Local Street Plan (typically local streets aren't included in TSPs). Nick requested that these streets not be included, for future flexibility.
  - Final memo should include the Johnson Bridge analysis and narrative. This was completed on the date of the call and Nick will provide to DKS.
- Group discussed Tech Memo #9. Matt Hastie will incorporate final edits. Nick mentioned an interest in adding good landscape-scale lighting at trail overlooks, but not in areas that might have residential impacts. Question will be asked to the Planning Commission and City Council. Nick suggested that Matt build a presentation to the PC/CC that asks key questions about items like blocks lengths, building heights, outdoor work areas and other topics.

- Cheryl asked that TM#9 include a glossary of key terms, as well as a key for the land use tables.
- Cheryl suggested that future hotel should be designed to ensure ADA accessibility.
- Nick requested that the team ensure consistency between memos, particularly on cost tables, so all deliverables reflect the BUILD grant costs.
- Team discussed logistics for August 27<sup>th</sup> team visit to John Day for PAC/TAC and PMT site tour and PC/CC study session. Next steps for the project:
  - TM#8, TM#9 and the Draft Plan will be distributed to City and Committees for review prior to August 27<sup>th</sup> work session.
  - Matt Hastie to update TM9 as needed for DLCD after session.
  - Draft memos will be sent to DLCD as part of 35-Day notice by September 3<sup>rd</sup>
  - o Planning Commission and City Council Hearing on October 8th.
  - Final Memos and Final Plan will be delivered by October 18th
  - Project will wrap following the Hearing and Final Plan delivery, with Notice of Adoption, City Celebration and Title VI Report.

end

### MEETING NOTES

Time: 5pm-7pm	Topic: Joint PAC/TAC Meeting #4 (Task 6.5)	Meeting Date: 8/27/2019
Location: John Day Fire Hall	Project: TGM 5A-17 John Day Innovation Gateway	Project #: P3465.01

### Attendees:

Ken Pirie, Walker Macy

Matt Hastie, Angelo Planning Group

Cheryl Jarvis-Smith, ODOT

Nick Green, City of John Day

Aaron Lieuallen, City of John Day

Kristen Kibler, JLA

PAC/TAC members On August 27th, 2019, the combined TAC and PAC for the John Day Innovation Gateway Area Plan convened at the John Day Fire Hall for a 90-minute tour of the project area to review the plan concepts and view implementation progress to date. The group, which also included Cheryl Jarvis-Smith from the PMT as well as several Planning Commissioners and City Councillors, visited the new greenhouses, the new Public Works shop, the proposed hotel site and Johnson Bridge, the recently-completed riverfront trail, the new Davis Creek trail system and the proposed Hill Family City Park site. City Manager Nick Green and Senior Planner Aaron Lieuallen led the tour, with input from Ken Pirie, Walker Macy and Matt Hastie, APG.

Following the tour, the groups reconvened at the Fire Hall for the Joint PC/CC Work Session (Task 6.6), which featured a presentation summarizing the project concepts by Ken Pirie, followed by a discussion of recommended code and policy updates, by Matt Hastie.

end

### CITY OF JOHN DAY CITY COUNCIL/PLANNING COMMISSION MINUTES JOHN DAY, OREGON

August 27, 2019

### **COUCILORS PRESENT:**

Ron Lundbom, Mayor David Holland, Councilor Paul Smith, Councilor Elliot Sky, Councilor Shannon Adair, Councilor Steve Scheutty, Councilor Gregg Haberly, Councilor

### **COMMISSIONERS PRESENT:**

Tim Unterwegner, Commissioner Ken Boethin, Commission Chairman Neal Ledgerwood, Commissioner Tom Wilson, Commissioner

### **STAFF PRESENT:**

Nicholas Green, City Manager Monte Legg, Public Works Director Aaron Lieuallen, Senior Project Manager

### **GUESTS PRESENT:**

Levi Manitsas John Morris Brett Uptmor Randy Hennen Ken Pirie, Walker Macy Matt Hasty, Angelou Planning Group

### **Open and Note Attendance**

### **COMMISSIONERS ABSENT:**

Darin Toy, Commissioner

**COUNCILORS ABSENT:** 

Julie Larson, Senior Account Clerk Matt Manitsas, Assistant Project Manager

Chris Cronin Rob Raschio Cheryl Jarvis-Smith, ODOT Angie Jones Digette McKracken Lisa Weigum

Mayor Lundbom called the work session to order at 5:30 pm, noting everyone was present.

### **Innovation Gateway Area Plan**

City Manager Green noted Ken Pirie from Walker Macy would be leading the discussion, which will be the final session before materials are sent to DLCD to be codified. Mr. Pirie introduced himself and noted Cheryl Jarvis-Smith and Matt Hasty from TGM Planning team were also present. He highlighted it has been an honor to work with the community and he has been impressed with the speed of implementation the City has been able to achieve as they worked on this project.

Mr. Pirie asked if everyone had seen the Perspectus which was the vision for the strategy for growth. Key sectors to focus on include the greenhouse industry, marketing and tourism focused on recreation, getting good broadband, helping build local businesses, encouraging new housing construction and understanding areas of opportunity in the community. The key being to create a model for rural economic development. They focused on the Gateway, the Iron Triangle property and a few other areas. Ken discussed using local recreation opportunities to pull the different focus areas together. They will also discuss the attached memos about the transportation system and funding, memos about funding and finance, and 2 memos about policy and code updates. The Advisory Committee worked with them to set up a series of goals and principles that were widely accepted. At the end of the presentation they will look at the goals and see what was met and what needs work.

Mr. Pirie presented and discussed the Draft Innovation Gateway Plan attachment. He discussed the restoration of the John Day River and the history of the river. This would include flood mitigation. He discussed the trail system and the extension of 7<sup>th</sup> Street. The park system should be integrated with the new riverfront. He passed out maps of an improved transportation system. They applied for a BUILD grant for extensions to 7<sup>th</sup> Street, connections to Valley Vew and trail improvements, costs could be seen in Memo 8.

City Manager Green wanted to clarify that the Charolais Heights project on the map was funded but will be delayed until next spring because they can't start work on anything that they are using as a match and that's when they know if they get this award. Bridge improvements (#12 on the map) at Oregon Pine is funded also. A bike/pedestrian bridge (#13 on the map) and 3<sup>rd</sup> Street extension bridge plus some connector pieces to the northwest have also had funding applied for with some funding already. They will also need to get agreements with property owners in place. Memo 8 also covers other potential sources of transportation funding.

Mr. Pirie wants to go through details of the plan starting at the west in the Oregon Pine site. The planer shed was going to be a restored community space. The sawmill would be renovated into potential meeting areas. The Johnson bridge would be redecked with donated cross laminate timber. There is a lot of development space on the north side of the river that would be good for an office building. This would all connect to the 7<sup>th</sup> Street extension. There is also potential for an open market space or an art gallery/studio with a public restroom at the back. The sawmill is a well built building with a lot of potential.

Moving east, the 7<sup>th</sup> Street extension winds down north of the river with a multi use path south of the road. There is a trailhead with a restroom. The existing wastewater treatment plant will be gone with that space restored with a campground. The final piece is the Davis Creek Park, the trail system will go up to an overlook at the park. There is also the potential for an aquatic center in the NW corner of 7<sup>th</sup> Street park. South of the John Day River could be a new pedestrian bridge connecting 7<sup>th</sup> Street to the Fairgrounds to integrate it into the system. Mr. Pirie referenced and discussed Green's vision of an arch to welcome people to the community and if not an arch at least some sort of monument with signage. The members of the committee and council discussed and agreed there needed to be some kind of structure. City Manager Green touched on a point from his memo and discussed what it should feel like to come to John Day. Also discussed was the possibility of reconfiguring the City Hall parking area as a welcome area with restrooms. Mayor Lundbom advised it would be good to find out from the state what factors limit the kind of structures you can put on a state highway.

Mr. Pirie discussed other opportunities in the community such as integrating public art and encouraging context appropriate design. There also needs to be signage to encourage wayfinding, the signs should have consistent designs to tie it together. The proposed approach to downtown parking is to buy the

Wright Chevrolet lot and the old City Hall lot and put in parking and electric vehicle stations and work with local property owners to provide other parking opportunities. The city would create a downtown improvement district and share the cost of improvements with the property owners who benefit the most from the new parking. The city would put the money upfront and recover that from the improvement district. The entire plan would only require the removal of one home that the property owner is willing to sell to the city.

City Manager Green's question to the study group was if residents cared enough about the parking problem in John Day to invest their own money to solve the problem. Someone said they were always able to find a spot to park downtown right not. Councilor Adair said that from a merchants standpoint the problem is the big vehicles that come through and park on Main Street and block the entrances which is not only a problem for the businesses but for other vehicles like the People Mover other big vehicles trying to pass through. Angie advised that there are currently no bus stops on Main Street because there are no safe places for stops as it is now. Someone suggested an app telling people where to stop or park or eat or stay. Green suggested the brown signs with the white P to direct to new parking areas. Councilor Adair advised that it might be hard to get business owners to want to spend money on this but some money out would also mean money coming in. Green stated that now is the time because the interest rates are as low as they will be and property owners to the north are ready to go when the city is.

Councilor Adair voiced concern over the layout of the parking lot behind US Bank, the Elks Lodge and the strip of businesses on Main Street. Green advised that there was no debt in the street fund so a modest investment would not break the bank. There was a consensus from the group that getting started on off street parking was a good way to go. Councilor Adair volunteered to talk to the Main Street merchants and see if they are on board.

Green discussed the electric vehicle charging station that OTEC will help with at the old City Hall location. Someone asked if they would get used, Councilor Adair said the ones at the Chamber of Commerce get used all the time. It was asked how they know where to find a charging station, someone said you could find them on an app. Another question was how you pay to charge at the station, someone advised it was like a vending machine you just put your card into that OTEC might get royalties for. Someone suggested that there should be a more comfortable walking space that is separated from the cars parked on the street on Main Street which may end up taking away parking spots.

Mr. Pirie asked the group if they felt goals for the project had been met. Mayor Lundbom said he felt they had hit the nail on the head with each goal, some more than others but he felt they were all met. City Manager Green agreed and said that the potential is there but thinks we could more to enforce staying true to the history of John Day. He also thought there should be more time and thought put into attracting investment in jobs; at the next Council meeting he and Allison will talk about a venture capitalism opportunity that Ford Family Foundation has proposed for our area. Somene asked how a member of the public would feel about the goals being met and Lisa advised that as far as community health was concerned there was much more than just trails and parking that should be considered such as poverty, unemployment, substance abuse and mental health. Rob advised there was a low income housing crisis that was not being addressed in a city with relatively high housing costs. Someone else said it was a great plan that was targeting a very specific demographic in the community but she wanted to make sure that it was promoted to everyone across the board and she said she was a big fan of bringing in tourist but didn't want to leave behind the families that have been here already. Rob said that if the city was committed to making sure those long term families that have been here are able to

maintain a life here there needs to be a discussion about how to accomplish that. Greg voiced that there is a drug problem in the community and half of these problems being brought up could be solved by solving the drug problem. Councilor Adair cautioned that communities like McCall and Bend that have brought in a lot of tourism have shut out a lot of people who just couldn't afford to live there anymore (Lisa called it gentrification) and that we should consider and talk about how to avoid bringing in lots of wealth and pushing out the people that already live here. Chris pointed out that the goals of the plan don't directly address these problems but the plan will create opportunities and she didn't think it would increase property rates but would increase investment and didn't see anything about the plan that caused concern about increasing the problems that people had voiced. Ron said that by creating housing for people who can afford it you are opening up more housing for people who can't afford it because those who can afford to will build houses and those who can't afford to build can buy the houses that are being moved out of by those who are building. Rob said he agreed that there is a systemic drug problem in our area but there are also a lot of hard working, non-criminals who are also poor and we need those hard workers in our community too to keep it running.

### Code Amendments

Matt Hasty gave his presentation on the Code Amendment memos (memo attachments 8 & 9). He discussed the points that are laid out in the attachments. He covered the potential amendments to the Comprehensive Plan and Development Code. He summarized what the Comprehensive Plan was for and what they looked for when they looked for amendments that might need made. They will be recommending some additional economic development policies and references to the innovation gateway plan. The Development Code amendment goals will be consistency with the innovation gateway plans and building on what base zones allow, application of an overlay zone, and proposed new districts. All of the proposed changes will go to the council to be voted on in about 5 weeks and they are open to comment until then.

Mr. Hasty asked for comment on the special limitations on the floodplane overlay district, they didn't see the need to add limitations but wanted to know if anyone had thoughts on it. City Manager Green said in the current code the greenway overlay zone is 20 feet from Canyon Creek and the JD River, 20 feet is awfully close if the city ever subdivided in the future. Lisa said she wouldn't want to see anything within that 20 foot space because when the city purchased this space it was intended for a green space for the community, not for commercial or residential property. John advised that the temperature of the river is a major problem, he said changing the river to meander it will slow it down and affect the water quality and habitats for 2 migratory species, and if the changes effect the 1866 water rights at the mill end of the river it will be an issue. He also wants to know who will be responsible for water safety issues with the new area being passed off as a place for kids to play.

In response to these concerns, Hasty advised that there was no intention to change the diversion and that the meandering would require a lot more study, planning and work to address Johns concerns. They are not proposing to change the city's development code in regard to the floodplane and the greenway but are asking to think about the possible northward expansion of the greenway in that area. Green said he was generally on the same page about what is possible here, they don't know what people will want 20-30 years from now and expansion of the greenway would lock down that area to prevent development along the river on land owned by the city and would be pretty much permanent.

Mr. Hasty continued with his presentation on the code and plan changes. He discussed minimum lot areas and allowed heights for new development. They are not currently recommending a change but he

asked if anyone thought they should recommend a lower maximum height to the south of the river in the commercial zone to have less of an impact on views. Someone asked what height the existing structures are in that area, Hasty advised probably between 35 and 40 feet. Lisa advised that she did not think a tall hotel would be aestethically pleasing in that area. It was advised that 35 feet is 3 stories and 45 feet would allow 4 stories. Someone suggested keeping it at or below the height of current structures. Hasty advised that for the fire department they will make you install sprinklers if it is above what they can get a ladder to and to check with the fire chief to make sure it is not more than an aesthetic requirement.

The proposed changes to the area north of the river where retail and certain commercial uses are allowed would also be allowed in the new area but with restricted sizes to keep those uses small with a 5000 square foot maximum being proposed and no maximum proposed for areas south of the river. They are proposing that the ? coverage standard be reduced from the current 90-95% and the minimum landscape coverage be increased from 5-10% to 15% throughout. The proposed changes to the build-to line would be to change the requirement to build from within 60 feet of the street to a requirement to build within a 20 foot distance of a path, road, or access way, not just a street. *Mr. Hasty's remaining 3 slides were covered at the beginning of the August 27, 2019 regular council meeting*.

### <u>Adjourn</u>

The meeting was adjourned and the presentation continued at the regular council meeting.

**Respectfully Submitted:** 

Nicholas Green City Manager

ACCEPTED BY THE CITY COUNCIL ON OCTOBER 8, 2019

Ron Lundbom, Mayor

### CITY OF JOHN DAY CITY COUNCIL/PLANNING COMMISSION MINUTES JOHN DAY, OREGON

October 8, 2019

### **COUCILORS PRESENT:**

Ron Lundbom, Mayor David Holland, Councilor Paul Smith, Councilor Elliot Sky, Councilor Shannon Adair, Councilor Gregg Haberly, Councilor

### **COUNCILORS ABSENT:**

Steve Scheutte, Councilor

**COMMISSIONERS ABSENT:** 

### COMMISSIONERS PRESENT:

Ken Boethin, Commission Chairman Tim Unterwegner, Commissioner Neale Ledgerwood, Commissioner Tom Wilson, Commissioner Darin Toy, Commissioner

### **STAFF PRESENT:**

Nicholas Green, City Manager Chantal DesJardin, Secretary Monte Legg, Public Works Director Anna Bass, Solutions CPAs (City Recorder) Daisy Goebel, Associate Planner Aaron Lieuallen, Senior Project Manager

### **GUESTS PRESENT:**

Patti Salvino, 328 N Humbolt, Canyon City

Rudy Diaz, Blue Mtn. Eagle

### Agenda Item No. 1 – Open and Note Attendance

Mayor Lundbom called the work session to order at 6:00 pm, noting everyone was present except Councilor Schuette was absent and excused

### Agenda Item No. 2—Approval of Prior City Council Minutes

Councilor Sky noted the minutes stated he was not in attendance in Item 1 but he was actually present.

### Councilor Haberly moved to approve the August 27<sup>th</sup> work session minutes, as amended. The motion was seconded by Councilor Adair and passed unanimously.

### Agenda Item No. 3—Appearance of Interested Citizens

Mayor Lundbom welcomed the audience and asked if anyone had anything to add to the agenda. Patti Salvino asked if the council had made a decision regarding her RV occupation; Mayor Lundbom noted it was one of the items they would be discussing as part of the agenda but they would not be making a decision yet.

### Agenda Item No. 4—Annual Planning Report

City Manager Green reviewed some of the planning activity included in the agenda memo and planning activity log. Ctiy Manager Green noted as a result of the increase in activity he has offered a position to Daisy Goebel; she will do all the land use reviews, property line adjustments, site design reviews, assist with code enforcement and with long range planning required by law such as the five year natural hazard mitigation planning and FEMA updates, and the 2021 comprehensive plan update. He hopes it will reduce the city approval timelines, since they are the first stop for any development and reduce unpermitted development and the resulting code enforcement activity. Green added that all the actions for review tonight are Type IV, as code amendments, require review and approval by the planning commission and city council after public hearing and notice. The public hearing will be noticed next Wednesday and held on November 12<sup>th</sup>, so tonight is the opportunity to make any further changes. He noted that the fees have not been updated since 2005 and may need to be revisited to make sure the city is capturing the extent of the work that goes into some of the applications. The intent is not to have fees that achieve full cost recovery, but more than the roughly five days they currently cover.

### Agenda Item No. 5—AMD-19-01, Comprehensive Plan Map Change, Comprehensive Plan Text Change, and Land Use Regulation Change for the John Day Innovation Gateway

City Manager Green reviewed the information included in the agenda memo and technical memo attachment. Green highlighted some of the ways the amendments maintain the current policies such as keeping development harmonious with the history and heritage of the site as former timber mills. He also added that the economic policies are intended to encourage investments that increase profitiablity for the community and businesses, increase growth, or improve quality of life. The public facilities policy change will require future developments of any scale use reclaimed water for any non-potable uses wherever the reclaimed water is available, an effort to conservation and shift away from freshwater uses. The reclaimed water will have to be discounted appropriately in order to make affordable and those details will be finalized with the treatment plant planning. Mayor Lundbom asked about how some of the recommendations had been reached, it was noted they were drafted by APG from from efforts by the integrated planning team. Green added that he spoke to Iron Triangle, the only private property owner directly impacted, about the northwest changes and they are very supportive of the proposals because it creates greater flexibility fo the use of their property. Green noted the remainder of the memo is planning and implementation focused.

### Agenda Item No.6 —AMD-19-02, Land Use Regulation Change for the John Day Special Flood Hazard Area

City Manager Green reviewed the agenda memo and staff report and noted some of the ways he tried to tailor the state ordinance to our community. An example is the base flood elevation could be three feet, which adds development costs, but virtually negates the flood insurance cost. Communities can allow allow zero elevation, but it significantly increases flood insurance rates for everyone; the council chose one foot in the past, so Green maintained that in the new ordinance to keep rates near the same. Green also noted that most of the changes are regarding implementation; the new ordinance has more specific requirements to enforce and hopefully clarify for those developing. Green noted that a significant portion of the city is in the flood hazard area so it is an important topic and part of the development code. Green noted that existing structures in the floodway are allowed to stay but likely new development would not be approved, even after extensive review. He also noted that FEMA regulates on the basis that any structure with any portion touching the floodplain or floodway is

included. Green added that it delineates the city's requirements for notification and provides methods for removal if a property undergoes some kind of qualifying change in circumstance. Rudy Diaz asked if the those properties in the floodway need to do anything. Green stated that every property that would be affected by changes in the floodplain received notice a year ago of the proposed changes; there was also a county-wide public meeting. Any development applications will be checked to see if it is part of the updated floodplain map; the greenway overlay is another overlay that an often forgotten update from 2005. There was further discussion about the parameters outlined in the code regarding the greenway overlay.

### Agenda Item No. 7—AMD-19-03, Land Use Regulation Change for Housing and Temporary Use Permits for Recreational Vehicles

City Manager Green reviewed some of the changes to the development code lined out in the staff report. He noted that some of the language included may be reviewed or refined by the city's professional planners so the intent should be the focus of review. Planning Chairman Boethin commented that "satisfactory progress" referenced in 5-2.2.100(L)(3)(A)(2) needs to be defined because it means something different to everyone in the room. There was discussion about how this could be defined. There was also discussion about whether the permit would be reviewed after 18 months or periodically during the 18 month period. Green added he would speak with the planners about definitions and metrics for determining whether review of progress is necessary before 18 months. There was clarification about the implementation of the terms regarding removal of a unit once the permit expires, specifically in regards to a recreational vehicle. Those kind of units are allowed to stay on the property but must be removed from services and cease occupation, they can remain on the premise for storage purposes only. Green noted he would work on better refining the explanation of that term. He also reviewed which types of review specific requests would fall under and under what conditions the planning commission would need to be involved. Commissioner Wilson asked about the definition of a "self contained RV"; Green commented he would look into it and see if it needed to be removed from the language. Mayor Lundbom asked about the requirement for RV's to be licensed and insured; Green noted if it was not stipulated, he would make sure that was included, potentially falling under safety hazards referenced in 5-2.2.100 (L)(3)(B)(4)(f). Rudy Diaz asked if there was a limit to the number of times the permit could be renewed; Green noted there was not a limit included just the stipulation that it be an annual renewal. Couniclors commented that it is anticipated that conditions that qualify for a special situation would have resolved within a year and if it cannot be resolved it that time, it is likely not extenuating circumstances that warrant a temporary residence. City Manager Green gave some examples of anticipated ways the amended provisions of 5-4.9-010 would be utilized. He also briefly touched on examples of properties that would be affected by Amendment 6 in the staff report. It was noted that internal improvements to facilitate transition between uses will be allowed. New residential development or additions will not, the city will need to be vigilant to ensure modular accessory structures being approved under certain accessory uses are not being converted for potential residential occupation.

### <u>Adjourn</u>

Chairman Boethin moved to adjorn the meeting at 6:53PM. The motion was seconded by Councilor Adair and passed unanimously.

**Respectfully Submitted:** 

Nicholas Green City Manager

ACCEPTED BY THE CITY COUNCIL/PLANNING COMMISSION ON NOVEMBER 12, 2019

Ron Lundbom, Mayor

# CITY OF JOHN DAY CITY COUNCIL/PLANNING COMMISSION MINUTES JOHN DAY, OREGON

November 12, 2019

# **COUCILORS PRESENT:**

# **COUNCILORS ABSENT:**

Ron Lundbom, Mayor Steve Schuette, Council President David Holland, Councilor Paul Smith, Councilor Elliot Sky, Councilor Shannon Adair, Councilor Gregg Haberly, Councilor

# **COMMISSIONERS PRESENT:**

Ken Boethin, Commission Chairman Neale Ledgerwood, Commissioner Tom Wilson, Commissioner Darin Toy, Commissioner

# **STAFF PRESENT:**

Nicholas Green, City Manager Chantal DesJardin, Secretary Monte Legg, Public Works Director Mike Durr, Police Chief

# **GUESTS PRESENT:**

Sherrie Rininger, etc. Jody Moulton **Dean Nodine** Bonnie Maplesden Jonna Bishop, GC Fair Board Donna Palmer, GC Fair Board Courtney Montague, GC Fair Board Rudy Diaz, Blue Mtn Eagle Barb Oliver Tim Oliver Melissa Jones, Riverside Home Park Tim Nodine, Grant County Snowballers Garrett Carniglia, Grant County Snowballers Jeremy Adair Hannah Hinman Kelvin Taysom, Oregon Telephone Don Merritt, OPRD-Kam Wah Chung Marc DesJardin, OR Dept. of Forestry Tom Olson, John Day Gregory Goebel, John Day

# **COMMISSIONERS ABSENT:**

Tim Unterwegner, Commissioner

Daisy Goebel, Associate Planner Aaron Lieuallen, Senior Project Manager Casey Myers, Water System Operator Don Gabbard, Fire Chief

Patti Rathbone, Damascus, OR Chris Fox, Corvallis, OR Judy Schuette, John Day Kathleen Cathy, La Grande, OR Jim Spell, John Day Janice Provencher, John Day Pam Minster, John Day Sandie Gilson, Mt. Vernon Luke Palmer, Pendleton, OR Sam Palmer, Grant County Commissioner Didgette McCracken, John Day Kathy Smith, John Day Stephanie LeQuieu, Mt. Vernon Charlie Tracy, OTECC Eva Harris Nancy Nickel Ryan Seal, ODFW Mike Jensen, ODFW Jerry Franklin Patty Salvino

Cheryl Jarvis-Smith, ODOT Cort Priday Rory Priday Angela Ostrander Troy Clark

# Agenda Item No. 1 – Open and Note Attendance

Mayor Lundbom called the meeting to order at 7:00 pm, noting everyone was present.

# Agenda Item No. 2—Approval of Prior City Council Minutes

Councilor Sky highlighted some typos in the minutes, under Item No. 6, noting "Day" and "significant" were spelled incorrectly.

# Councilor Schuette moved to approve the October 8<sup>th</sup>, 2019 work session minutes, as amended. The motion was seconded by Councilor Haberly and passed unanimously.

Councilor Smith noted the motion for Item No. 9 had a typo and should include another zero, clarifying it was for ten thousand dollars.

# Councilor Smith moved to adopt the City Council minutes for October 8<sup>th</sup>, 2019, as amended. The motion was seconded by Councilor Schuette and passed unanimously.

Councilor Holland commented that under item No. 6 the summary of costs was only for the development of three sites, the city would not be developing the OTECC site.

# Councilor Adair moved to approve the October 22nd council minutes, as amended. The motion was seconded by Councilor Holland and passed unanimously.

# Agenda Item No. 3—Appearance of Interested Citizens

Mayor Lundbom welcomed the audience and asked if anyone had anything to add to the agenda. Jim Spell wanted to thank the City specifically Public Works for putting railing up on the north end of the Bridge Street bridge, which was a real big safety hazard.

# Agenda Item No. 4—Consent Agenda

Mayor Lundbom noted a representative of the Fair Board had asked to speak regarding the consent agenda item pertaining to the fairgrounds. Donna Palmer, Fair Board member, requested that Mindy Winegar, Manager, be included in all meetings and that the city or the consultants address the fair board at one of their meetings to have an open dialogue. They feel there has not been an open dialogue and spent most of the morning on the phone with County Court giving their concerns. She thanked the city for contributing the money to move forward but would like to make sure they are part of the plan every step of the way, to make sure everyone is in agreement. She noted there is a lot that needs to be done at the Fairgrounds and would like to see money going to tangible uses. The paper doesn't make the Keerins Hall look better, so they just want to make sure moving forward they can realize some of these plans. Mayor Lundbom noted they have been very happy with the deliverables produced by the consultants and will make sure they are involved, including the progress reports. He added they made sure to get some assurances from the court that there will be some actions to follow up the study. Councilor Adair moved to approve the consent agenda. The motion was seconded by Councilor Schuette and passed unanimously.

# Agenda Item No. 5—Innovation Gateway Phase 1 Presentation & Next Steps

City Manager Green noted the presentation of the Innovation Gateway is a culmination of three-years' worth of in-depth planning taken on by the city to heal the community and bring us together to forge a path to a brighter future. Green spoke on the history of booms and busts in the community and stated the mission is to fight back and take control of the economy and social outcomes to create the kind of place they will be proud to have their children and grandchildren live. He noted that Kim Randleas has used the term "Small but Mighty" when talking about the community; he feels this is a good description of Grant County because we often have to be stronger than our numbers would indicate. He spoke further on some of the presentations he has been involved with for the city in order to better advocate how our unique community can be sustainable and resilient in the 21<sup>st</sup> century. Green went on to cover the information included in the Phase 1 PowerPoint presentation, which included points regarding the effects of geography on the community, history and significance of gold rush, natural resource mining and the importance of thinking about the future trajectory when considering how to develop the community. He highlighted it is important to honor the past and not lose our identity while implementing some forward-thinking approaches and made notes about how the massive decline in the mainstay industry has affected the community, shown in some of the slides. Green pressed that the City decided instead of having a pity party or building a grudge against the government, they are going to "fight back" against the population projections and statistical predictions about the community. He reviewed the sustainability challenges, noting it often leads to bureaucratic paralysis, but explained how the city chose to react in the past and currently with the Innovation Gateway, which included an update on the new treatment plant work currently in progress and reuse efforts.

Didgette McCracken, Grant County Open Campus Coordinator for the OSU Extension, and Don Merritt, Museum Curator for Kam Wah Chung, presented the slideshow included in the agenda regarding findings from surveys completed by museum visitors. Some of the information presented was other tourism related data that brought context for survey answers. Merritt noted some interesting comments received during the survey and how the State is looking into marketing approaches to reach other demographics like millennials. McCracken noted she will continue to survey the visitors along with possibly adding a hotel survey because her office is looking at potentially expanding for hospitality education. Don Merritt also provided an update on the program the Discovery channel in the US and China are working on following filming, and their efforts toward a worldwide release within the next year. Mayor Lundbom asked if the other 44% of comments were negative, McCracken noted not so much negative as constructive criticism or suggestions. Green noted they have been in negotiations for many months working towards achieving a heritage site and they are very close to reaching an agreement that will allow them to significantly expand the visitor center; it will take a real test of the communities ability to work together to build a new pool—which they plan to have the financials and a path forward by February to present on the ballot in May or November.

Green emphasized that in conjunction with all these amenities, there will need to be investment in hospitality; he noted three months ago, the City started working with the Priday family out of central Oregon, who will be presenting their plans to invest six million dollars in a new hotel in John Day. Court Priday presented the Inn at Whiskey Flat slideshow included in the agenda. He introduced several of the present team members, highlighting their backgrounds and roles in the company. He noted the various locations in central and eastern Oregon and highlighted they always try to be uniquely local in

accommodating and make it feel like it's part of the area with an agricultural motif based on the local history; they go in to small communities and are involved in the communities, to be known beyond a product the town would be proud of, through philanthropic involvement.

There were questions and discussion about proposed site plans including the potential shared investment convention center. Answers from city staff and Priday noted a site is proposed on the west side of Oregon Pine, which is out of the flood area; they hope to transform the neighboring 6,000 square foot saw mill building into a convention center, and they are anticipating it will take a year to complete. There was also discussion about the name "Inn at Whiskey Flat"; Priday noted the name is not set in stone, they used the location from the Chamber's website because they aim for catchy names that are historic to the area and unique. City Manager Green summarized that the city's intent was to lead with public sector investment and attract the private sector investment, the Priday's vision is absolutely in line with those efforts. The city will bring the integrated planning team, landscape architects, civil engineers, surveyors, and local contractors to help their team get off the ground to a positive start. He displayed the Innovation Gateway Area Concept, noting that connecting streets is vital; the city was not selected for the 15-million-dollar BUILD Grant but they will continue working towards achieving the street network developed through the planning process.

Mayor Lundbom recessed the meeting for a short break at 8:06PM.

# <u>Agenda Item No.6 — Ordinance No. 19-770-01, An Ordinance Amending the John Day Comprehensive</u> <u>Plan and Development Code to Adopt Policies and Code for the Innovation Gateway Plan (Type IV</u> <u>Procedure) - AMD-19-01</u>

Mayor Lundbom called the meeting back into order at 8:16PM and City Manager Green noted the Commission will first hold a public hearing on the staff report, then council ratification would follow for the ordinance based on the commission's recommendation.

Chairman Boethin explained the process the commission follows for reviewing items for a public hearing. He emphasized that failure to raise an issue in person or by letter to the planning commission or city council precludes raising that issue at such a time of appeal. He asked that anyone with an issue make sure to raise their concerns in a sufficient manner for the commission to consider. He noted there were sufficient members of the commission to constitute a quorum and opened the public hearing at 8:19PM.

City Manager Green reviewed the staff report and supporting documentation included in the agenda. Chairman Boethin asked if any member of the planning commission wished to declare a conflict of interest. *None appearing*.

There were no comments in favor or in opposition to the proposal, so Chairman Boethin closed the public hearing at 8:24PM and opened for discussion among the commission.

Commissioner Boethin noted the only portion that causes him concern is the possibility of changing the river channel. Commissioner Ledgerwood noted he understood and has heard that voiced by others, mainly those along Luce Long Ditch. City Manager Green stated that he has worked closely with them; specifically, John Morris was appointed to represent them and has participated actively in the planning process. He also noted there are conditions the city is under in regards to riverfront restoration. Firstly, any changes must be permitted through multiple state and federal agencies. Secondly, there must be a

source of funding to engage in any type of riverfront restoration, which they currently do not have, no proposal has been presented to the council for any funding proposals and is conceptual at this point. Third and most important, there cannot be any harm done to the existing interests in the waterway, which includes both upstream and downstream users. Keeping in mind that it was dramatically altered in the past, if they were to restructure a portion of the riverfront, they would have to ensure there are not negative outcomes with regards to water rights or access and they achieve the positive benefits and is not an attractive nuisance. It is not a near term part of the plan but they wanted to include it in the twenty to thirty year plans.

# Commissioner Wilson moved to recommend to the council and the planning commission approve AMD-19-01 based on the findings contained in the staff report. The motion was seconded by Commissioner Ledgerwood and passed unanimously.

Chairman Boethin turned the meeting back to the council. Councilor Smith commented that in their conceptual planning for the river restoration they wanted to make sure it would not affect infrastructure work that the city plans to do, while not definite it was important to make considerations for future restoration to eliminate investment waste if it was ever achieved. Mayor Lundbom added that experts in riverfront restoration suggested it but the council recognizes it is not likely to be achieved anytime soon due to the financing constraints.

Councilor Schuette moved to have the Mayor Lundbom read Ordinance No. 19-177-01 by name only. The motion was seconded by Councilor Haberly and passed unanimously.

Mayor Lundbom read the Ordinance by title only.

Councilor Adair moved to adopt Ordinance No. 19-177-01, An Ordinance Amending the John Day Comprehensive Plan and Development Code to Adopt Policies and Code for the Innovation Gateway Plan. The motion was seconded by Councilor Holland and passed unanimously.

<u>Agenda Item No. 7—Ordinance No. 19-178-02, An Ordinance Amending the John Day Development</u> <u>Code to Comply with Federal Flood Plain Management Regulations Relating to City Participation in the</u> <u>National Flood Insurance Program (Type IV Procedure) – AMD-19-02</u>

City Manager Green explained this is a response to changes in Federal Regulations that affect development constraints of the city.

Chairman Boethin opened the public hearing at 8:32PM. City Manager Green noted this process begin with a study that was completed in 2012, it has taken many years to realize into actual changes to the flood maps.

Chairman Boethin asked if any member of the commission wanted to declare a conflict of interest. None appearing. There were no comments in favor or in opposition to the proposal; Chairman Boethin closed the public hearing at 8:33PM and opened for discussion among the commission. *No further comments*.

Commissioner Ledgerwood moved to recommend city council approve AMD-19-02 based on findings in the staff report. The motion was seconded by Commissioner Toy and passed unanimously.

Councilor Adair moved to have the Mayor read Ordinance No. 19-178-02 by name only. The motion was seconded by Councilor Sky and passed unanimously.

Mayor Lundbom read the Ordinance by title only.

Councilor Smith moved to adopt Ordinance No. 19-178-02, An Ordinance Amending the John Day Development Code to Comply with Federal Flood Plain Management Regulations Relating to City Participation in the National Flood Insurance Program. The motion was seconded by Councilor Haberly and passed unanimously.

Agenda Item No. 8—Ordinance No. 19-179-03, An Ordinance to Adopt Minor Development Code Amendments Related to 1) Broadening the Conditions for Temporary Recreation Vehicle Use as Dwellings in All Zones, and 2) Deleting the Restrictions on Residential Dwellings in the D and GC Zones to Allow Housing Units that Existed Prior to November 24, 2005 but were Occupied as Businesses to Convert Back to Residential Use (Type IV Procedure) – AMD-19-03

City Manager Green noted these amendments were requested by residents; council took it up in several sessions to try to create greater flexibility to accommodate housing choices within the city, recognizing until more buildable land can be opened, they are constrained.

Chairman Boethin opened the public hearing at 8:36PM. He reminded the audience that an issue in favor or opposition must be clarified and sufficient to be raised in an appeal. He asked if member of the commission wished to declare a conflict of interest. *None appearing*. City Manager Green highlighted some of the portions of the staff report explaining how it relieves the restriction on homes in downtown and commercial zones going back to residential use, even if it was a commercial use in 2005. He also explained how it allows temporary RV dwelling in lots under construction and greater flexibility for unique circumstances to allow one RV as a temporary dwelling unit through a conditional use permit.

There were no comments in favor or in opposition to the proposal; Chairman Boethin closed the public hearing at 8:39PM and opened for discussion among the commission. City Manager Green noted that there was a text change due to a typo carryover, striking out language referencing the JDIG from the previous reports in finding number three.

Commissioner Wilson moved to recommend the city council approve AMD-19-03 based on the findings contained in the staff report as amended. The motion was seconded by Commissioner Toy and passed unanimously.

Councilor Schuette moved to have the mayor read Ordinance No. 19-179-03 by name only. The motion was seconded by Councilor Haberly and passed unanimously.

Mayor Lundbom read the Ordinance by title only.

Councilor Adair moved to adopt Ordinance No. 19-179-03, An Ordinance to Adopt Minor Development Code Amendments Related to 1) Broadening the Conditions to Temporary Recreational Vehicle Use as Dwellings in All Zones, and 2) Deleting the Restrictions on Residential Dwellings in the D and GC Zones to Allow Housing Units that Existed Prior to November 24, 2005 but were Occupied as Businesses to Convert Back to Residential Uses. The motion was seconded by Councilor Schuette and passed unanimously. It was noted the amendment to the staff report would already be in effect and does not need to be a part of the council ordinance motion.

# Agenda Item No. 9—Conditional Use Permit (Type III Procedure) CUP-19-01

City Manager Green noted this is the first application under the proposed amendments just passed; once the thirty-day window has lapsed and it becomes effective the conditional use permit could be issues. Chairman Boethin opened the commission hearing for CUP-19-01 at 8:43PM and asked if any members wished to declare a conflict of interest. None appearing. Chairman Boethin reminded that in raising an issue there must be sufficient clarify for an appeal to the city council. He then asked the applicant if they had any further comments. Patty Salvino stated she spoke to the neighbors about their negative comments, informing them it was not in her plans, and they concluded she could place her motorhome anywhere on her property. There were no comments in favor or in opposition to the proposal; Chairman Boethin closed the public hearing at 8:44PM and opened for discussion among the planning commission. Commissioner Ledgerwood clarified the permit was for a one-year period. Chairman Boethin asked about the Oregon Fair Housing Council comments included in the staff report for AMD-19-03. City Manager Green reviewed their concerns and noted they are largely regarding a housing needs analysis, which the city does not have since their Comprehensive Plan was completed in 2003; they did have a housing market assessment completed by ECONorthwest this year, so they referenced the available land supply as documented in that report and identified the number of houses that would be affected or is anticipated to be affected.

Commissioner Ledgerwood moved to approve the CUP application CUP-19-01 based on the findings and subject to the conditions of approval contained in the staff report. The motion was seconded by Commissioner Wilson and passed unanimously.

# Agenda Item No. 10—Conditional Use Permit (Type III Procedure) CUP-19-02

Chairman Boethin opened the hearing for CUP-19-02 at 8:48 and asked for City Manager Green to briefly review the staff report. He explained the application is in response to an employee who was unable to acquire housing within the community and thought it made more sense to make a permanent connection to water and sewer at this location. Green also noted the proposed site is similar to what was done by the Department of Forestry in fully developing two RV sites. Chairman Boethin noted there were no written comments regarding the proposal and the applicant stated they did not wish to add any further comments. There were no verbal comments in favor or in opposition to the proposal in the meeting; Chairman Boethin closed the public hearing at 8:49PM and opened for discussion among the commission. Commissioner Ledgerwood clarified it would be for employees only.

Commissioner Wilson moved to approve the conditional use application CUP-19-02 based on the findings and subject to the conditions of approval contained in the staff report. The motion was seconded by Commissioner Toy and passed unanimously.

# <u>Agenda Item No. 11—Site Design Review (Type III Procedure) SDR-19-01 & Property Line Adjustment</u> (Type II Procedure) PLA-19-08 with Transfer/Sale of Public Property

Chairman Boethin opened the public hearing for SDR-19-01 at 8:50PM and asked City Manager Green to briefly review the staff report. Green highlighted some of the conditions due to complicating situations

from previous site use. Charlie Tracy spoke briefly on the design concept included in the application, noting OTECC has been working on an initiative to facilitate EV ownership and access to communities, like John Day, getting them on the map for tourism that depends on electric vehicles. There were no verbal comments in favor or in opposition to the proposal in the meeting; Chairman Boethin closed the public hearing at 8:54PM and opened for discussion among the commission. *No further comments.* 

# **Commissioner Wilson moved to approve PLA-19-08 based on the findings contained in the staff report. The motion was seconded by Commissioner Toy and passed unanimously.** Green noted the city council needs to have a public comment period for the sale of property to OTECC.

# Councilor Haberly moved to the open public hearing at 8:55PM regarding the property sale. The motion was seconded by Councilor Adair and passed unanimously.

Mayor Lundbom noted they are excited to find a use for the property, removing an eyesore, and asked for details regarding the cost to charge an EV. Mr. Tracy noted it is high speed, they charge for cost recovery of the power utilizing an app that charges monthly similar to a cardlock system. He noted it can add up to 250 miles of range within an hour, giving time to for users to visit downtown while they wait. Tracy also spoke on the stations OTECC currently has and plan to implement in Eastern Oregon, noting he is driving OTECC's first EV tonight. Councilor Sky asked about timeline; he replied that they plan to begin development in the spring. Councilor Smith asked about the number of stations; he replied they will begin with one but anticipate having two or three in the future. Smith also asked about the signage and decorative wall; Tracy explained how the enclosure would look, providing the area needed for large equipment working in the substation requires. Councilor Holland asked for clarification about the city's utility easements. Green noted there will be utility easements and access easements to service the utilities in place before the property line adjustment is finalized. He noted it will also be conditional of not the permit issuance but the approval of the water right transfer. Councilor Holland commented it was a small water right; Green agreed and noted that it is old but he didn't think it was wise to relinquish it if not necessary.

# Councilor Adair moved to close the public hearing at 9:01PM. The motion was seconded by Councilor Holland and passed unanimously.

City Manager Green noted he would bring a transaction document back for the city council approval.

[Note: prior to the conclusion of the Planning Commission hearing, staff noted the SDR was not approved by motion. The Commission re-opened this agenda item and moved to approve SDR-19-01, as submitted. Motion passed unanimously]

# Agenda Item No. 12—Land Partition (Type III Procedure) LP-19-02

Commissioner Boethin opened the hearing for LP-19-02 at 9:01PM, reviewing the proposed application and asked City Manager Green to review the staff report. Green noted Land Partitions typically don't go in front of Commission unless the size warrants. He reviewed some of the conditions that must be met with regard to the final plat. Chairman Boethin asked if Sandy Gilson would like to add any comments.

Gilson noted the Elk's lodge has a 50-year agreement with the Senior's Center to lease space at their site. They are concerned about decline of lodge membership, and want to be guaranteed if the paternal organization went away the Senior's Center would not lose their building because they don't own the

land. If dissolved, the Chicago grand lodge would take over. They would like to provide security for the Senior's Center, as they provide an essential service to the community. They plan to do an access easement and usage agreement under this with the Senior's Center for parking. They would also like to note the third lot is being created as a space for sale and future development.

Chair Boethin opened the public hearing and asked if anyone in the audience wished to speak in favor. *None appearing.* Boethin asked if any opposed. *None appearing.* The commission moved to close the public portion at 9:05PM and open to members of commission for discussion.

# Following discussion, Commissioner Wilson moved to approve LP-19-02 as submitted. Second by Toy. Unanimous.

No further matter brought before planning commission.

# Agenda Item No. 13—Other Business & Upcoming Meetings

Mayor Lundbom noted the –Sherrie Rininger on behalf of merchants asked if member of the council would like to judge the decorating competition on November 29<sup>th</sup> at 6PM. Councilor Sky and Mayor Lundbom noted they would attend.

Mayor Lundbom reviewed the upcoming meetings and checked in with staff and council. *There were no further comments.* 

# <u>Adjourn</u>

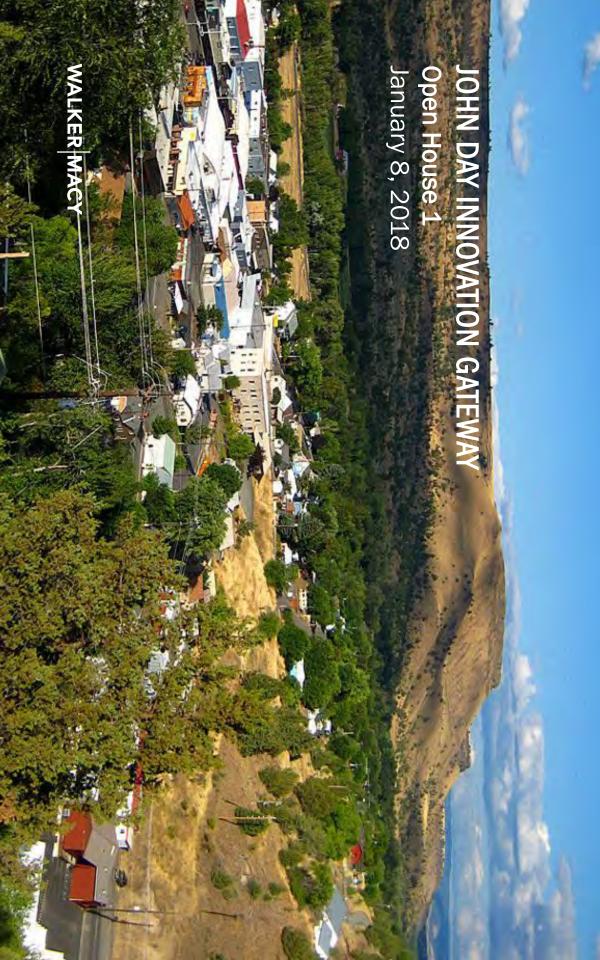
Councilor Schuette moved to adjourn the meeting at 9:08PM. The motion was seconded by Councilor Haberly and passed unanimously.

**Respectfully Submitted:** 

Nicholas Green City Manager

ACCEPTED BY THE CITY COUNCIL/PLANNING COMMISSION ON DECEMBER 10, 2019

Ron Lundbom, Mayor



# Q&A

**Existing Conditions & Opportunities** 

John Day Landscape & Identity

Vision Statement & Guiding Principles

**Project Overview** 

# Walker Macy

\_andscape Architecture & Urban Design Aaron Maples Mike Zilis Ken Pirie Tiffany Swift

# DKS

Transportation Kevin Chewuk

# **J L**

Public Involvement Kristen Kibler

# Interfluve

**River & Stream Restoration** Gardner Johnston

# Angelo Planning Group

Land Use Planning Matt Hastie

# Johnson Economics

Economic Opportunity Assessment Jerry Johnson

# ECO Northwest

Economic Development and Strategy Bob Parker Terry Moore Matt Craigie

# Bell + Funk

Branding Jen Bell

to foster long term economic health Leading with public investment

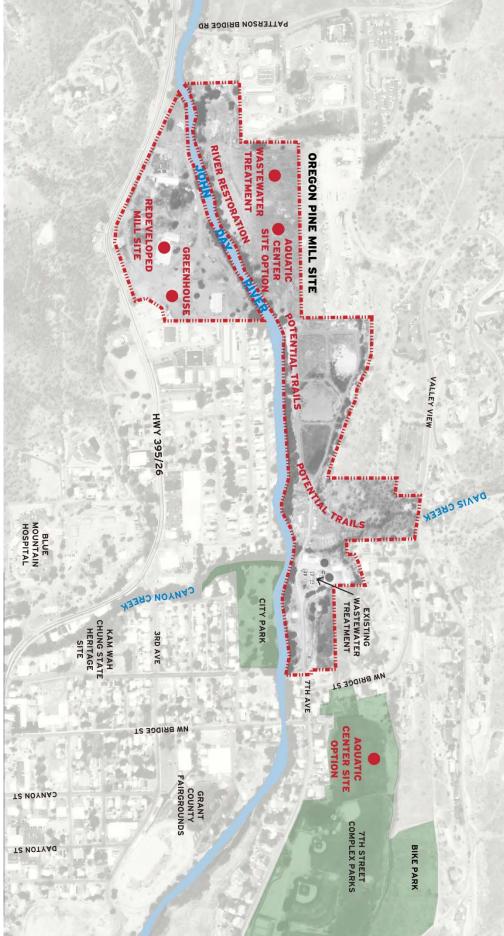


focal point for the community. 83-acre riverfront property to function as a gateway and project is to plan and coordinate redevelopment of the The purpose of the Innovation Gateway Area Plan

to integrate transportation and development solutions Oregon Pine mill property and adjacent City-owned land The Project will focus on redevelopment of the former

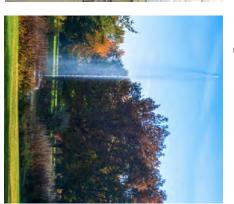
to guide the realization of the vision. Final result is a Plan document and supporting policies





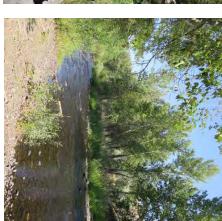
# **PROJECT ELEMENTS**

# **Botanical Garden**



Aquatic Center

River and floodplain restoration







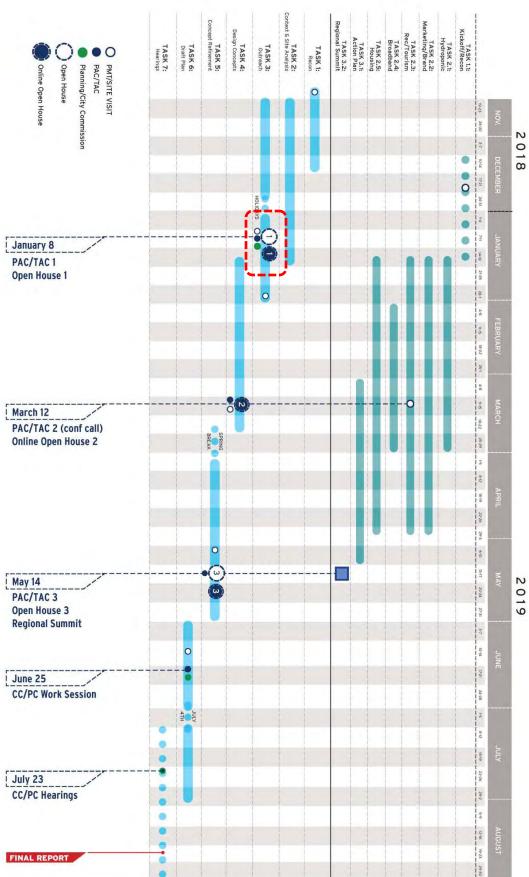


Greenhouses and agricultural research

Wastewater Treatment Plant



Trails



ODOT TGM GRANT-FUNDED PROJECT (WALKER MACY)

ECONOMIC DEVELOPMENT PROJECT (ECONW)

**PROJECT SCHEDULE** 

through restored floodplain habitat along the banks of the John will be connected to the rest of the city with public trails properties as a dynamic and thriving public space. The Gateway revitalization of the former Oregon Pine mill site and adjacent urban design plan that will envision and support the An integrated community development, transportation and

Day River.

VISION STATEMENT

Day River. through restored floodplain habitat along the banks of the John will be connected to the rest of the city with public trails properties as a dynamic and thriving public space. The Gateway revitalization of the former Oregon Pine mill site and adjacent urban design plan that will envision and support the An integrated community development, transportation and

range of residents and businesses. sustainable and resilient economy that retains and attracts a innovation and efficiency, build partnerships and spur a linked within the City's Strategy for Growth to maximize vision for the use of reclaimed wastewater, offer significant New land uses, including recreational facilities and an exciting public investments for John Day's future. These are cohesively

# The Gateway Project should:

- Create a destination
- Attract investment and jobs
- Honor John Day's identity and character
- Promote community
- Create opportunities for walking and biking
- Restore public access to the John Day River and Canyon Creek
- Efficiently use public resources and land
- Support innovation in conservation

"...keep what is great about our community and enhance it."

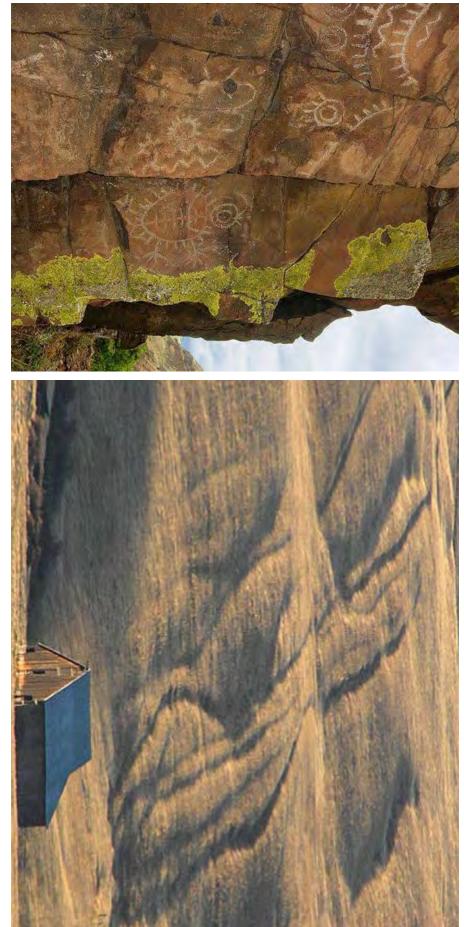
**GUIDING PRINCIPLES** 

THE JOHN DAY LANDSCAPE

# **REGIONAL IDENTITY & HISTORY**

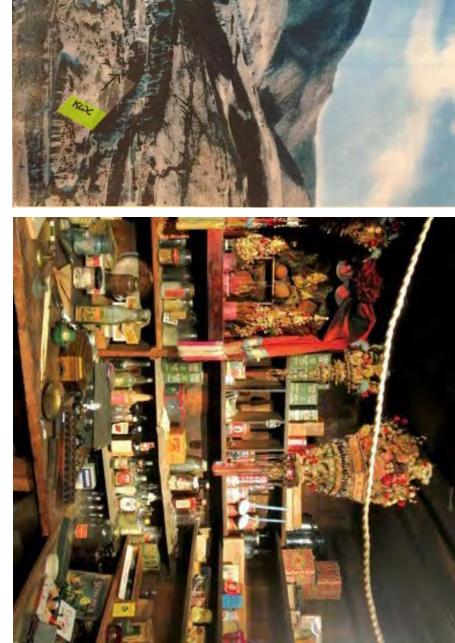
Dalles Military Road & Oregon Trail





# **REGIONAL IDENTITY & HISTORY**

Kam Wah Chung

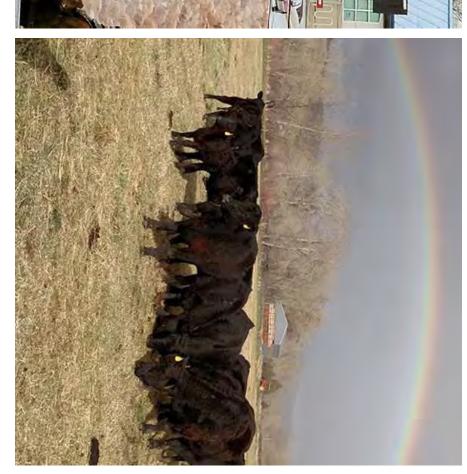


1939 Gold Dredge Mining

# **REGIONAL ECONOMY: Natural Resources**

Loop Ranch

N

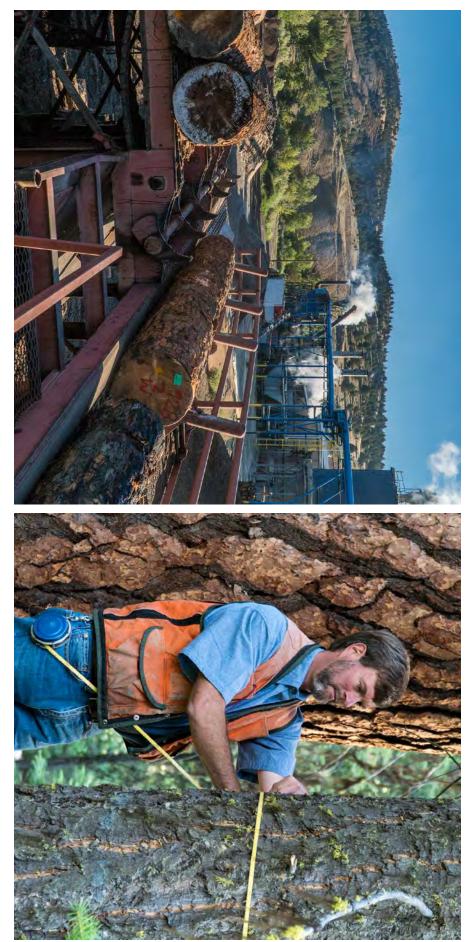




# **REGIONAL ECONOMY: Natural Resources**

**Blue Mtn Forest Partners** 





Biomass

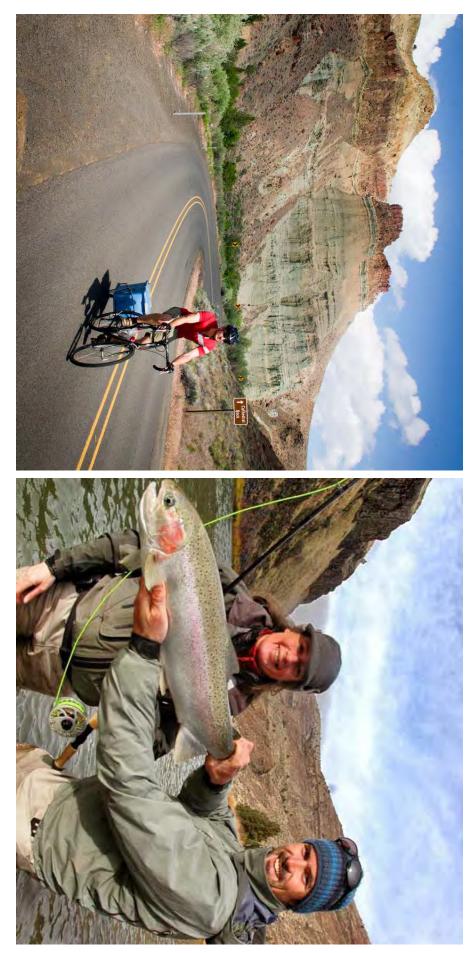


Silvies Valley Ranch, Seneca, OR

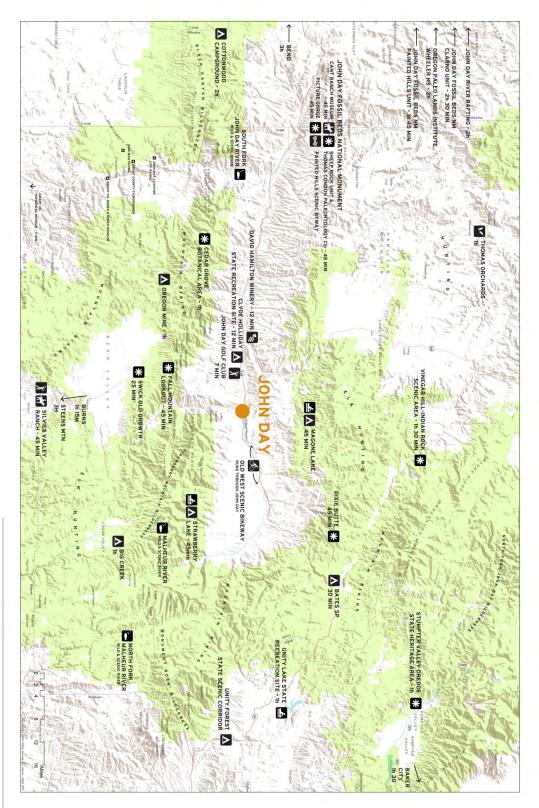


Steelhead fishing on the John Day River

# Voyage Through Time Scenic Byway







# Family-focused

# **Rural values**

# Pragmatic

# Resilient

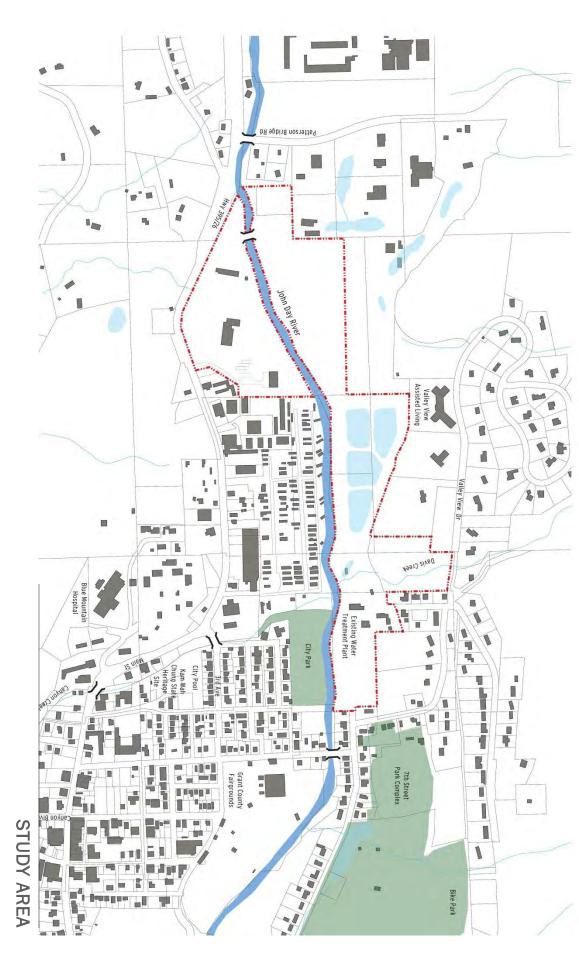
Independent

Terms used to describe John Day:

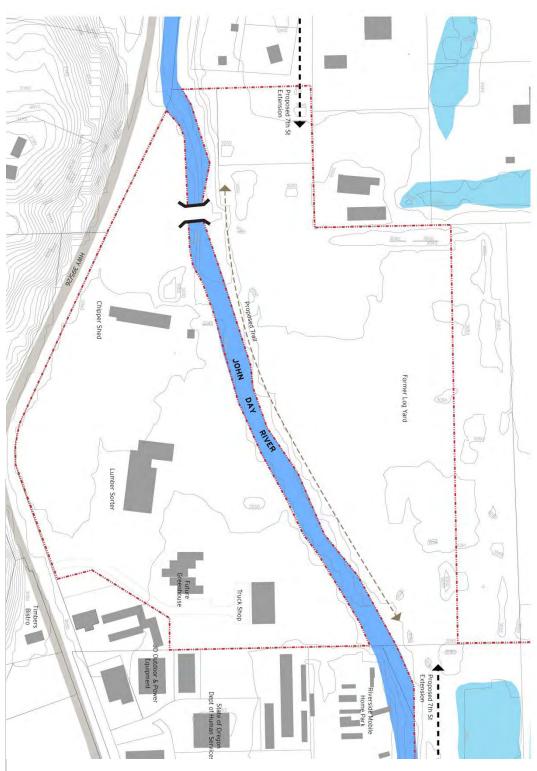
Self-reliant

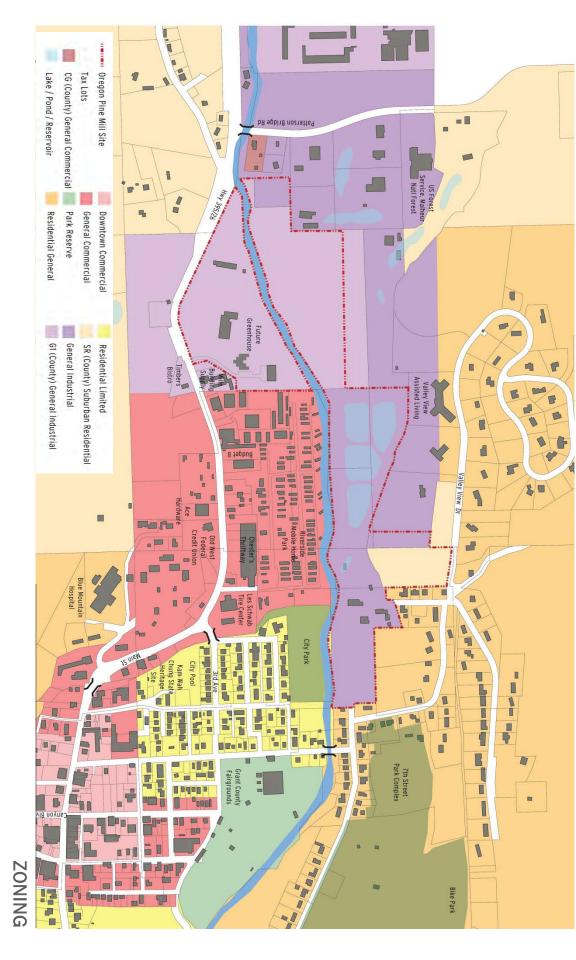
development? What aspects of John Day's character are most important to recognize as we plan for future economic













**EXISTING CONDITIONS: Rivers** 

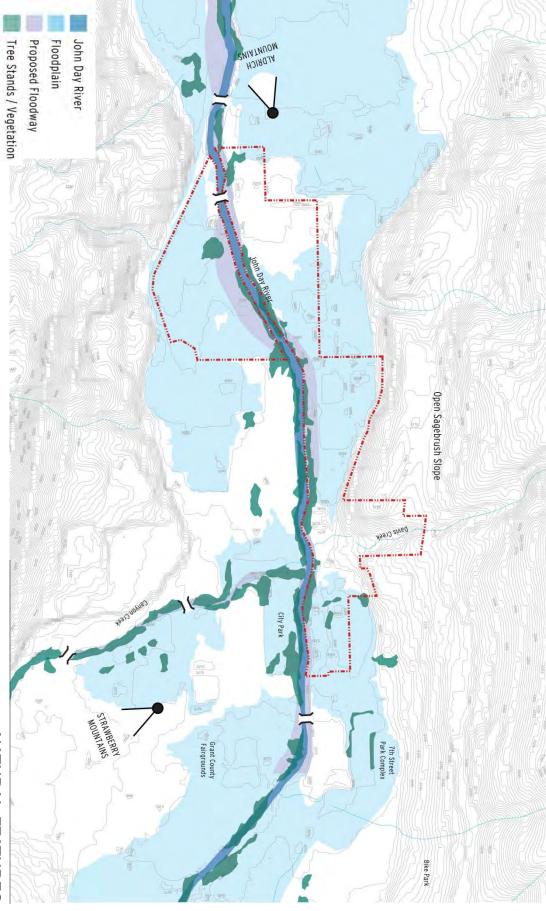
## **EXISTING CONDITIONS: Natural Features**

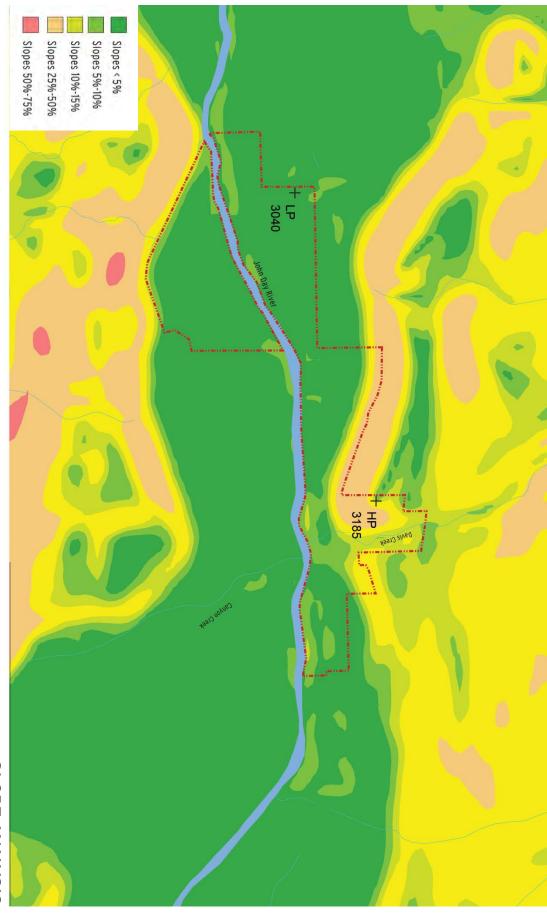
Strawberry Mountains





#### NATURAL FEATURES





SLOPE ANALYSIS



**River Restoration** 







## **EXISTING CONDITIONS: Mill Structures**

**Chipping Shed** 









### **OPPORTUNITIES: Mill Structures**

Pybus Public Market, Wenatchee, WA



## **EXISTING CONDITIONS: Auto Circulation**

### **Connection to Patterson Bridge Road**

#### Old Mill bridge

7<sup>th</sup> Avenue route













North Riverbank conditions

Route of trail around wastewater ponds





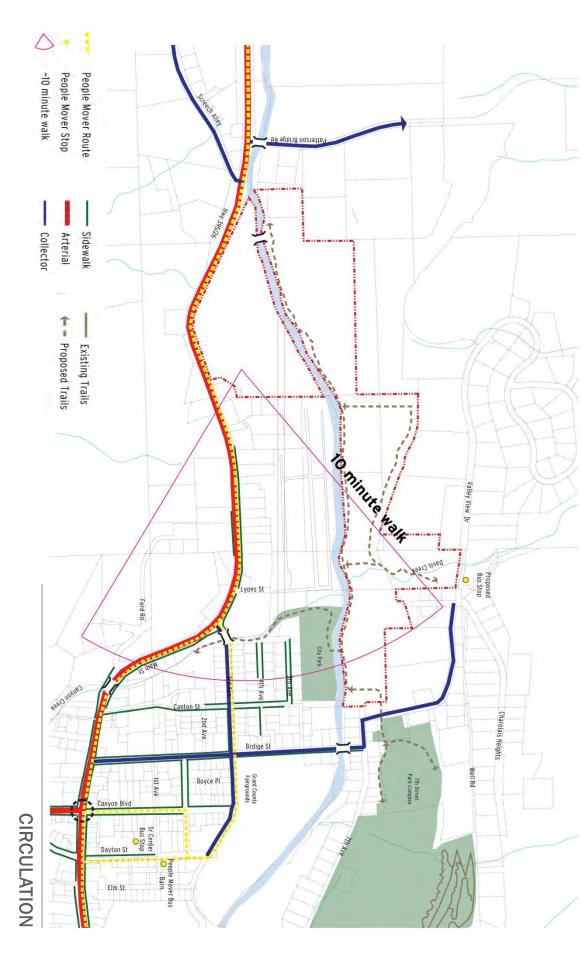
City Park/Canyon Creek trail to KWC

Mountain Bike trail park

Potential Davis Creek trail







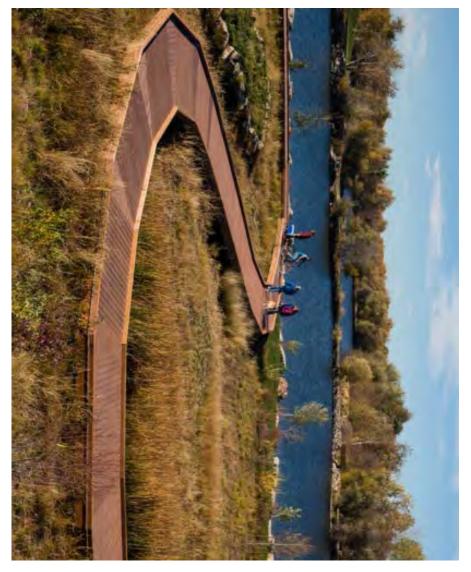
### **OPPORTUNITIES:** Circulation

Truckee River Trail, Lake Tahoe

Signage









Trail, John Day Fossil Beds FRONT TRAILS



**DISCUSSION: EXISTING CONDITIONS** 

What else should we know about the Gateway area?

• What's your vision for John Day in 2035?

Survey: https://www.surveymonkey.com/r/XXL62RV

March 12: PAC/TAC 2, Online Open House 2

May 14: PAC/TAC 3, Open House 3, Regional Summit



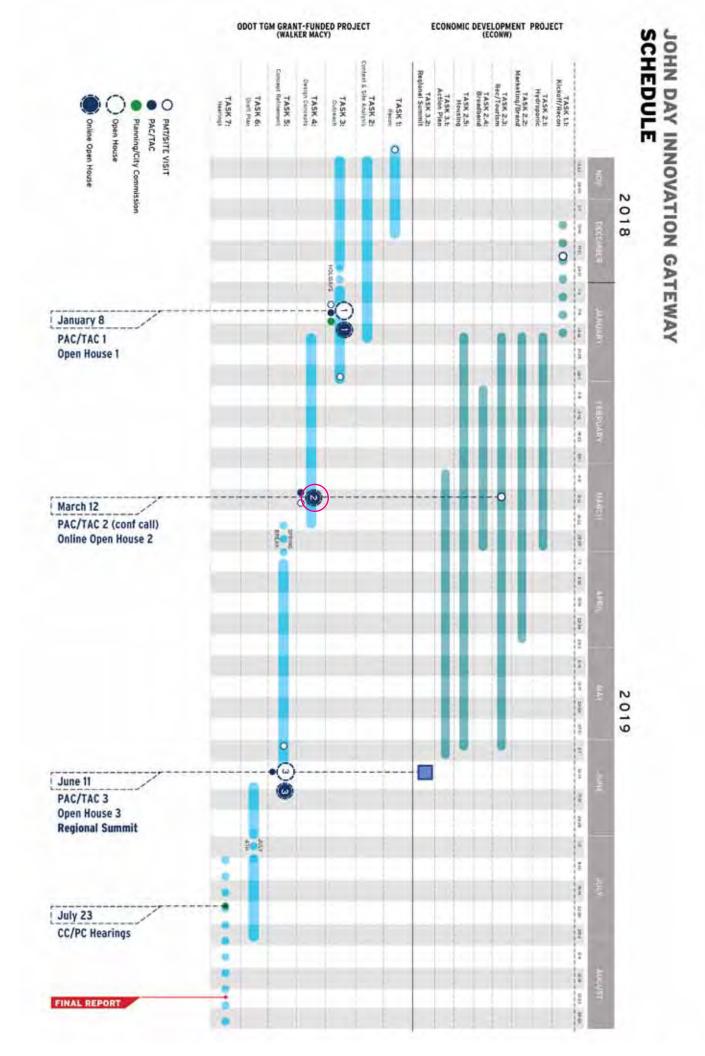
### WORKSHOP #2 March 12, 2019 JOHN DAY INNOVATION GATEW











#### Walker Macy

Landscape Architecture & Urban Design Ken Pirie Mike Zilis Aaron Maples Tiffany Swift

#### DKS

Transportation Kevin Chewuk

#### JLA

Public Involvement Kristen Kibler

#### Interfluve

River & Stream Restoration Gardner Johnston

### Angelo Planning Group

Land Use Planning Matt Hastie

### Johnson Economics

Economic Opportunity Assessment Jerry Johnson

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- 3. Honor John Day's identity and character
- 4. Promote a connected and healthy community
- 5. Create opportunities for walking and biking
- 6. Provide public access to the John Day River and
- Canyon Creek
- 7. Efficiently use public resources and land; and
- 8. Support innovation in conservation

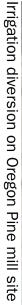








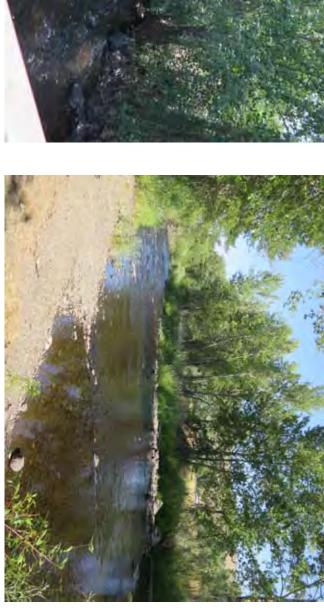
EXISTING RIVER CONDITIONS











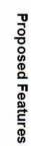


Canyon Creek

## RIVER RESTORATION CONCEPT: "go big"

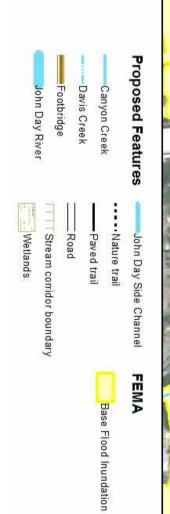


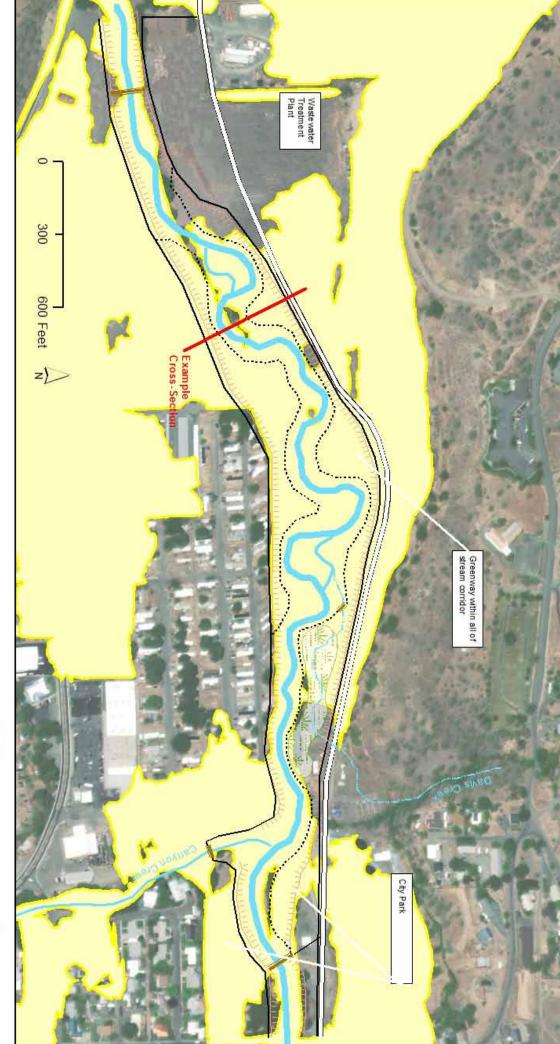
----Nature trail John Day Side Channel

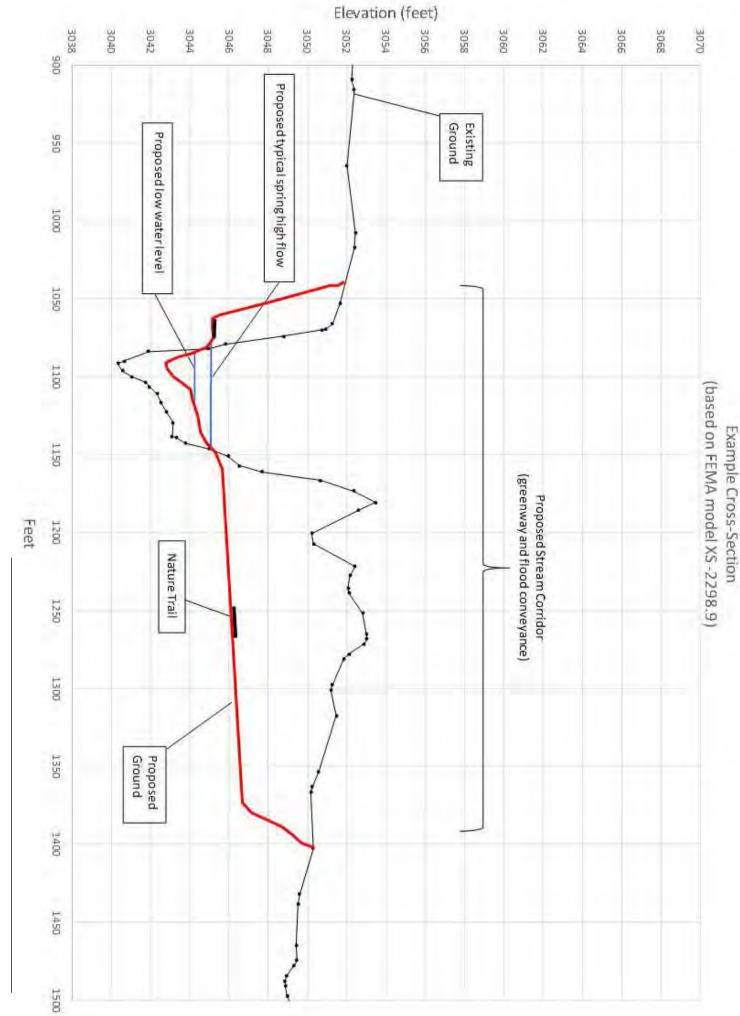




## RIVER RESTORATION CONCEPT: "go big"

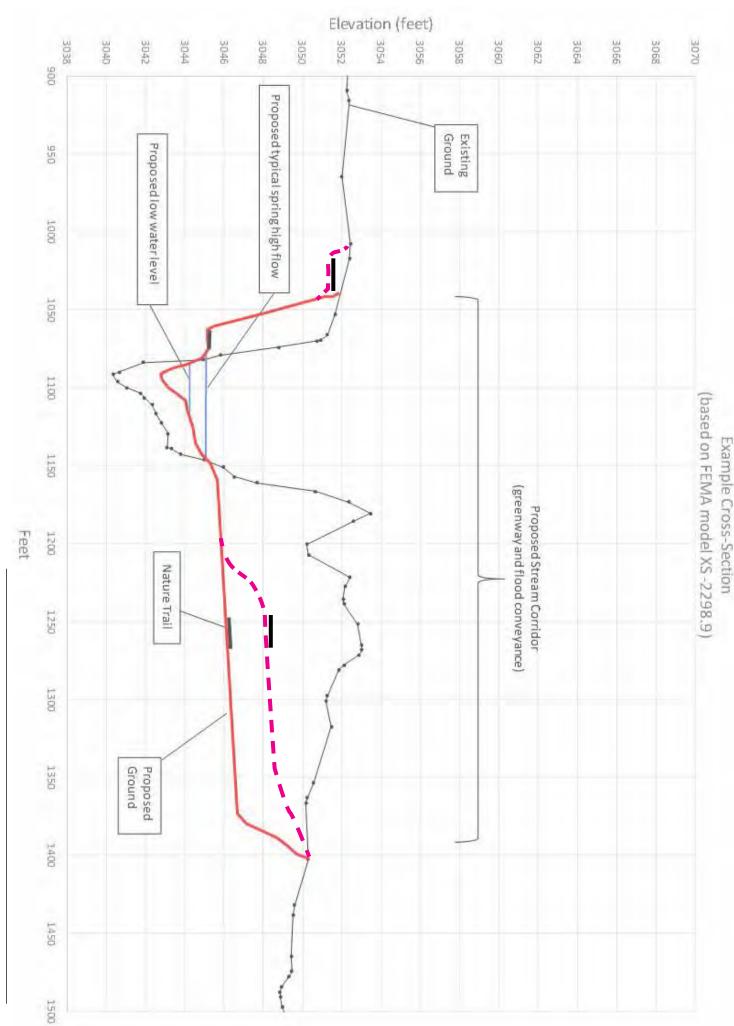




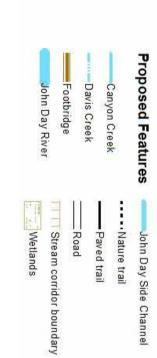


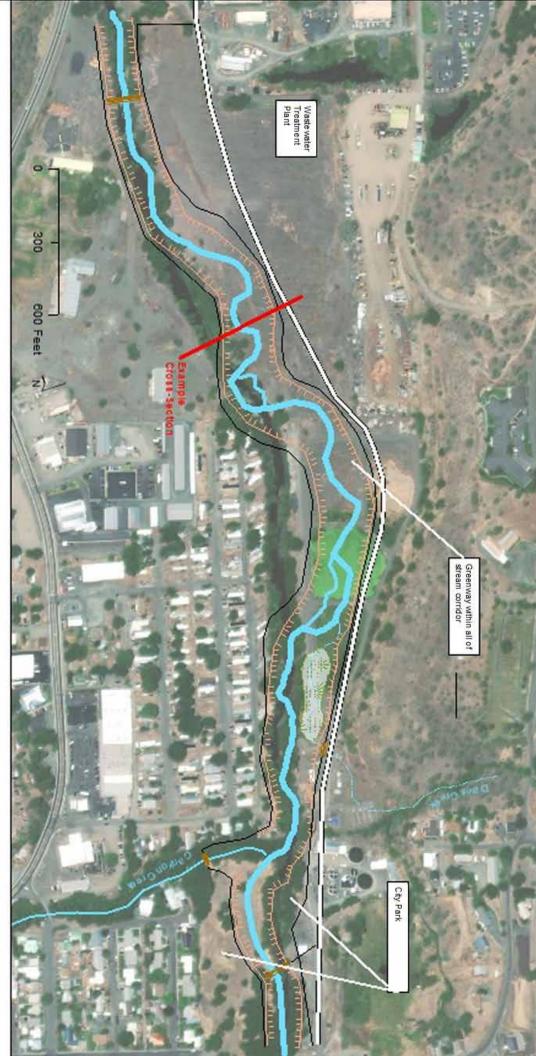
**RIVER RESTORATION CONCEPT** 



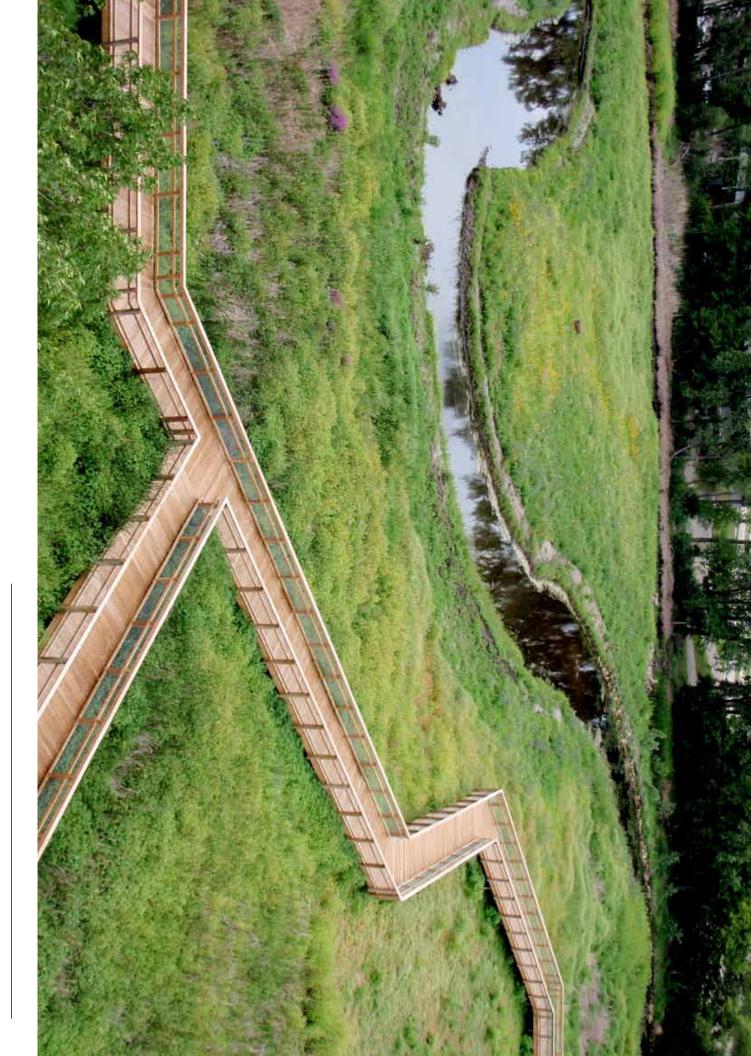


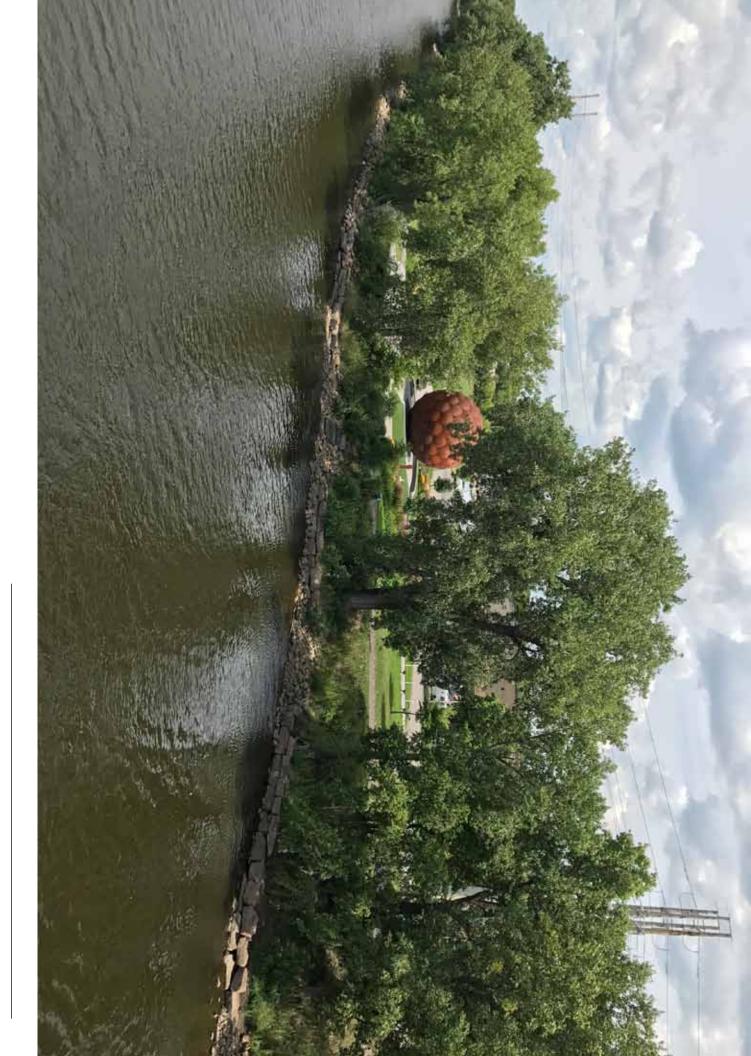
## RIVER RESTORATION CONCEPT: "narrow corridor"





# RIVER RESTORATION CONCEPT: Minnehaha Example



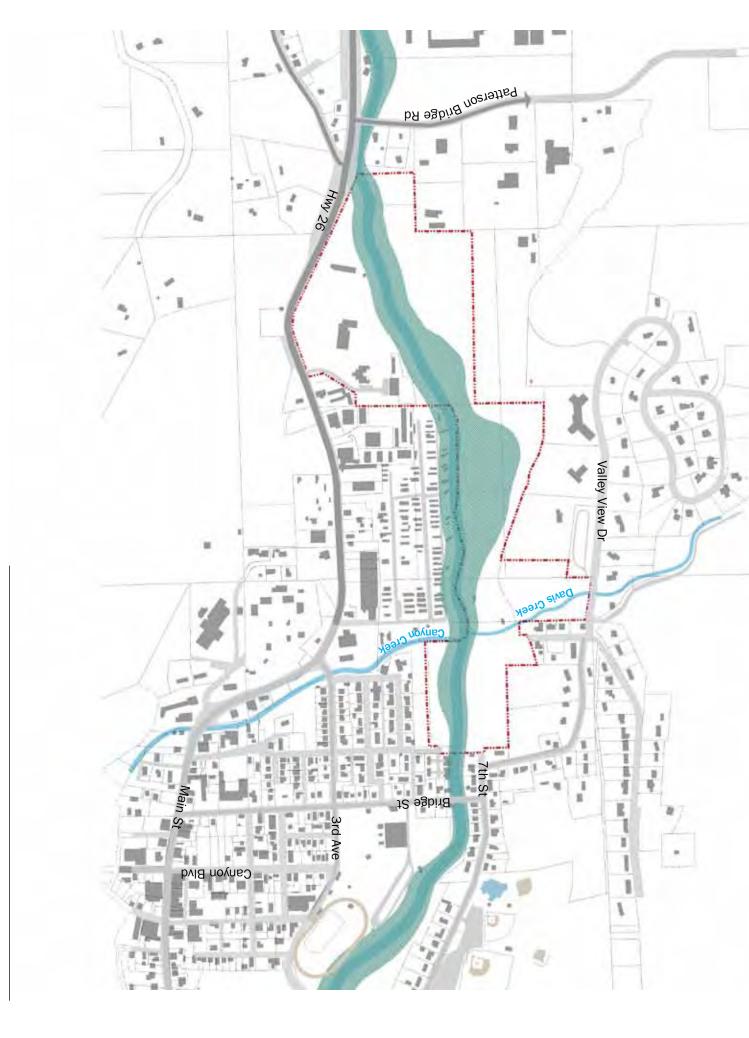


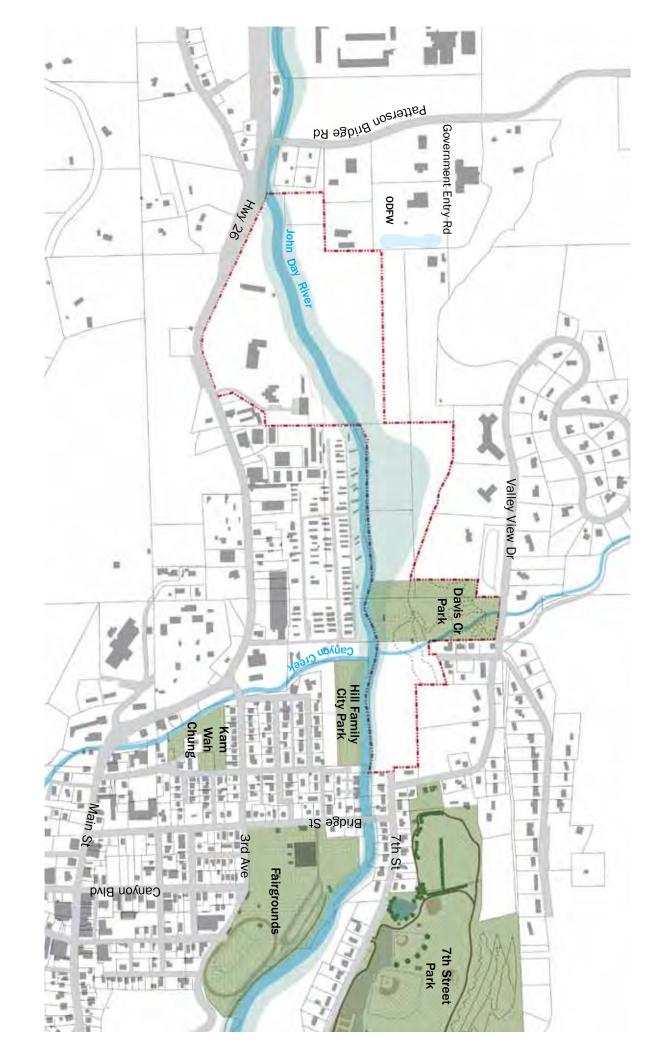
#### Benefits

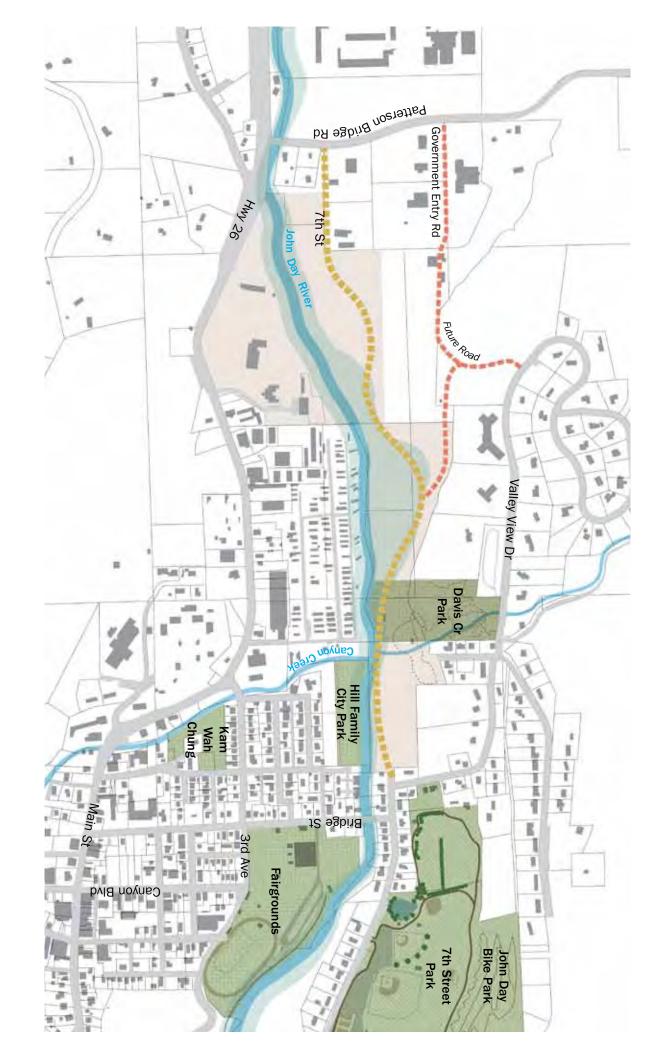
- 1. Overall increased flood conveyance is likely to reduce flood impacts to nearby infrastructure during large flood events
- 2. New inset floodplain surfaces would have more frequent inundation to better mimic natural river conditions, and can also be utilized for multiple recreational uses
- 3. Potential to create constructed wetlands to reduce need for City stormwater treatment
- 4. More recreational trails and access to natural area
- 5. Improved fish habitat
- 6. Additional access for fishing
- 7. Enhanced land values for neighboring properties
- 8. Improved visual identity for City
- 9. No impacts to current irrigation diversion

# Considerations for Future Study / Challenges

- 1. Current wastewater ponds need analysis and permitting to be integrated with river restoration
- 2. Past gold dredging left unstable subsurface conditions, need more geotechnical study
- 3. Needs additional hydrologic engineering feasibility study
- 4. City could still build the proposed improvements without river restoration, but there would be less visual and recreational benefit



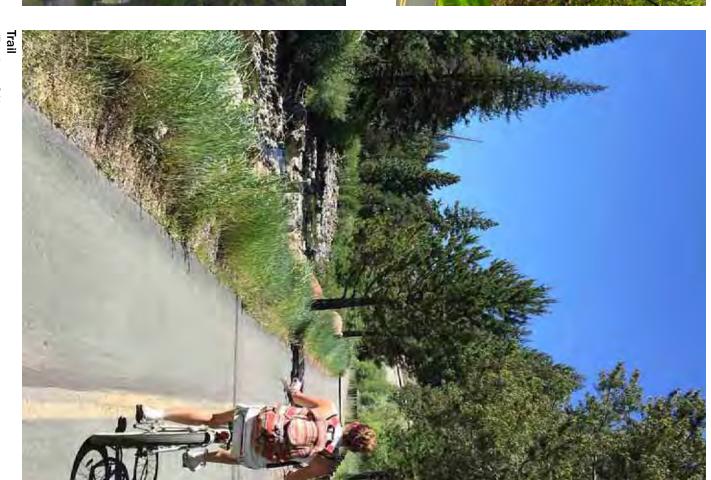






**7TH STREET - 'PARK STREET'** 

(Truckee, CA)

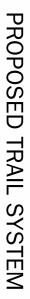


**Parkway** (Natchez Trace, Tennessee)



**Parkway** (Yosemite NP) 125



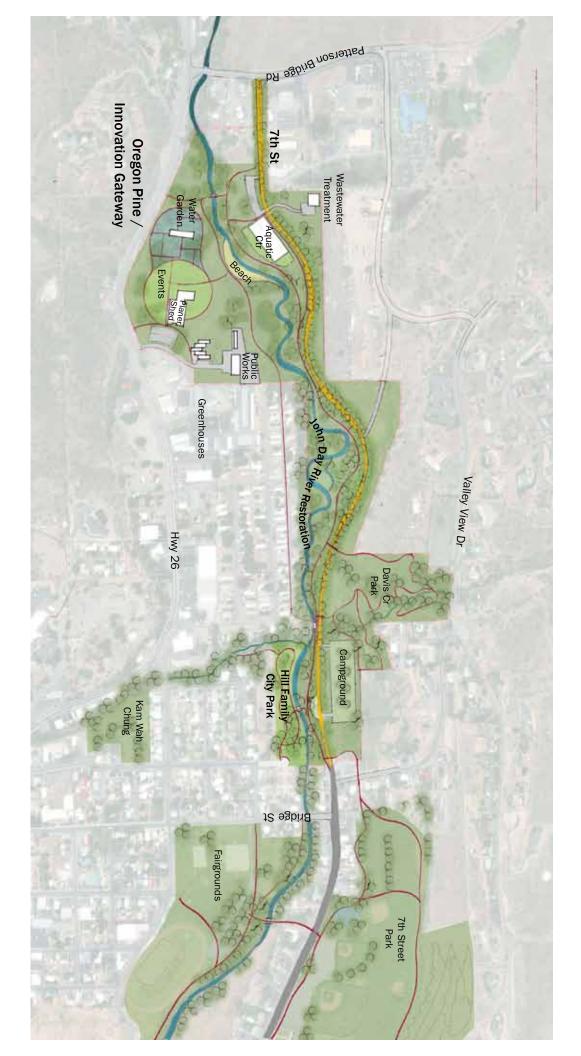


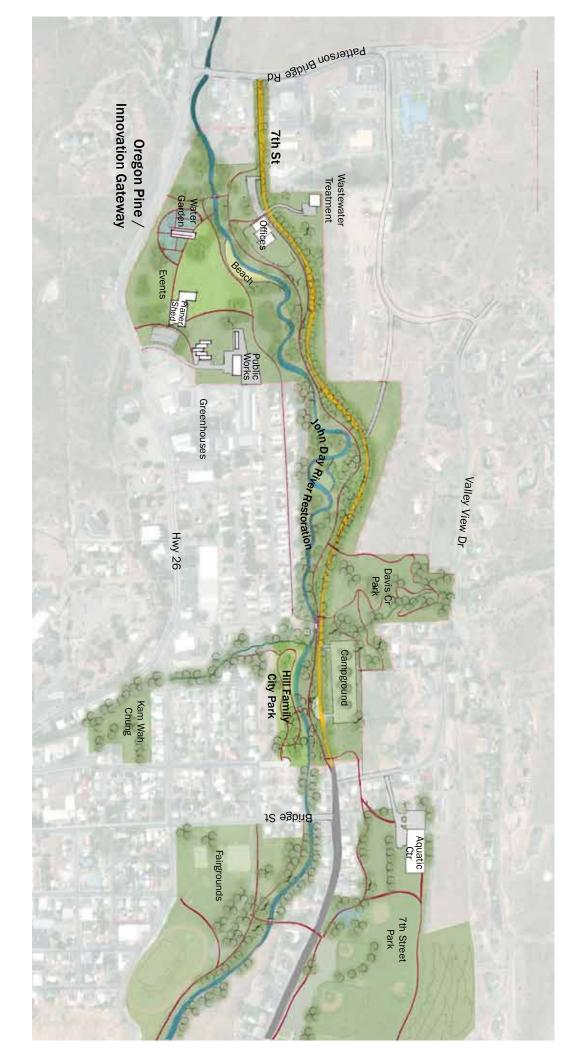


---- Proposed On Street Pedestrian Connection

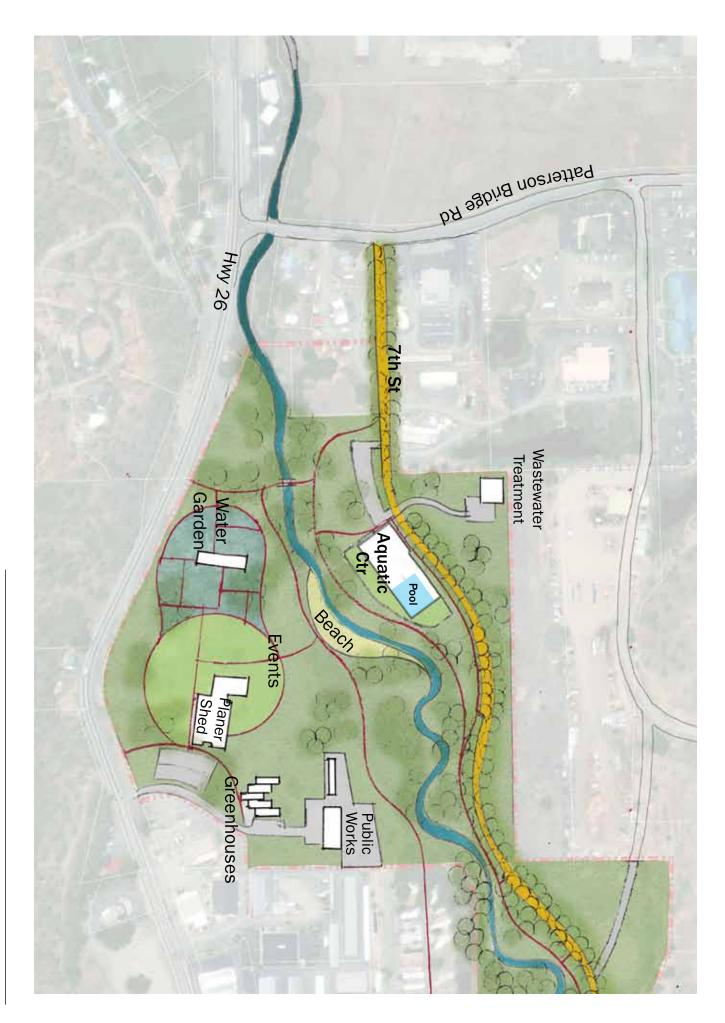
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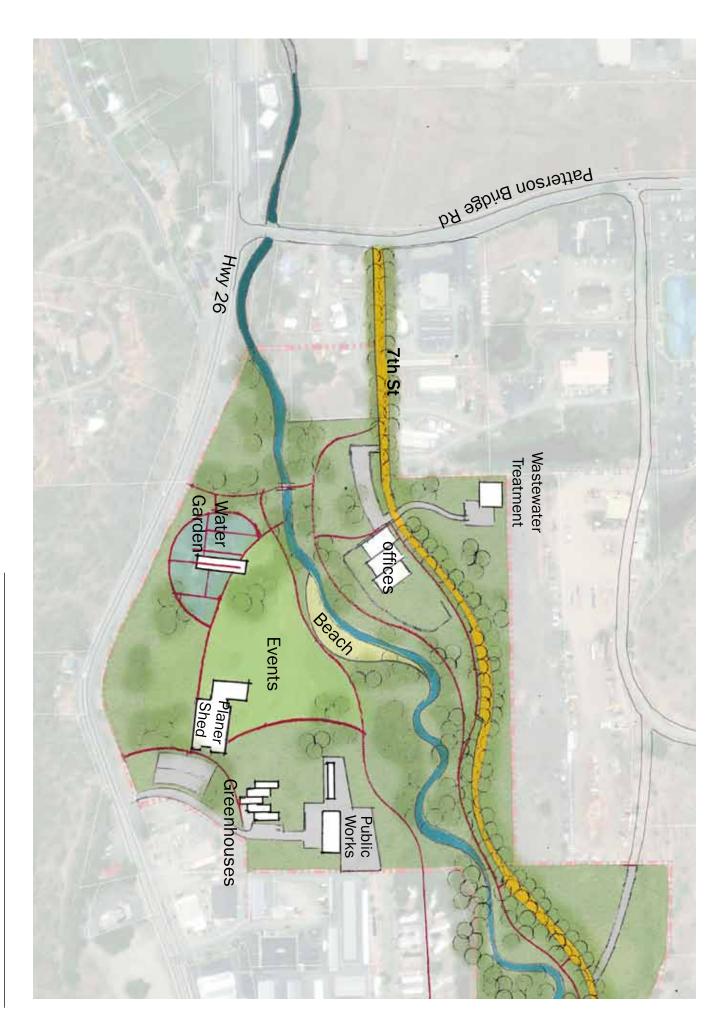






















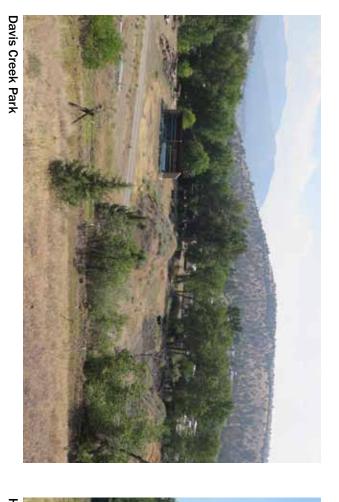
# CENTRAL REACH: EXISTING CONDITIONS

Future route of 7th Street, looking west





**Current Wastewater Treatment Ponds** 

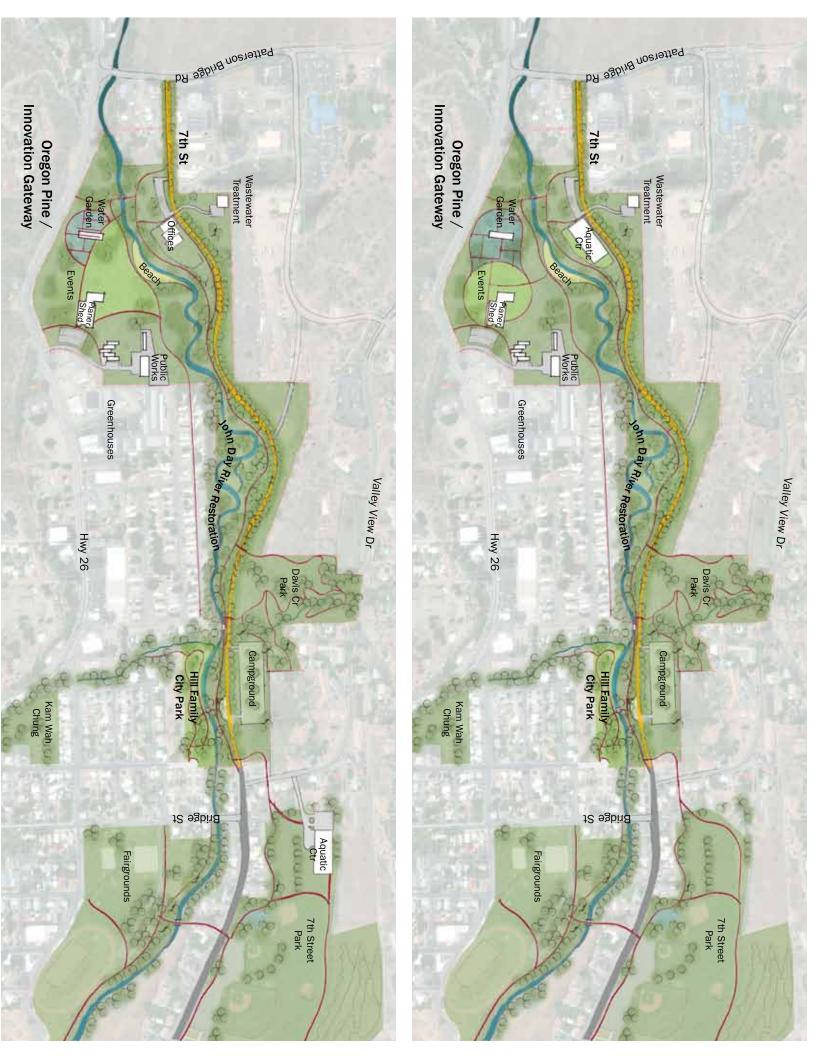


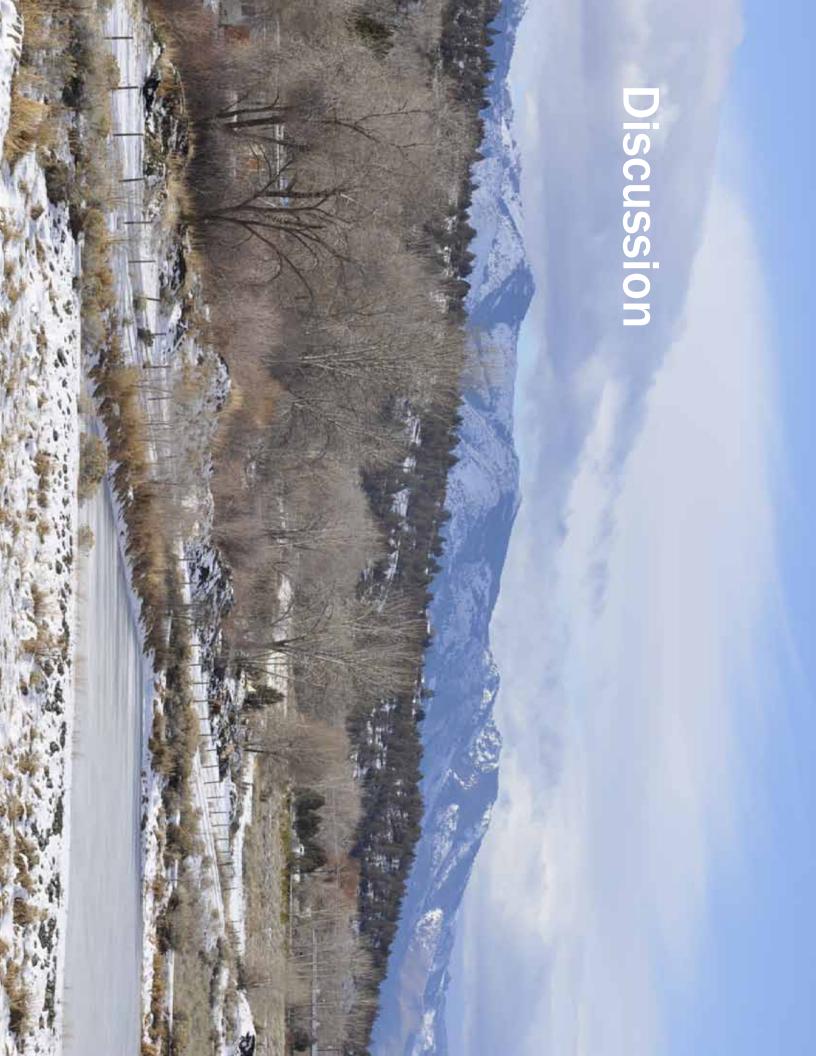


Hill Family City Park site



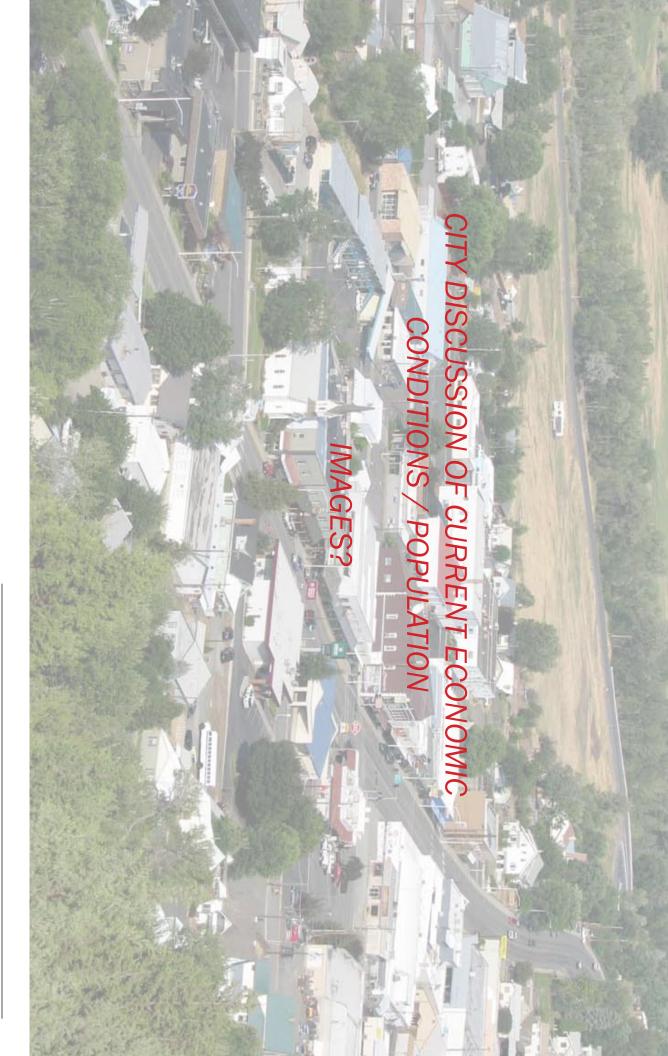


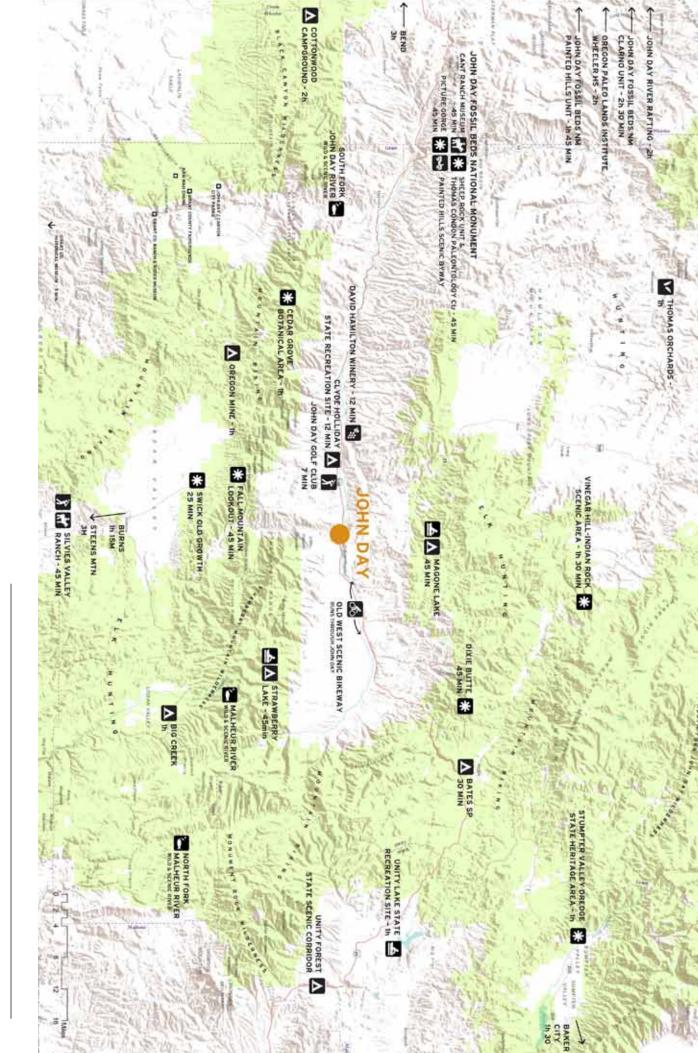




JOHN DAY INNOVATION GATE Regional Economic Diversification S June 11, 2019 

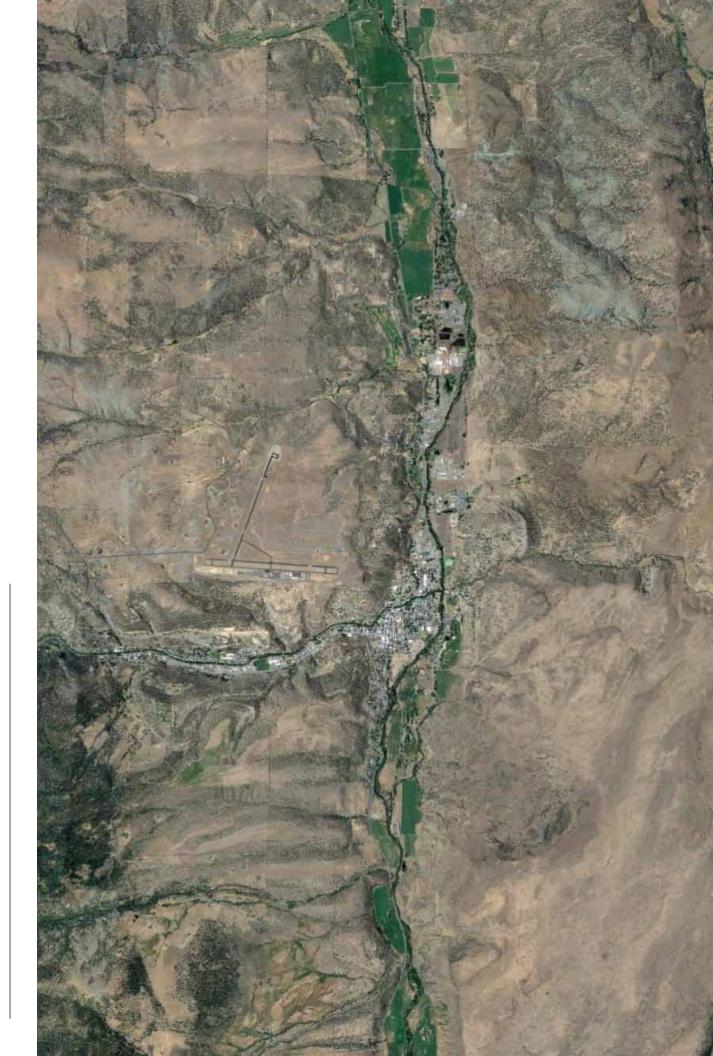
#### ECONOMIC CONTEXT





**REGIONAL RECREATION CONTEXT** 

### CHARACTER OF JOHN DAY



# The Gateway Project should:

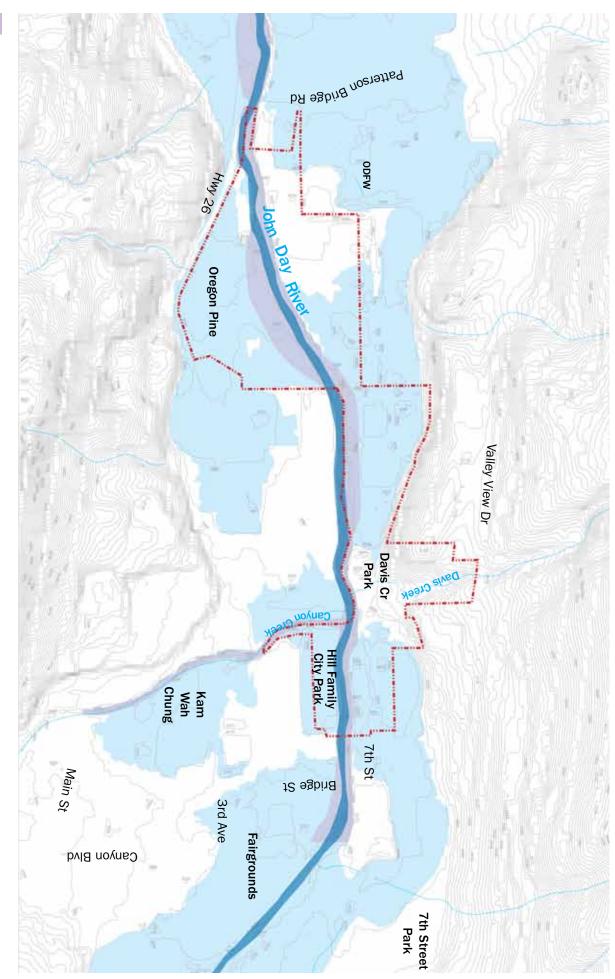
- 1. Create a thriving destination
- 2. Attract investment and jobs
- 3. Honor John Day's identity and character
- 4. Promote a connected and healthy community
- 5. Create opportunities for walking and biking
- 6. Provide public access to the John Day River and
- Canyon Creek
- 7. Efficiently use public resources and land; and
- 8. Support innovation in conservation



# **Project Components:**

- RIVER RESTORATION
- PARKS/OPEN SPACES + TRAILS
- 7th STREET EXTENSION

FLOODWAY 100 YR FLOOD PLAIN



#### **Historic Mining Operations**



**Existing Conditions** 

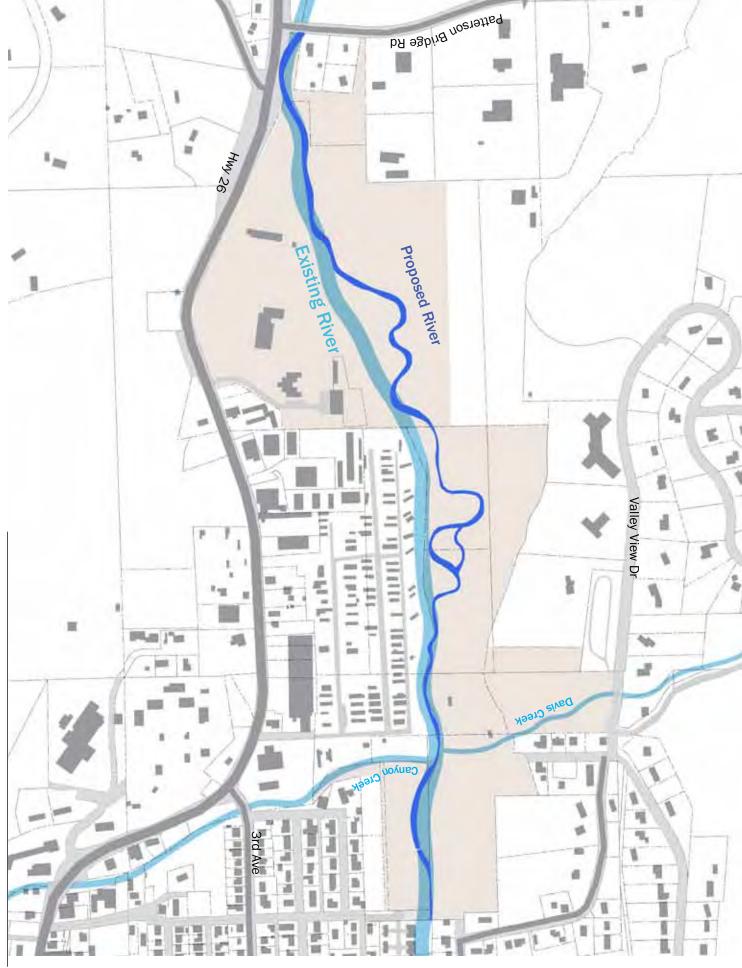


### **River Restoration Concept Example**



JOHN DAY RIVER RESTORATION

## JOHN DAY RIVER RESTORATION





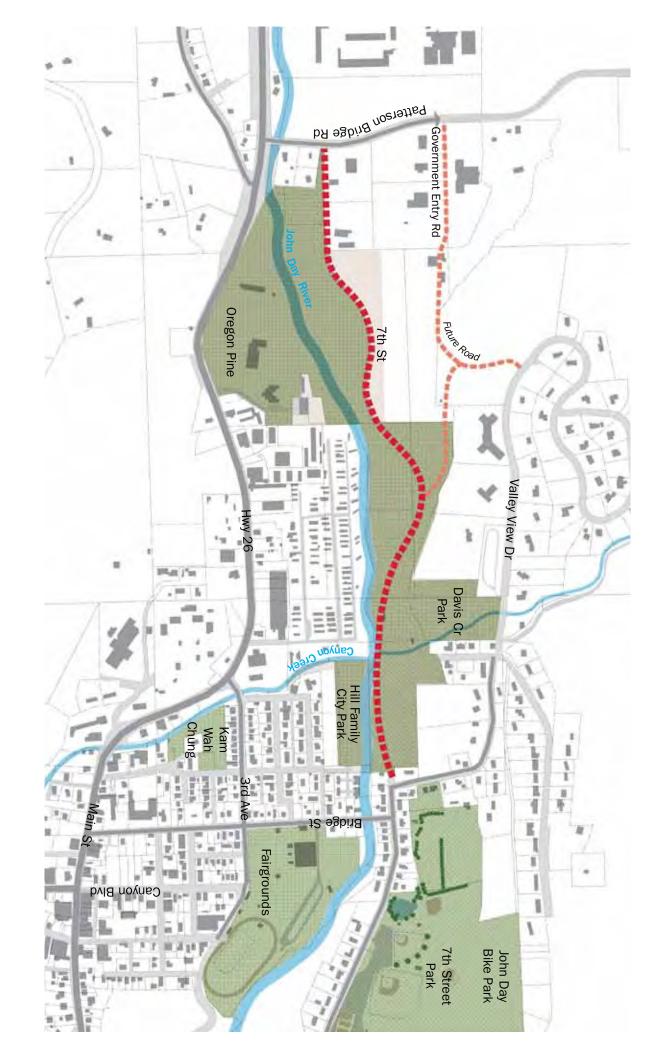
# PROPOSED PARKS/OPEN SPACES & TRAILS

Proposed Trail

I

Proposed On Street Pedestrian Connection







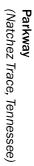
**7TH STREET EXTENSION- 'PARK STREET'** 

(Truckee, CA)

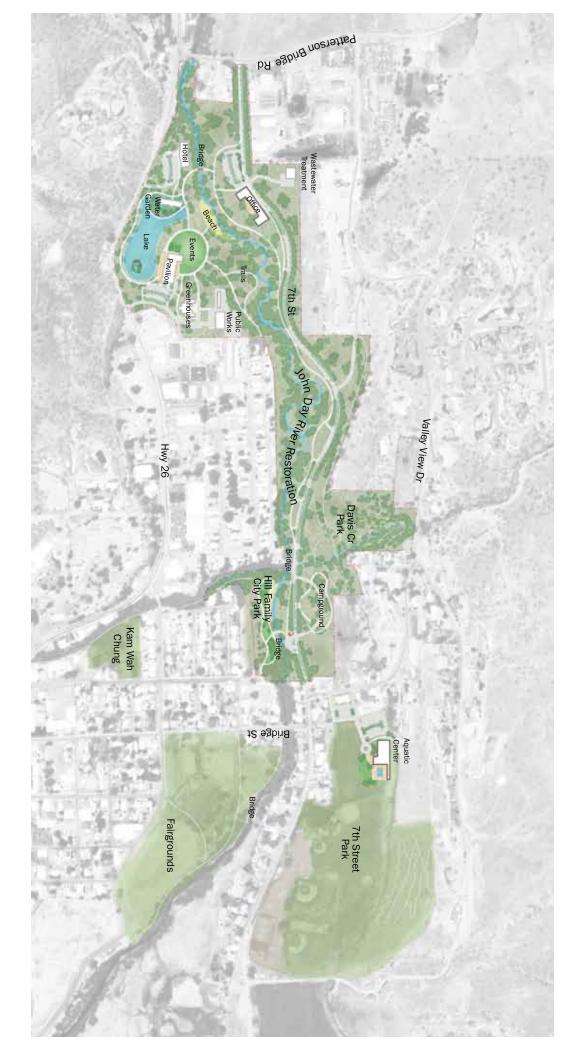






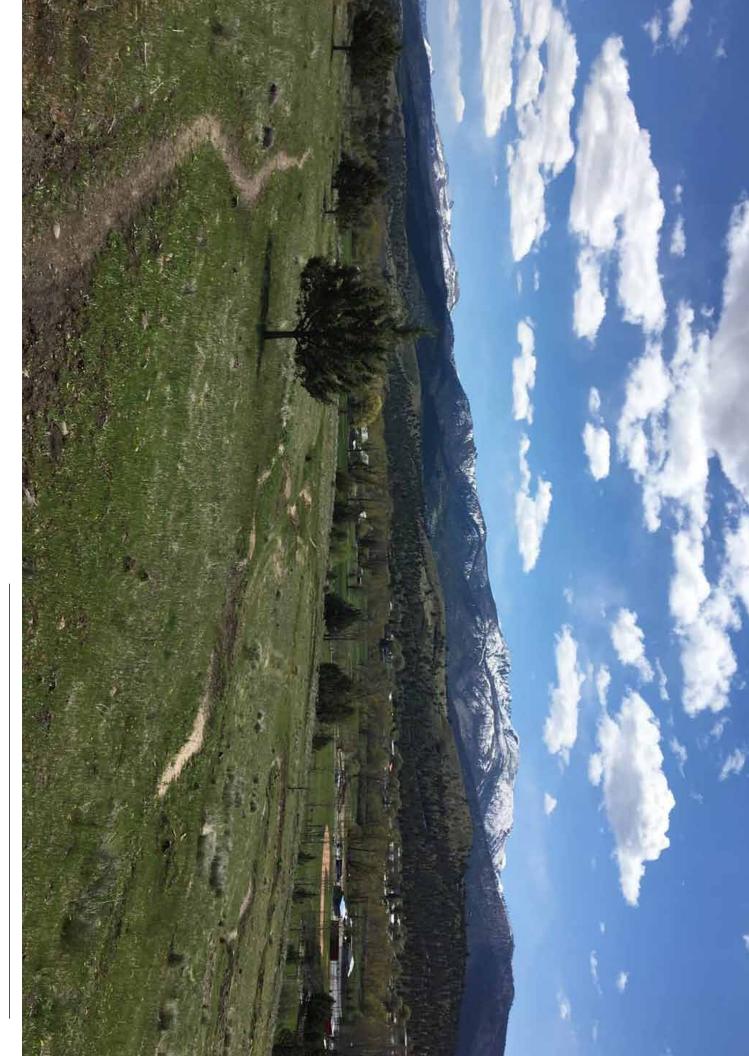


**Parkway** (Yosemite NP)

















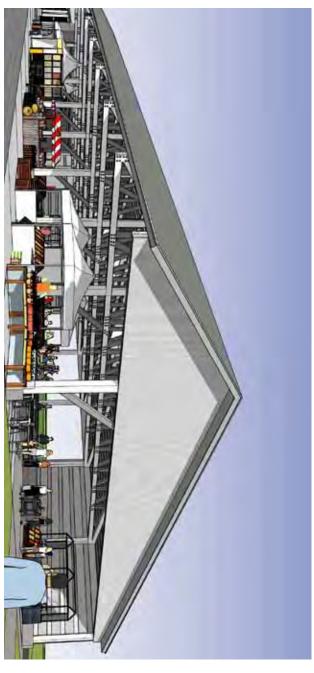


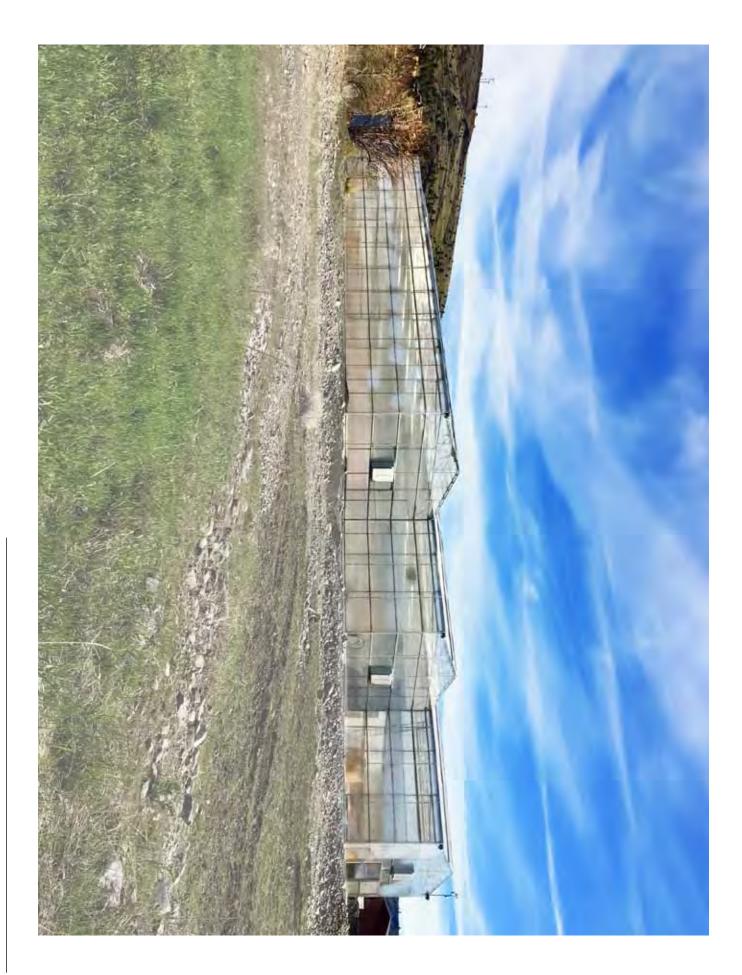




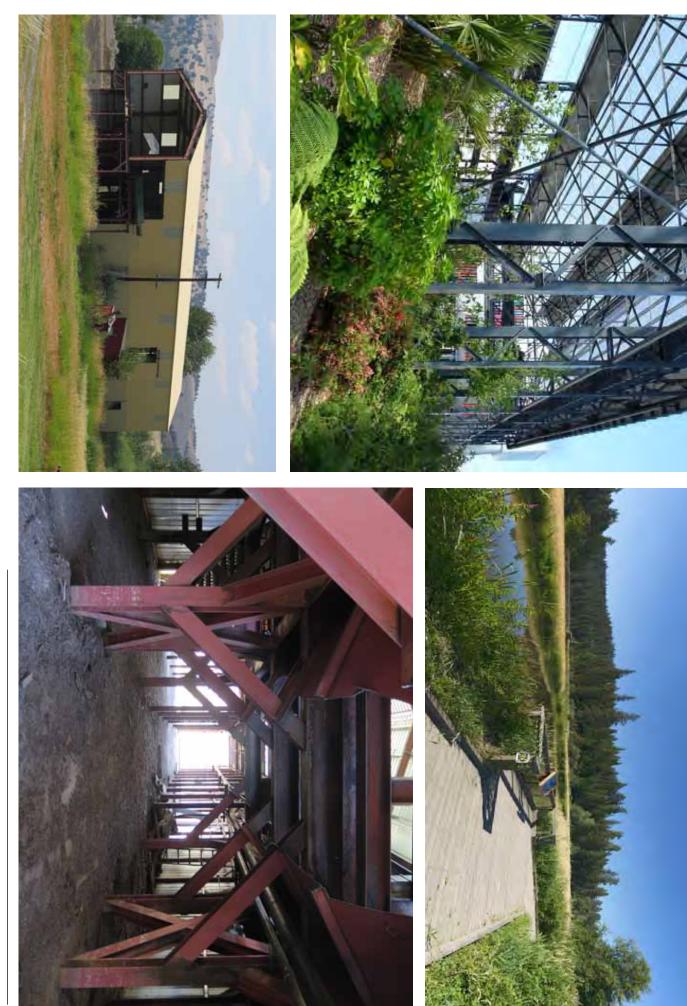
## PAVILION PLAN & PERSPECTIVE



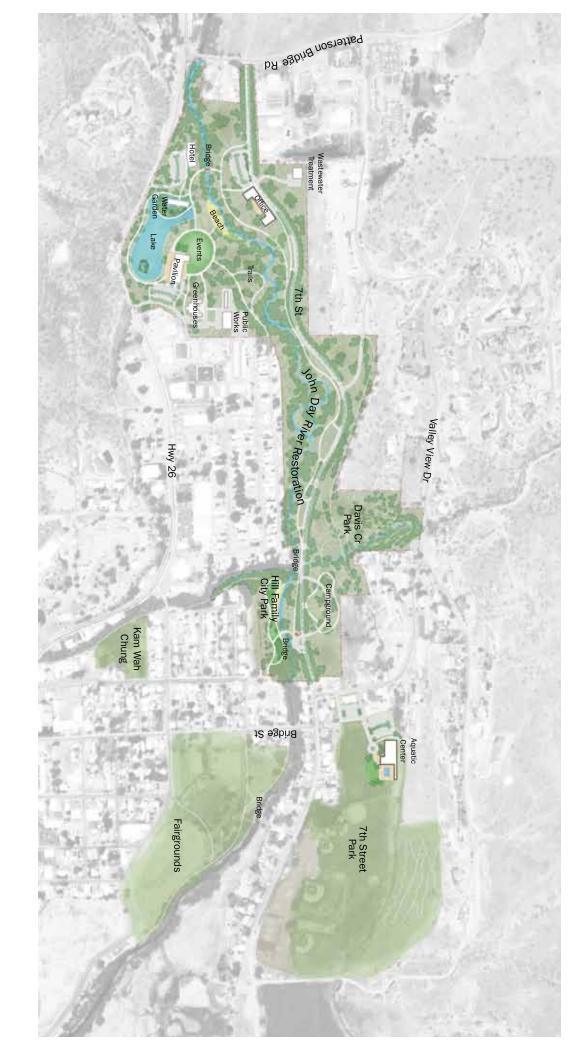


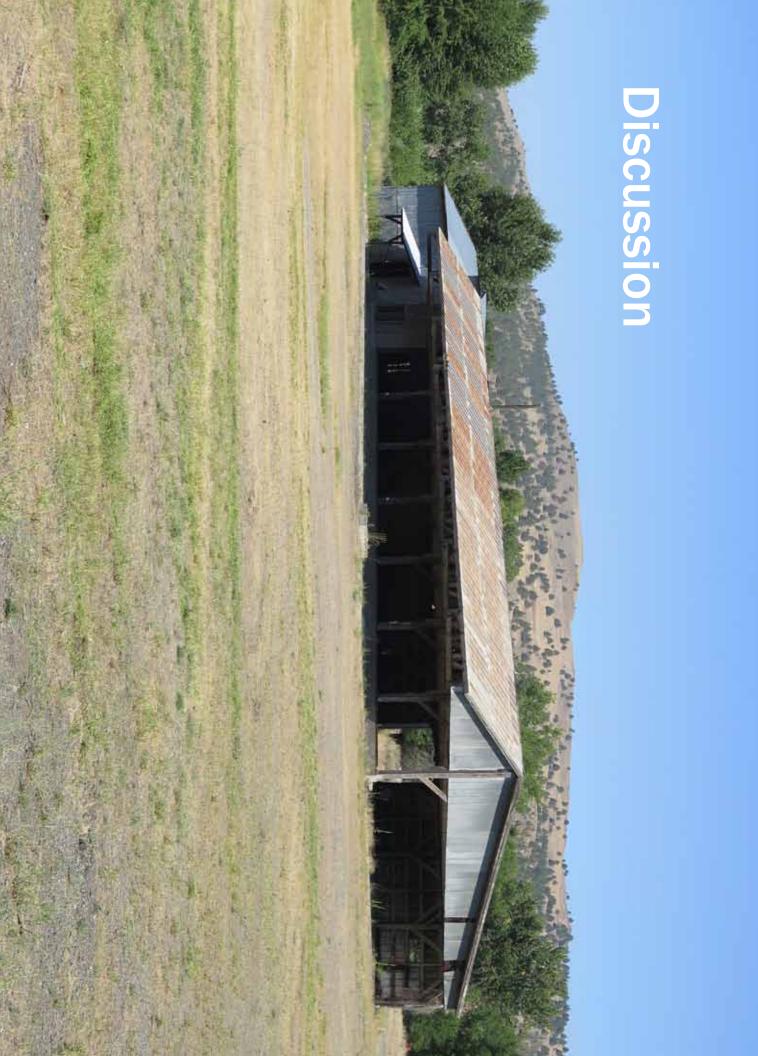


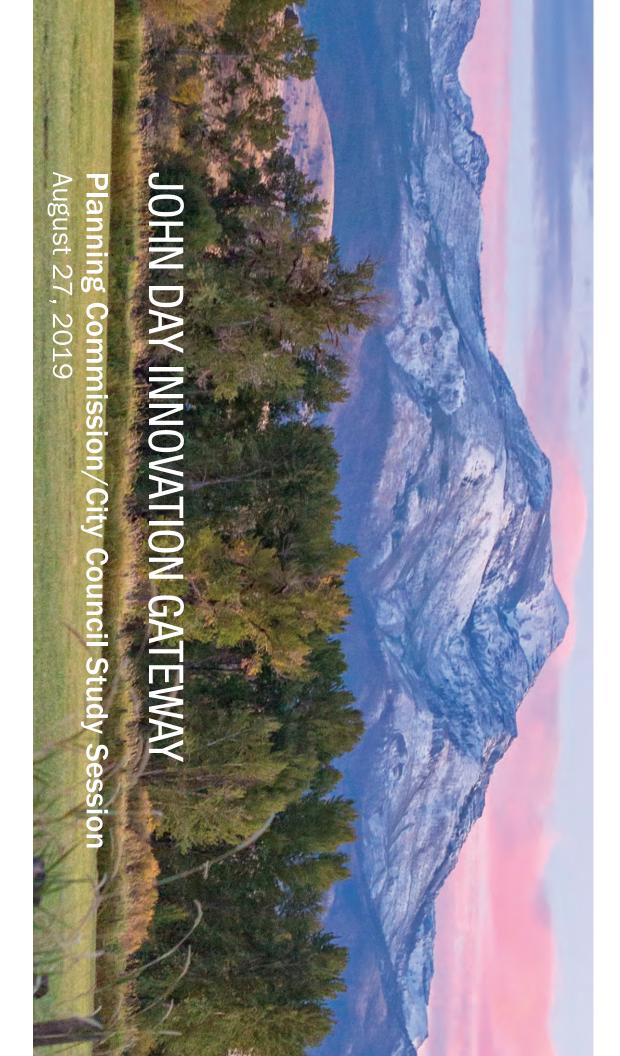
## **OREGON PINE: SAWMILL RESTORATION**

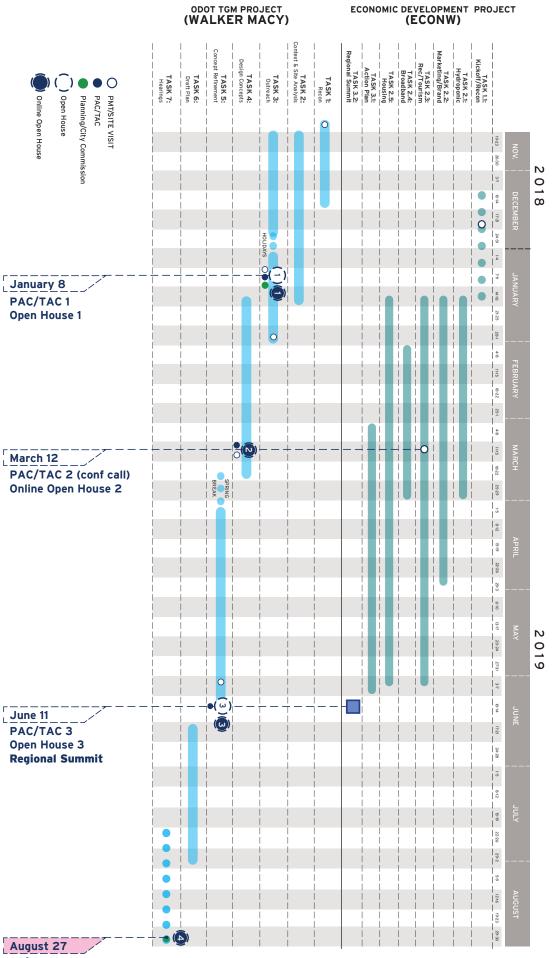










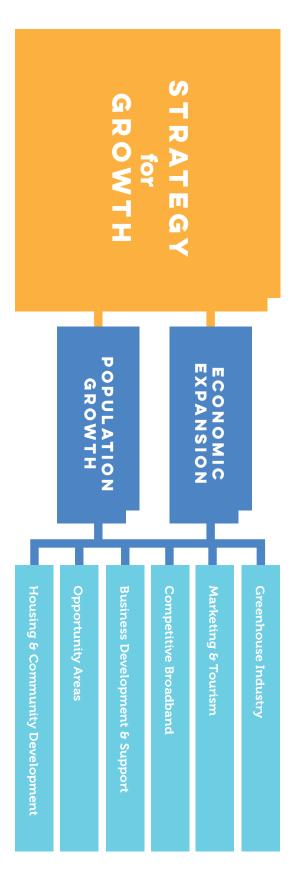


CC/PC Hearings

JOHN DAY INNOVATION GATEWAY SCHEDULE

## tors of some of the second sec

TARGET POPULATIONS: ACTIVE RETIREES · DIGITAL COMMUTERS · YOUNG WORKING FAMILIES



#### EXPERIENCED & DRIVEN CITY LEADERSHIP

*le* are a proactive City. We are not waiting for help to arrive. We are actively reating our desired future rough persistent advocacy f the needs of our citizens and businesses.

#### PROVEN SUCCESS

We have secured over \$13 million in funds to support projects across our community. We are investing in housing, roads, utilities, and park and recreational facilities. We have already constructed new greenhouses to produce local food. We built a mountain bike park. Soon we will have a new wastewater treatment plant, pool, and community center.

#### A MODEL FOR RURAL ECONOMIC DEVELOPMENT

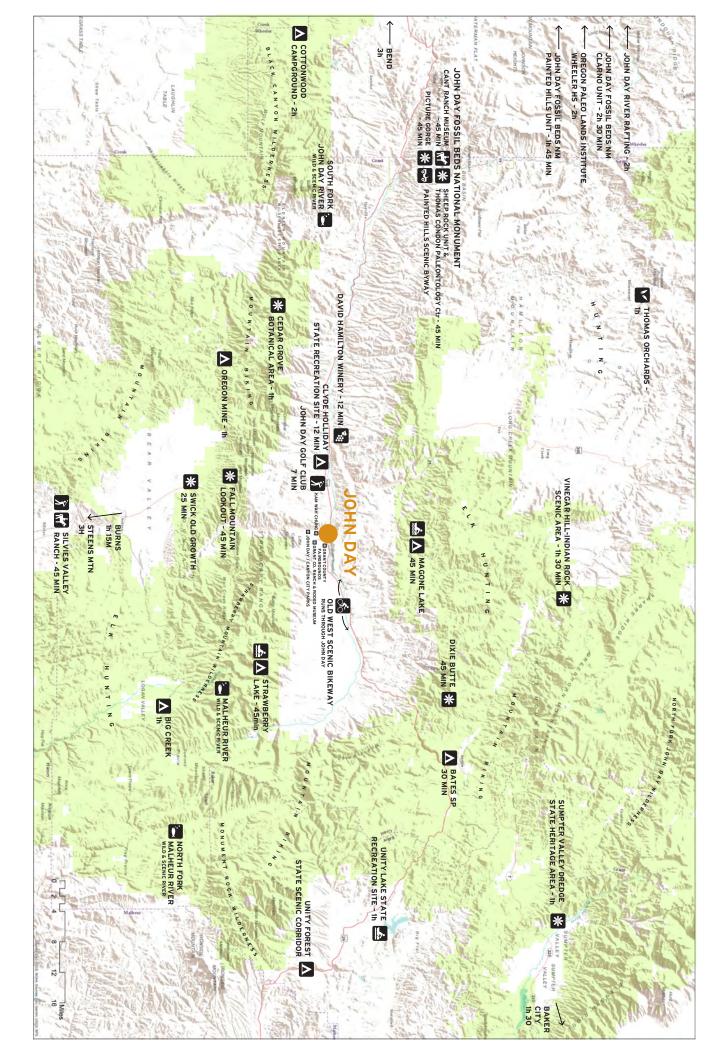
investing a new model for rural economic development for the 21st century. Many rural cities across the Western United States face similar nallenges. Our success car ierve as a lesson for these communities.

#### FUTURE INDUSTRIAL DEVELOPMENT IRONWOOD ESTATES INNOVATION GATEWAY JOHN DAY FIF POTENTIAL RESIDENTIAL DEVELOPMENT II.

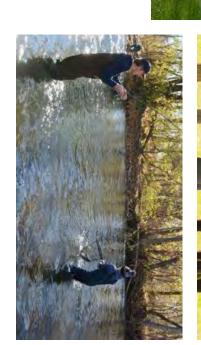
### **OPPORTUNITY AREAS**

# OPPORTUNITIES





### JOHN DAY BASECAMP





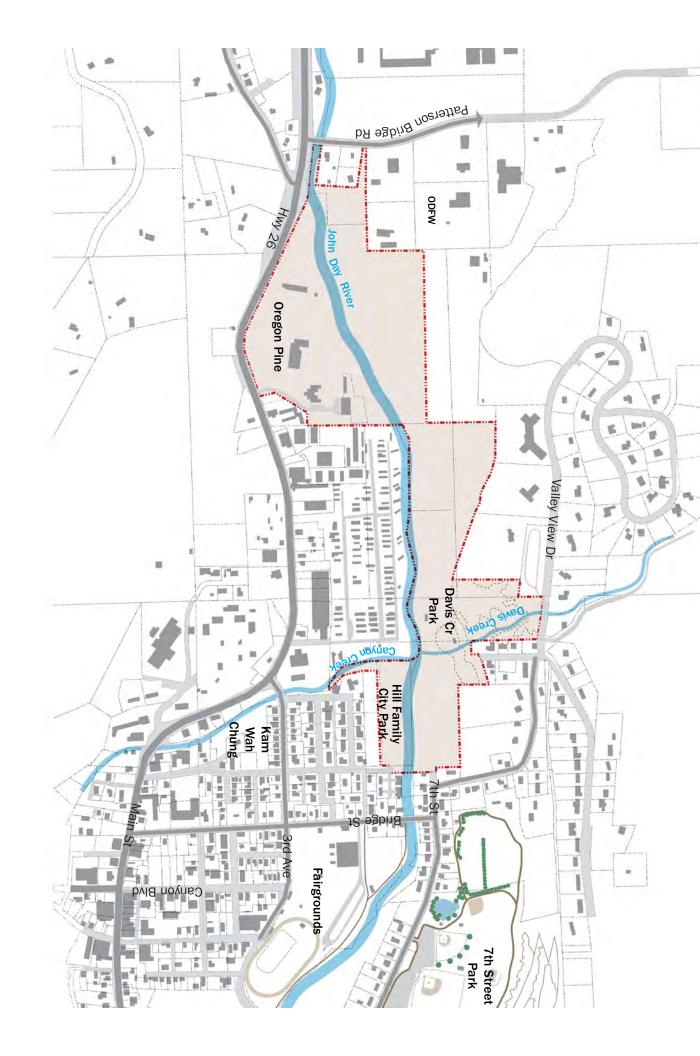




## INNOVATION GATEWAY AREA PLAN JOHN DAY'S

# The Gateway Project should:

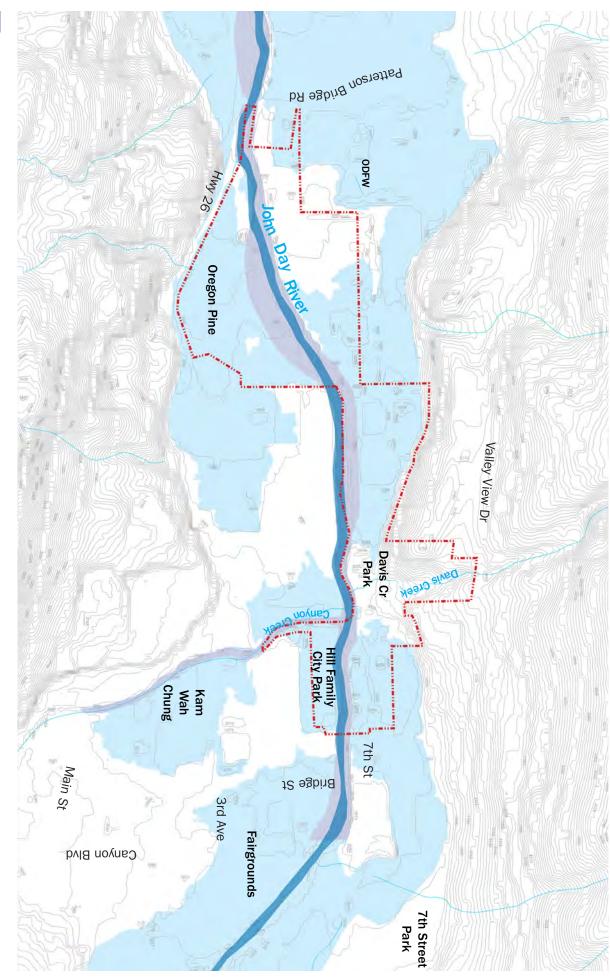
- 1. Create a thriving destination
- 2. Attract investment and jobs
- 3. Honor John Day's identity and character
- 4. Promote a connected and healthy community
- 5. Create opportunities for walking and biking
- 6. Provide public access to the John Day River and
- Canyon Creek
- 7. Efficiently use public resources and land; and
- 8. Support innovation in conservation



## **Project Components:**

- RIVER RESTORATION
- PARKS/OPEN SPACES + TRAILS
- 7th STREET EXTENSION

FLOODWAY 100 YR FLOOD PLAIN



#### Historic Mining Operations



**Existing Conditions** 



### **River Restoration Concept Example**



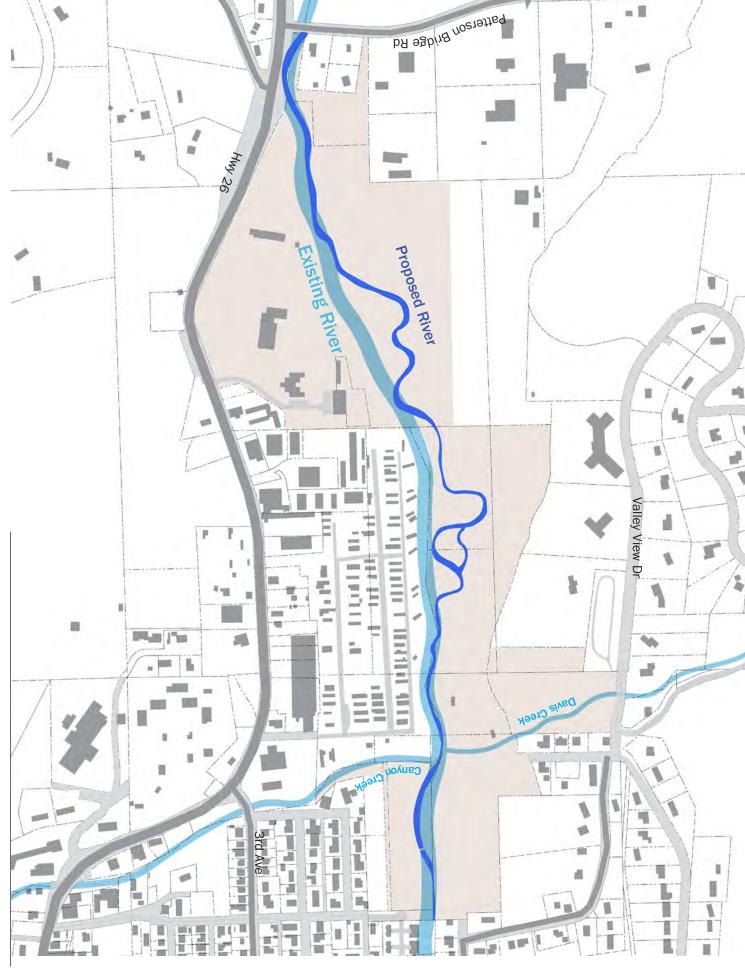
JOHN DAY RIVER RESTORATION







## JOHN DAY RIVER RESTORATION



#### Benefits

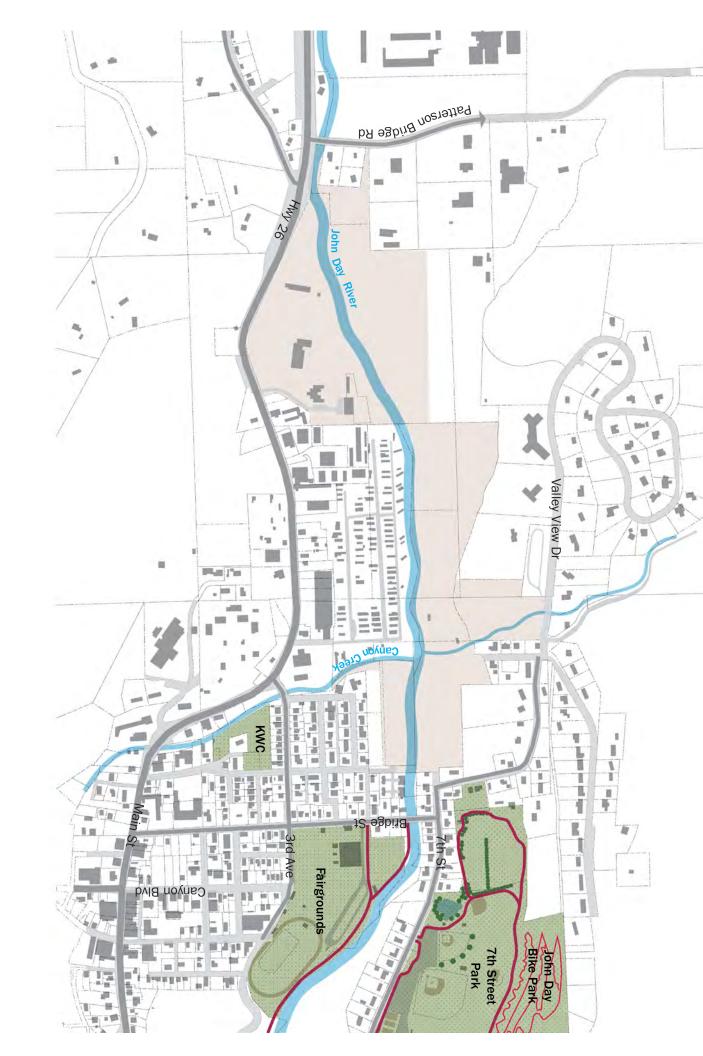
- 1. Overall increased flood conveyance is likely to reduce flood impacts to nearby infrastructure
- 2. Potential to create constructed wetlands to reduce need for City stormwater treatment
- 3. More recreational trails and access to natural area
- 4. Improved fish habitat
- 5. Additional public access for fishing
- 6. Enhanced land values for neighboring properties
- 7. Improved visual identity for City
- 8. No impacts to current irrigation diversion

# Considerations for Future Study / Challenges

- 1. Further analysis on current wastewater ponds
- 2. Past gold dredging left unstable subsurface conditions, need more geotechnical study
- 3. Needs additional hydrologic engineering feasibility study
- 4. City could still build proposed improvements without river restoration, but there less

visual and recreational benefit

- 5. Permitting in place
- 6. Funding in place
- 7. Benefits outweigh the costs



# AN INTEGRATED PARK AND TRAIL SYSTEM

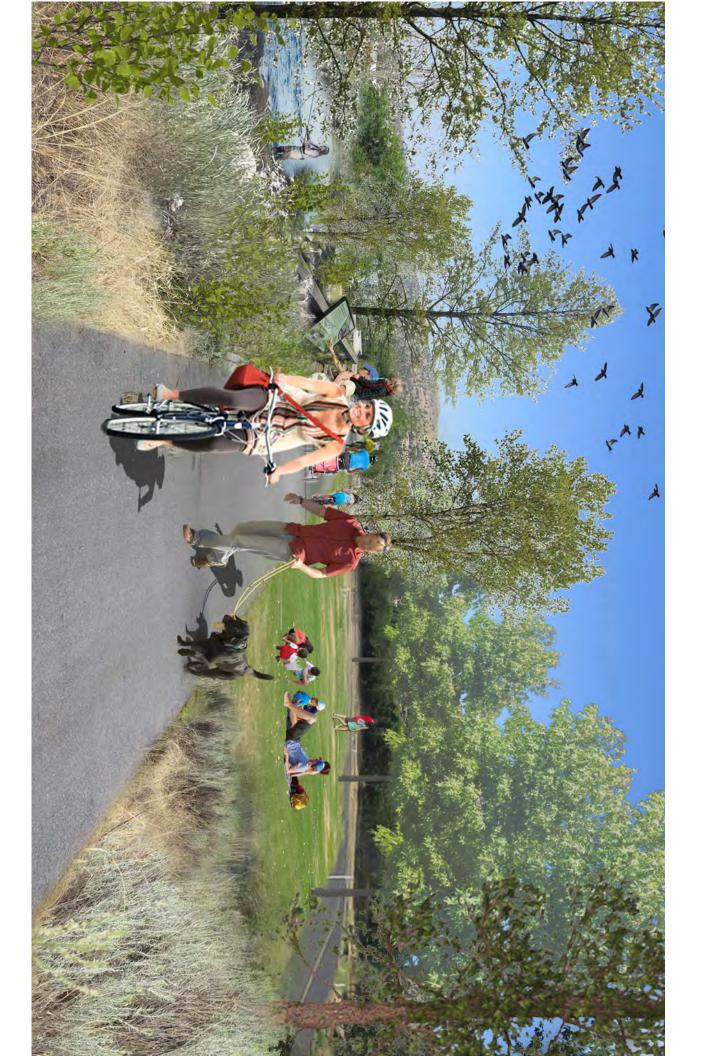
Proposed Trail

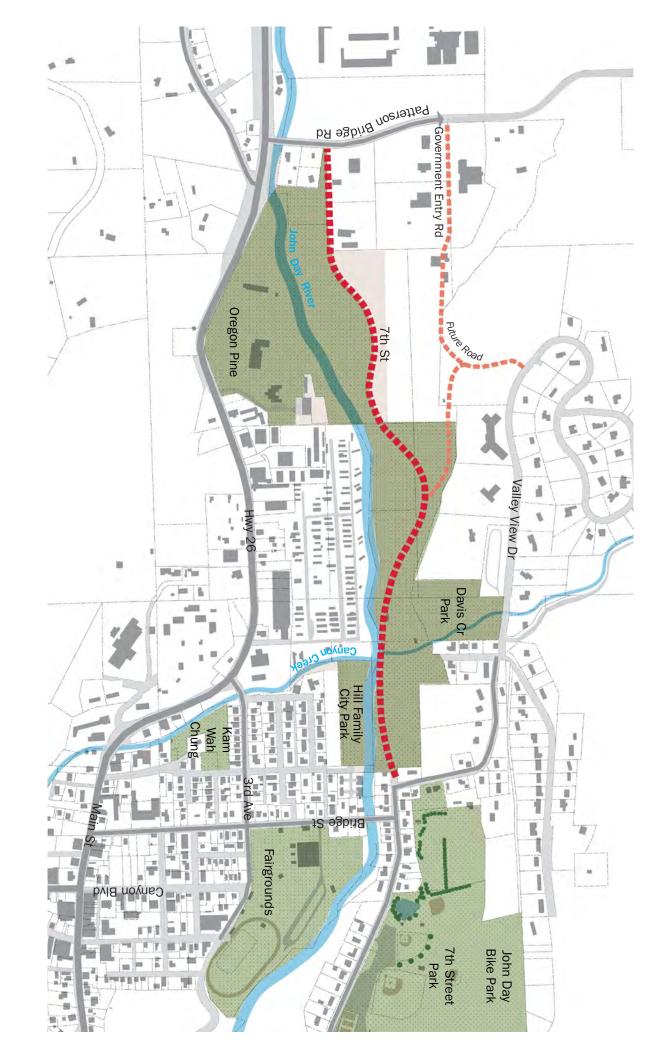
Proposed On Street Pedestrian Connection

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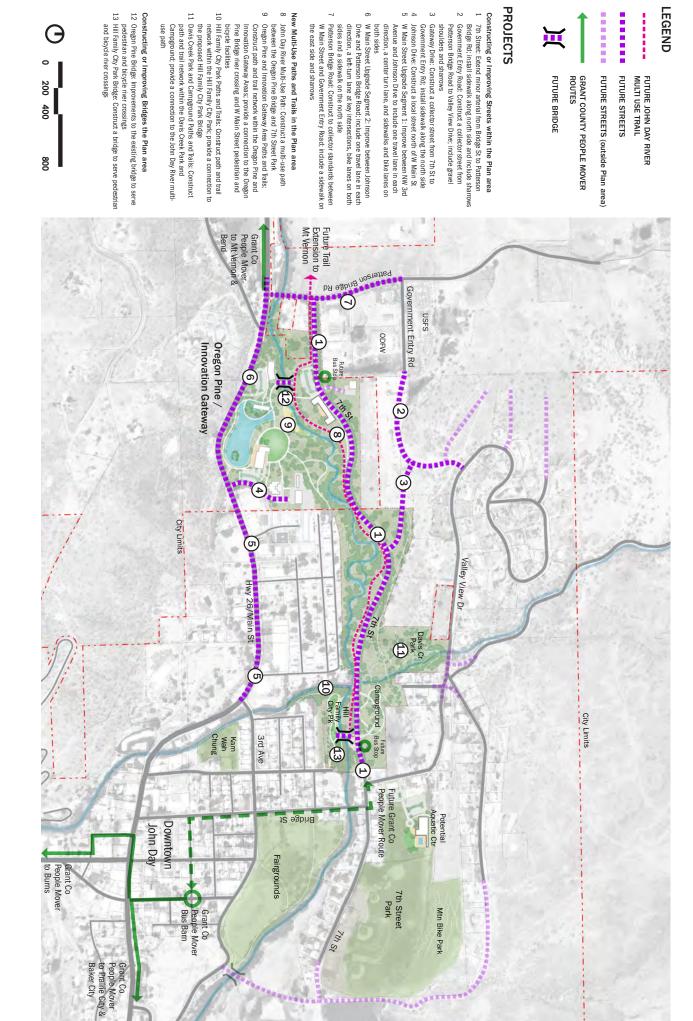
I.











Trails and Recreations Oregon Pine-area trails \$136,000 BUILD Grant and Funding acquired Phase 2 –	Charolais Heights     \$244,143     BUILD Grant and Non-Federal     Funding acquired     Phase 3 -       Intersection     Non-Federal     and development     Implementation       Improvements     Sources     initiated     /mid-term       Other City Roads     \$6,501,779     Sources     initiated	Other new City Roads         Trails and Recreation         \$1,778,984           3rd Ave Bridge and         \$6,257,636         BUILD Grant and         Funding acquired         Phase 3 -         Subtotal           Charolais Heights Street         Non-Federal         and development         Implementation         Grand Total         \$15,122,232           Ext.         Sources         initiated         /mid-term         Source: Walker Macy, ECONorthwest		acquired Phase 2 - pground Implementation Gravel Parking Lot (Planer \$153,000 ment /near-term shed)	initiated /near-term lot,/Trailhead \$23,970	Implementation Interpretive Overlooks on \$204,000 /near-term River \$165.068 BIIII D Grant Water treatment Phase 2	Implementation Integrated Park \$555,864 /near-term Transportation \$131,080 BUILD Grant Funding acquired Phase 2 - Infrastructure	Government Entry Road       \$652,363       BUILD Grant       Funding acquired       Phase 2 -       Campground Trails       \$34,150       BUI         Extension       Implementation       Implementation       Nor         Gateway Drive       \$684,097       BUILD Grant       Funding acquired       Phase 2 -	7 <sup>th</sup> Street Extension \$324,182 BUILD Grant Funding acquired Phase 3 - Multi-use Trail along John \$460,000 BUI (East) Implementation Day River Nor /mid-term	Study Area Roads         Oregon Pine-area         \$122,000         BUI           7th Street Extension         \$4,826,779         BUILD Grant         Funding acquired         Phase 2 -         Oregon Pine-area         \$122,000         BUI           (West)         Implementation         concrete paths         Nor         Nor         Sou	Project Cost Range Identified Triggers Phased Funding Actions/Timeframe Source
		\$1,778,984 \$15,122,232 Northwest			\$23,970		\$555,864	\$34,150		\$122,000	
			Sources	Sources BUILD Grant and Non-Federal	BUILD Grant and Non-Federal	BUILD Grant and Non-Federal Sources	BUILD Grant and Non-Federal Sources	BUILD Grant and Non-Federal Sources	BUILD Grant and Non-Federal Sources	BUILD Grant and Non-Federal Sources	
			renovation initiated	Funding acquired and planer shed	Funding acquired	Funding acquired	Funding acquired	Funding acquired and campground development initiated	Funding acquired	Funding acquired	

Phase 2 -Implementation /near-term Phase 2 -Implementation /near-term

Phase 2 – Implementation /near-term

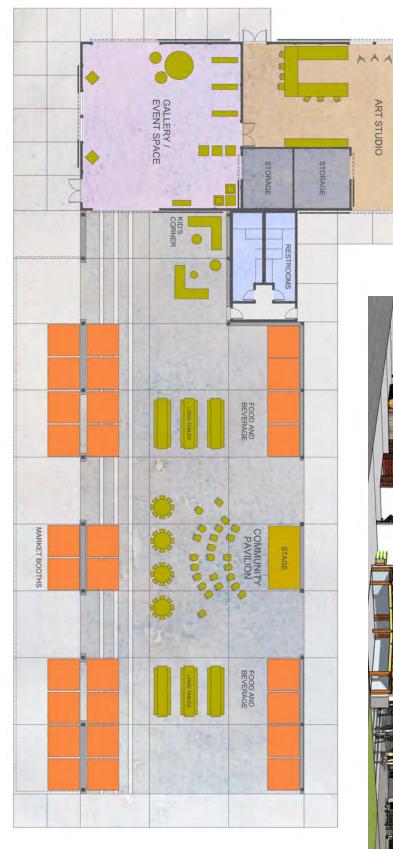
# IMPLEMENTATION: TRANSPORTATION FUNDING

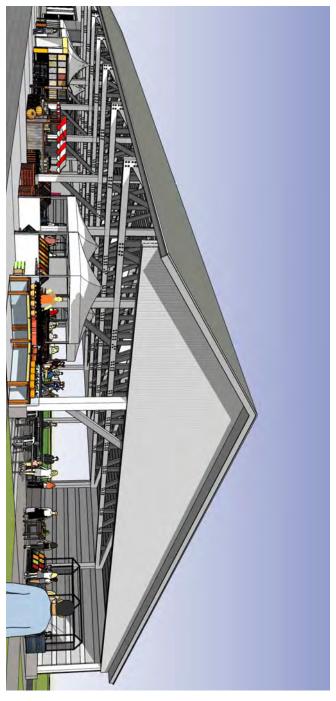
	n/a	Varies	New	improvements	Debt Financing
				Capital	
n/a		Affected Property Owners	New	Capital improvements	Local Improvement Districts
n/a		City Council action	New	Capital improvements or maintenance	Transient Room Tax
\$0.20 per \$1,000 in assessed value (per year, for 5 years)	\$0.20 assessed fo	Voter Approval	New	Capital improvements or maintenance	Property Tax Levy
\$18 for passenger cars, and \$8 for motorcycles per year	\$18 for p <i>a</i> \$8 for r	Voter Approval (County- wide)	New	Capital improvements or maintenance	County Vehicle Registration Fee
One cent per gallon	One	Voter Approval	New	Capital improvements or maintenance	Local Fuel Tax
\$1 per month for residential units and \$.01 per month per square foot for non-residential uses	\$1 F residen per mon for nor	City Council action	New	Capital improvements or maintenance	Transportation Utility Fee
\$500 per peak hour trip for new development	\$500 per new	City Council action	New	Capital improvements	System Development Charge
Example Charge	Exa	Action Required to Implement	Existing or New Funding Source	Allowed Use of Funds	Funding Option
		ing Options	ation Fund	Table 2: Potential Transportation Funding Options	Table 2: Poter











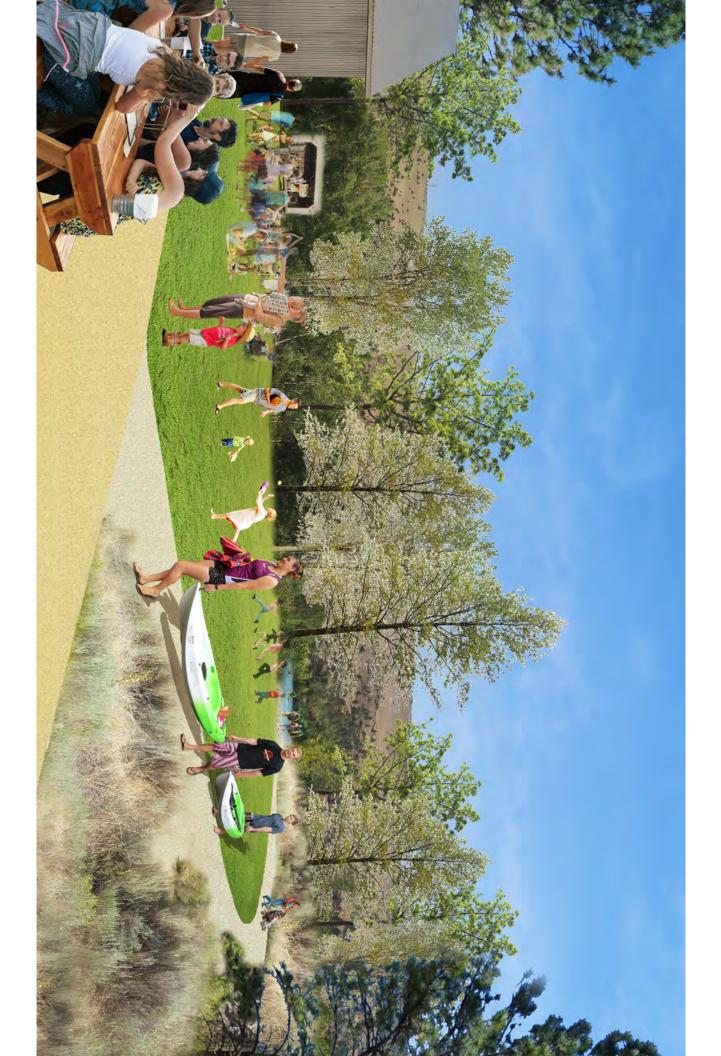
ART STUDIO

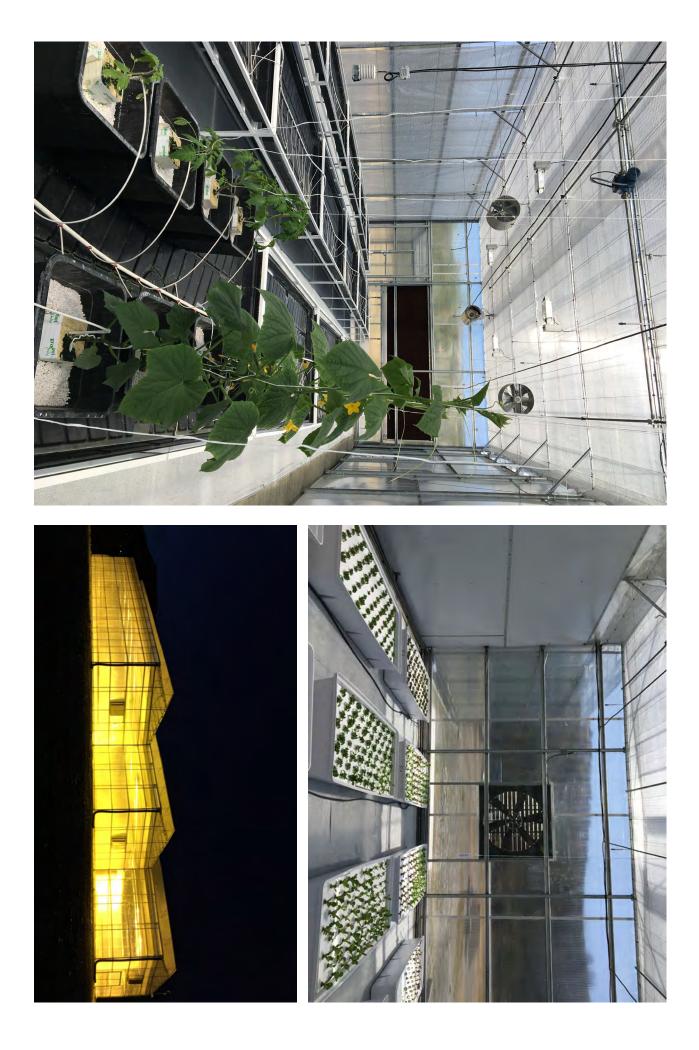










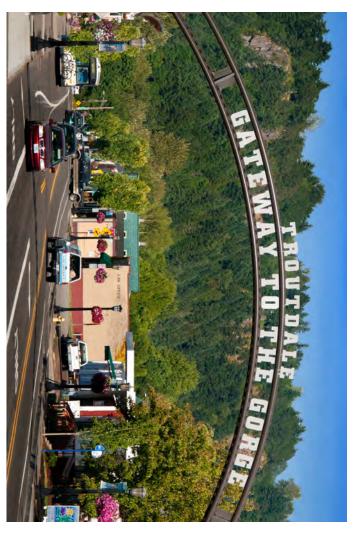








# 7TH STREET PARK + AQUATIC CENTER









## PLACEMAKING OPPORTUNITIES

### **Public Toilets**



### Wayfinding



### Nature-Play



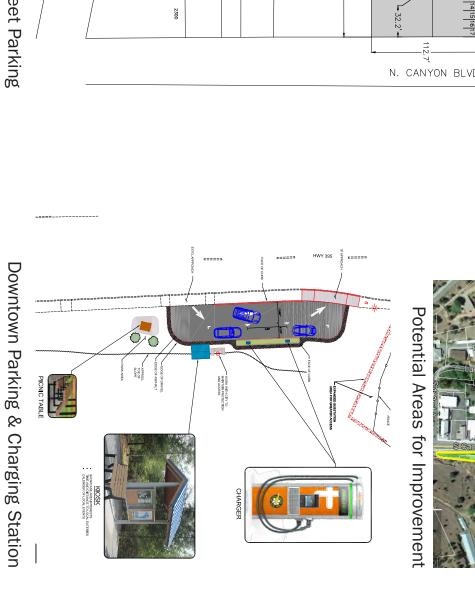
Context-appropriate design

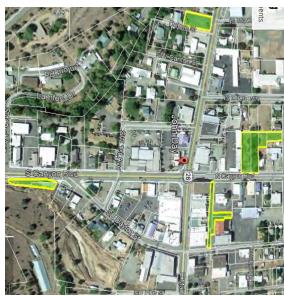


Public Art (Great Salt Lick, Baker City)

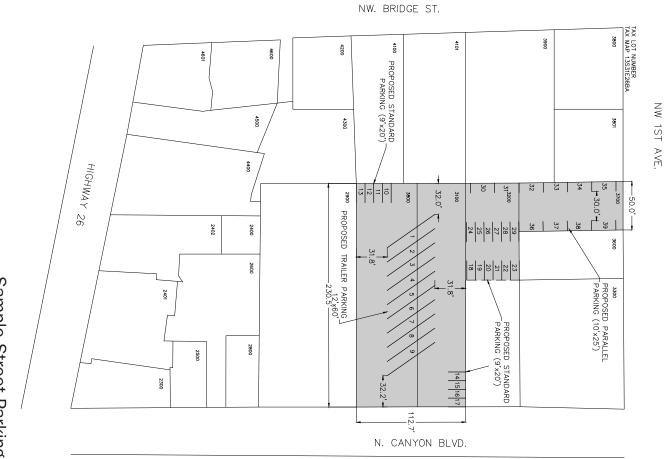


### DOWNTOWN PARKING





### Sample Street Parking





# Does the plan meet the Goals?

- **1.** Create a thriving destination
- 2. Attract investment and jobs
- 3. Honor John Day's identity and character
- 4. Promote a connected and healthy community
- 5. Create opportunities for walking and biking
- 6. Provide public access to the John Day River and
- Canyon Creek
- 7. Efficiently use public resources and land; and
- 8. Support innovation in conservation

# CODE & POLICY UPDATES IMPLEMENTATION:



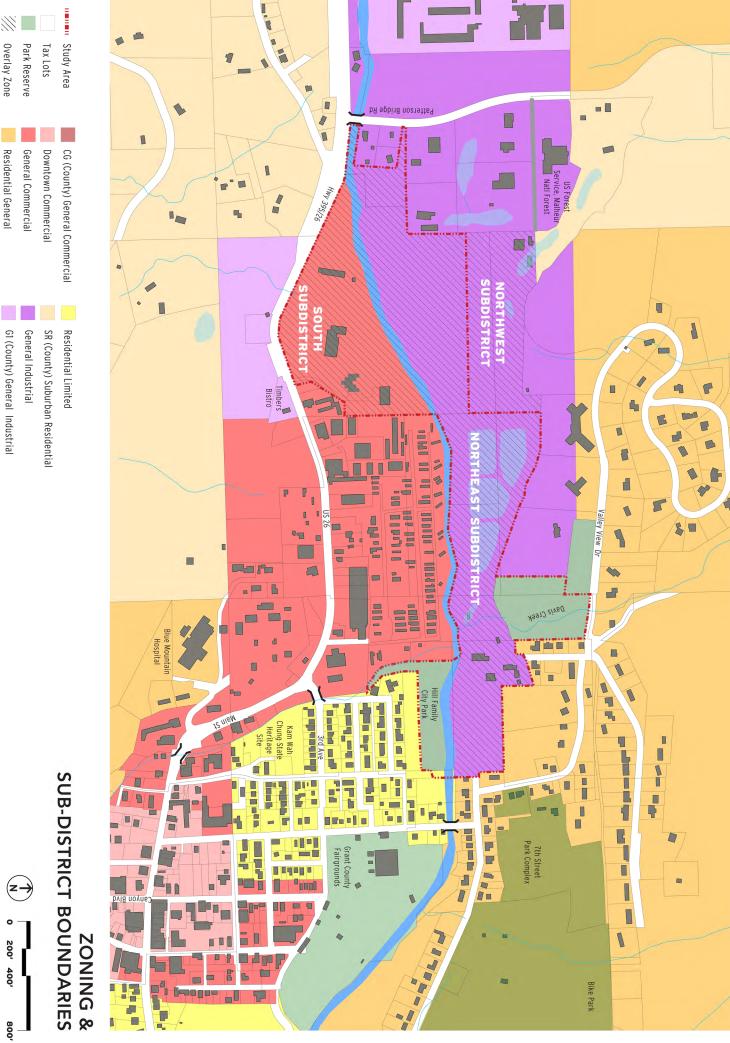
### Comp Plan Amendments Objective:

consistent with JDIG Plan Ensure Comp Plan includes policies supporting and

- New policies related to economic development and the IG Plan
- New Comp Plan text referencing the IG Plan New policy focused on water re-use

### Development Code Amendments Objectives:

- Ensure the Code implements and is consistent with the IG Plan
- Build on existing base zone requirements
- Adjust or vary requirements where applicable



800'

# INNOVATION GATEWAY OVERLAY ZONE ELEMENTS

- Subdistricts
- · Land Uses
- Site Design Standards
- Block Layout and Architectural Standards
- Pedestrian Amenities
- Special Standards

# IMPLEMENTATION: CODE & POLICY UPDATES

District			Northwest Sub-	Northeast Sub-
Is       Commercial Uses         Commercial Uses       Image         Commercial Uses       Image         age       Image     <	USE	South Sub-District	District	District
Is       Commercial Uses         Commercial Uses       Image         age       Image         ad       Image         floor area       Image         ad, equal to or great-       Image         OO square feet of       Image         Space       Image         and open space       Image	Residential uses	P	z	Z
Is       Commercial Uses         Commercial Uses       Image         rvicing or Vehicle       Image         age       Image         age       Image         e       Image         age       Image         e       Image         d       Image         filoor area       Image         d, equal to or great-       Image         OO square feet of       Image         Space       Image         ith the John Day In-       Image         eway Area Plan       Image         and open space       Image	Drive-up/Drive-in/Drive-through	N	N	N
Commercial Usesageageedd <t< td=""><td>lotels and Motels</td><td>P</td><td>N</td><td>Ν</td></t<>	lotels and Motels	P	N	Ν
age and open space	Retail Sales and Commercial Uses	ү	S (size limit)	S (size limit)
age       e         e       i         d       i         d       i         nd       Production         d       i         i       i	uick Vehicle Servicing or Vehicle	Z	S	S
e e d d nd Production d d freight Movement Freight Movement d, less than 20,000 of floor area d, equal to or great- 00 square feet of Space Space Space Space Space Space Space Space Space Space	Cepair	Z	Ś	Z
d nd Production d Freight Movement Freight Movement Freight Movement I I I I I I I I I I I I I I I I I I I	ndustrial Service		(	
nd Production d Freight Movement Freight Movement I I I I I I I I I I I I I I I I I I I	Fully enclosed	Z	P	P
nd Production d Freight Movement Freight Movement d, less than 20,000 of floor area d, equal to or great- d, equal to or great- 00 square feet of 00 square feet of Space Space Space way Area Plan and open space		Z	Z	Z
d     Freight Movement       Freight Movement     Integration       Gradie     Integration       Gradie     Integration       Space     Integration       State     Integration       Space     Integration       State     Integration       Space     Integration       State     Integration       Space     Integration       State     Integration       State <td>Nanufacturing and Production</td> <td></td> <td></td> <td></td>	Nanufacturing and Production			
Freight Movement Freight Movement (), less than 20,000 (), equal to or great- d, equal to or great- 00 square feet of Space Space Space eway Area Plan eway Area Plan and open space	Fully enclosed	S	P	P
Freight Movement         d, less than 20,000         of floor area         d, equal to or great-         00 square feet of         Space         Shace         ith the John Day In-         eway Area Plan         and open space		N	P	Р
d, less than 20,000 of floor area d, equal to or great- 00 square feet of Space eway Area Plan and open space	Varehouse and Freight Movement	N	I	Π
d, less than 20,000 of floor area d, equal to or great- 00 square feet of Space Space ith the John Day In- eway Area Plan eway Area Plan	Vaste Related	-	N	N
area area al to or great- are feet of John Day In- Area Plan en space	Vholesale Sales			
al to or great- are feet of John Day In- Area Plan	fully enclosed, less than 20,000 square feet of floor area	z	Р	υ
John Day In- Area Plan		Ν	Z	Z
John Day In- Area Plan en space		N	Ν	N
Consistent with the John Day In- novation Gateway Area Plan Other parks and open space	Parks and Open Space			
Other parks and open space		Р	Р	Ρ
uses		CU	CU	CU
Agriculture – Greenhouses and simi-	griculture – Greenhouses and simi- ar horticulture	Ρ	P	Ρ

**DISCUSSION:** 

already addressed in the Greenway or Floodplain Overly districts or the Base Zones? Are any limitations needed that are not

USE	South Sub-District	Northwest Sub- District	Northeast Sub- District
Minimum Lot Area			
<ul> <li>New dwelling built in conjunction</li> </ul>			
with a permitted commercial or industrial use located above the	None	1	I
ground floor			
All other residential uses	None	1	1
Minimum Lot Width (feet)	20	1	1
Minimum Lot Depth (feet)	None	I	1
Maximum Building/Structure Height (feet)	I	I	1
Minimum Setbacks	-	-	I
Maximum Building Size, Retail Com- mercial Uses (square feet)	I	5,000	5,000
Lot Coverage , maximum building coverage (Foundation plane as % of site area)	85%	85%	85%
Landscape Area (% site area). Landscape area may include	15%	15%	15%
a combination of plant beds, planters, and plaza space			
Build-to-Line (feet)	20	1	1

### DISCUSSION:

feet. Do we want the same maximum in this portion of the JDIG area? 35' height would help Maximum building height in CG zone is 45 Hwy 26. preserve views of the river and mountains from

development that might want to be 45' high to provide flexibility for certain types of (e.g., hotels, etc.)? Is that a good reason for maintaining a lower height in this area, or is it more important

## Site Layout and Design, Building Orientation and **Commercial Block Layout**

- Downtown commercial area block layout requirements not applicable
- Building entrances oriented to primary vehicular or pedestrian access road or pathway
- No parking between accessway and building
- Walkways must connect building to interior parking
- Block length standards?

### **DISCUSSION:**

What block length or perimeter standards should be used in this area?

# Architectural Design Standards

Using a subset of commercial zone design standards:

- Pathway protections
- Awnings, weather protection
- Regularly shaped windows
- Tall ceilings and windows for ground floor retail
- Distinct base, middle, top of multi-story buildings
- Roof detailing
- Block length standards?

### **DISCUSSION:**

- Are these appropriate?
- Are any other standards needed?
- Should any of these standards apply to the area north of the river?
- Are there any conflicts with JDIG objectives?

## Special Use Standards

- Landscaping use native and drought-tolerant vegetation
- Exterior lighting
- Focus light downward
- Pole mounted lights maximum of 20 feet tall
- No uplighting for façade lighting
- Pedestrian lighting required on all public walkways, plazas, and courtyards in areas open to the public after dark

### **DISCUSSION:**

specificity regarding locations needed? We limited the lighting requirements to areas open to the public after dark; is any more

# MAINTAINING MOMENTUM IMPLEMENTATION:



IMPLEN	
MENTATIC	
ON: PAF	
RTNERS	
<b>RS &amp; CHAN</b>	
<b>MPIONS</b>	

IMPLEMENTING PARTNERS:	FUNDING PARTNERS:	DUAL ROLE PARTNERS:
Organizations that will take a necessary and active role in project implementation	Public, private, and nonprofit organizations that will support strategic investments	Implementation and Funding
City departments (public works)	USDA Rural (broad array of funding programs)	Grant County (economic development, roads)
John Day / Grant County Chamber of Commerce	Business Oregon	Travel Oregon
John Day Canyon City Parks and Recre- ation District	Oregon Department of Transportation	Private businesses
Grant County municipalities: Canyon City, Prairie City, Dayville, etc.	U.S. Department of Commerce, Economic Development Administration (EDA)	
Greater Eastern Oregon Economic Development District (GEOEDD)	Oregon Department of Land Conservation and Development	
North Fork John Day Ranger District (USFS)	Oregon Parks and Recreation Department	
School District	Oregon Housing and Community Services	
Regional WIB	U.S. Housing and Urban Development	
Eastern Oregon Regional Solutions Team		
Community organizations		
Universities		
Consultants		
Planning Commission		
Advisory Committee		
City Council		

IMPLEMENTATION: GRANT FUNDING SUCCESS

Brownfield Redevelopment Fund CDBG Construction Ford Family Foundation USDA Rural Development	<b>\$13.7 Million</b> 6 grants	USDA ReConnect USDA Community Connect	<b>\$25.3 Million</b> 3 grants	<b>\$4 Million</b> 17 grants
nstruction USDA Community Facilities Development Oregon Water Resources Dept	<b>pending</b> in FY20 and beyond	inity Connect FHA Build	<b>applied</b> and <b>waiting</b> for decisions FY19	awarded, completed or recommended for funding

### **NEW HOUSING**

## **BROWNFIELD GRANTS**

# HOTEL AT INNOVATION GATEWAY











WASTEWATER TREATMENT PLANT CONSTRUCTION



# DOWNTOWN PLACEMAKING

# OPRD COORDINATION ON KAM WAH CHUNG

### TRAIL CONSTRUCTION





**AQUATIC CENTER DESIGN & BOND MEASURE** 

# PROMOTE JOHN DAY AS RECREATIONAL GETAWAY

- Maps that Make Sense
- **EPA Grant?**
- **Marketing Campaign**
- **Rentals & Outfitters**





